

# County of Los Angeles **WILLOWBROOK** TOD **SPECIFIC PLAN**

Draft - October 2015



**Metro**



# County of Los Angeles

# **WILLOWBROOK TOD SPECIFIC PLAN**

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October 2015

**DRAFT**

Prepared by



The Arroyo Group  
Stanley R. Hoffman Associates  
The Mobility Group  
JMC<sup>2</sup>  
ESA



**Metro**<sup>®</sup>

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## 1.1 Specific Plan Location

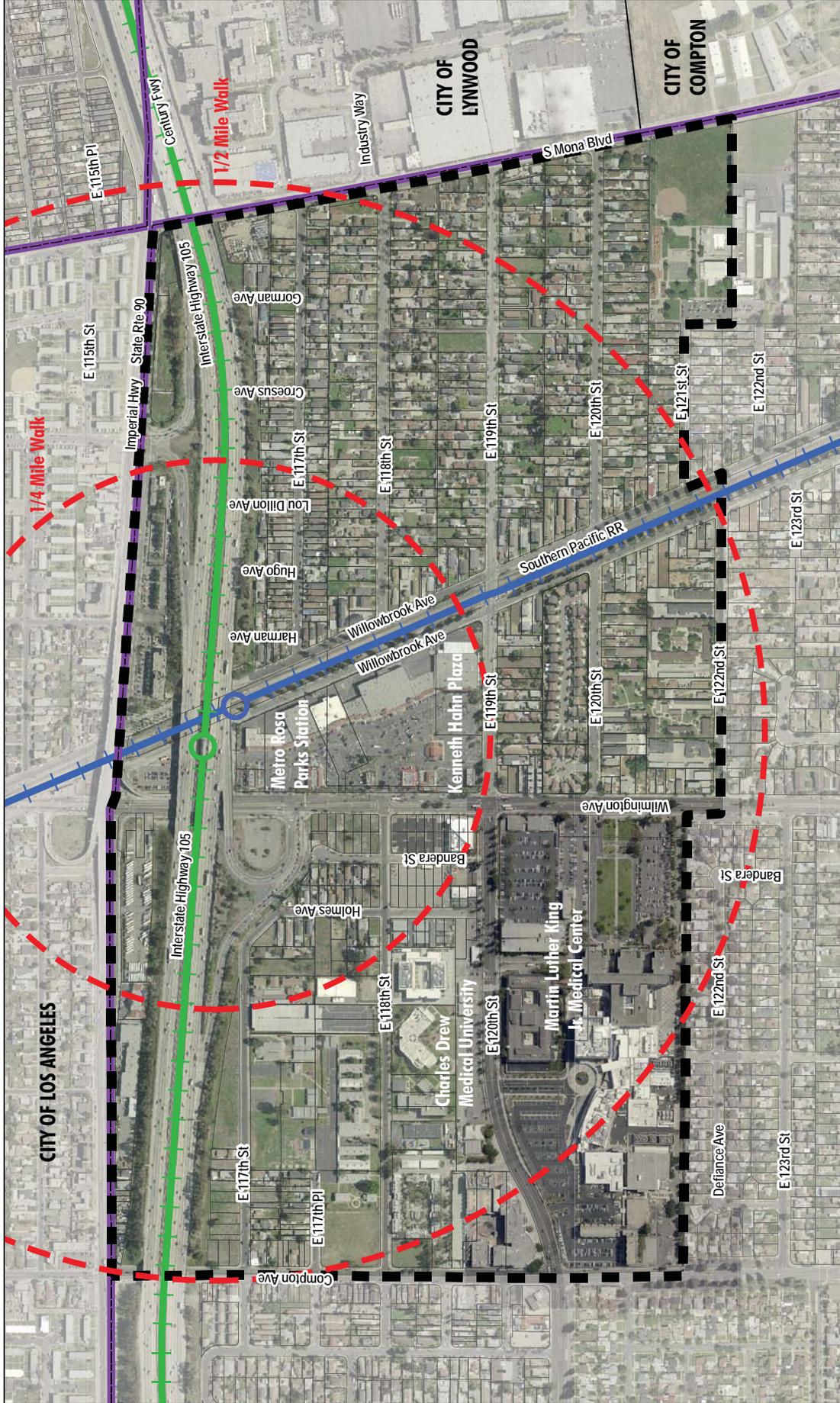
The Willowbrook Transit Oriented District (TOD) Specific Plan area is located in the unincorporated community of Willowbrook within Los Angeles County. It is located along the I-105 freeway and at the junction of the Metro Blue and Green Lines. The project area is approximately ten miles south of Downtown Los Angeles and is bordered by the City of Los Angeles to the north, and the City of Lynwood and City of Compton to the east.

The Specific Plan is focused around the Willowbrook/Rosa Parks Station, which is a transfer station on the Metro Blue Line and Green Line (see Figure 1-1). The Specific Plan generally contains the southern portion of the area within a half mile radius of the station. The Specific Plan area is approximately 312 acres. Some of the key facilities that are located within the Specific Plan area are Martin Luther King, Jr. Medical Center, Charles R. Drew University of Medicine and Science, Kenneth Hahn Plaza, Willowbrook Library, and Martin Luther King, Jr. Center for Public Health.

### ***What is a Specific Plan?<sup>1</sup>***

*A specific plan is a tool to systematically implement the General Plan within an identified project area. Specific plans are used to ensure that multiple property owners and developers adhere to a common plan or coordinate multiple phases of a long-term development. Specific plans must further the goals and policies of the General Plan. Specific plans must be consistent with the General Plan. No local public works project may be approved, no tentative map or parcel map for which a tentative map was not required may be approved, and no zoning ordinance may be adopted or amended within an area covered by a specific plan unless it is consistent with the adopted specific plan. The authority for preparation and adoption of specific plans is set forth in the California Government Code, Sections 65450 et seq.*

<sup>1</sup> Chapter 6: Land Use Element, Los Angeles County General Plan



**Figure 1-1  
Specific Plan Area**

-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  1/4 Mile and 1/2 Mile radius around Metro stations

**WILLOWBROOK TOD SPECIFIC PLAN**



## 1.2 Background

The Los Angeles County General Plan identifies TODs as priority policy areas throughout Los Angeles County. In order to create vibrant and healthy TODs, the County envisions developing specific plans for all of the TODs that are located in the unincorporated areas.

The Willowbrook TOD Specific Plan is intended to demonstrate a clear vision for the Willowbrook community, create community benefits, and is a tool to facilitate economic development. The Specific Plan includes an associated Program Environment Impact Report (EIR), which creates certainty for future development by providing environmental clearance for specified levels of development intensity, and potentially streamlines CEQA review.

## 1.3 Purpose

The purpose of the Willowbrook TOD Specific Plan is to revitalize the community within the project area and improve access to all modes of transportation, including transit, walking and bicycling. Building off of the goals and policies outlined in the General Plan, the Willowbrook TOD Specific Plan will encourage transit oriented development, promote active transportation, reduce vehicles miles traveled, create community benefits, and streamline the environmental review process for future projects.

The Specific Plan is anticipated to facilitate development, especially residential and employment-generating uses, proximate to the Willowbrook/Rosa Parks Station. The primary objectives of the Specific Plan are to identify land use options that include mixed uses, increased housing opportunities, and neighborhood-serving retail uses. In addition, the Specific Plan is intended to foster a healthy community by improving pedestrian linkages between the Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, Martin Luther King Jr. Medical Center, Charles R. Drew University of Medicine and Science, future mixed use areas, and existing residential neighborhoods; as well as improving the public realm.

## 1.4 Relevant Studies and Plans

The following are recent and ongoing planning projects that have provided direction and established a foundation for the Willowbrook TOD Specific Plan.

- Los Angeles County General Plan Update (2015)
- Martin Luther King Medical Center Campus Master Plan & the Willowbrook MLK Wellness Community Vision (2012)
- Martin Luther King, Jr. Medical Center and Surrounding Project Area ULI Technical Assistance Panel (2009)
- Rosa Parks Metro Station Master Plan & Transit Oriented District (2010)
- Los Angeles County Transit Oriented Districts Access Study, SCAG & County of Los Angeles (2013)
- County of Los Angeles Bicycle Master Plan, County of Los Angeles (2012)
- Implementation Plan for the MLK Medical Campus and the Willowbrook Wellness Community (Initiative for Inner City Competitiveness, Next Street and Genesis LA) (2013)
- Rosa Parks Station Improvements Planning, Metro, 2014-2015
- Imperial-Wilmington Rosa Parks Metro Blue Line Rail Station Non-Motorized Access Plan, City and County of Los Angeles, Metro, April 2009
- Willowbrook Area Access Improvements, County of Los Angeles Department of Public Works, 2015

## 1.5 Community Engagement

Prior to and throughout the planning process for the Willowbrook TOD Specific Plan, County staff from the Department of Regional Planning (DRP) conducted community outreach and participation with the Willowbrook community. They met with and received input from community groups and stakeholders, such as but not limited to:

- Concerned Citizens of Willowbrook and Pastor Glass
- Best Start Watts-Willowbrook
- Watts Century/Latino
- Watts Labor Community Action Committee
- Kimco Realty
- Charles Drew University
- City of Los Angeles
- HACLA
- MLK Community Hospital
- Metro
- Caltrans
- City of Lynwood
- Residential developers (Market Rate and Affordable)
- Commercial developers

In addition, the consultant team met regularly with a County Task Force and received input and feedback about the goals and direction of the Specific Plan.

## 1.6 Specific Plan Goals and Policies

The goals and policies set the framework for the Willowbrook TOD Specific Plan. They serve as guidelines for decision-making and provide direction for future activities. The goals and policies were derived from input received from the community and stakeholders during the community engagement process, County Task Force, and County staff during the planning process. The seven major goals identified for the Specific Plan area are as follows:

1. Preserve and enhance the character of the Willowbrook community.
2. Improve the Willowbrook/Rosa Parks Station and its environs.
3. Encourage transit oriented development.
4. Provide affordable housing opportunities.
5. Promote active transportation and reduce vehicle miles traveled.
6. Improve quality of life for existing residents with improvements to the public realm.
7. Improve economic vitality and employment opportunities.

### **Goal 1: Preserve and enhance the character of the Willowbrook community.**

*Policy 1.1:* Where appropriate, preserve the character of the residential neighborhoods.

*Policy 1.2:* Provide a mix of land uses in the Mixed Use Zones to accommodate employment, retail, and residential uses, as well as local-serving amenities.

*Policy 1.3:* Preserve existing neighborhood-serving retail at Kenneth Hahn Plaza.

*Policy 1.4:* Improve compliance with County zoning and building codes on private property by expanding code enforcement by various County Departments.

*Policy 1.5:* Facilitate the expansion of the MLK Medical Center and Charles Drew University (CDU) campus that is compatible and sensitive to the surrounding neighborhoods.

*Policy 1.6:* Coordinate with CDU to integrate the planned growth of the University's campus with the surrounding community, including creating pedestrian linkages and open space connections with other area institutions and the Willowbrook/Rosa Parks Station.

*Policy 1.7:* Implement the concepts and ideas contained in the *MLK Medical Center Campus Master Plan & the Willowbrook MLK Wellness Community Vision* by encouraging well-designed and continuous pedestrian paths and connections between the Willowbrook/Rosa Parks Station and the employment, campus, retailing, and residential areas.

**Goal 2: Improve the Willowbrook/Rosa Parks Station and its environs.**

*Policy 2.1:* Coordinate with Metro to enhance pedestrian and bicycle connections from the Willowbrook/Rosa Parks Station to the surrounding community.

*Policy 2.2:* Coordinate with Metro to enhance safety and circulation between the various transit modes at the Willowbrook/Rosa Parks Station to encourage transit use.

*Policy 2.3:* Work with Metro and Kenneth Hahn Plaza to create better connections and access to the surrounding employment, campus, retail, and residential areas.

**Goal 3: Encourage transit oriented development.**

*Policy 3.1:* Provide a variety of housing choices within walking distance of the Willowbrook/Rosa Parks Station.

*Policy 3.2:* Implement mixed use zoning in targeted areas to promote employment-generating uses proximate to housing and the Willowbrook/Rosa Parks Station.

*Policy 3.3:* Incentivize lot consolidation where appropriate to facilitate the development of cohesive projects in Mixed Use Zones.

**Goal 4: Provide affordable housing opportunities.**

*Policy 4.1:* Preserve existing stock of affordable housing.

*Policy 4.2:* Promote housing affordability through diversification of housing choices (ownership, rental, single-family, multifamily) for varied income groups.

**Goal 5: Promote active transportation and reduce vehicle miles traveled.**

*Policy 5.1:* Provide a multi-modal transportation system of complete streets.

*Policy 5.2:* Enhance access to transit and the Willowbrook/Rosa Parks Station.

*Policy 5.3:* Provide a connected pedestrian and bicycle network that links together Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, new mixed use areas, CDU campus, MLK Medical Center campus and residential neighborhoods,

*Policy 5.4:* Facilitate mixed use development that maximizes pedestrian connectivity and minimizes the need for vehicle travel.

**Goal 6: Improve quality of life for existing residents with improvements to the public realm.**

*Policy 6.1:* Enhance the public realm with street trees, street furniture, bicycle lanes, sidewalks and pedestrian paths.

*Policy 6.2:* Provide a consistent canopy of shade trees throughout the Specific Plan area to enhance pedestrian comfort.

*Policy 6.3:* Provide pedestrian-scaled lighting to improve safety and enhance pedestrian environment.

*Policy 6.4:* Encourage outdoor dining and seating areas and other pedestrian-friendly uses in mixed-use areas.

*Policy 6.5:* Explore joint use agreements with schools to better utilize existing and future open space resources.

*Policy 6.6:* Encourage new development to provide public open space as a community benefit. Consider providing incentives to developers for such provisions.

*Policy 6.7:* Consider building pocket parks and community gardens on County-owned vacant lots.

**Goal 7: Improve economic vitality and employment opportunities.**

*Policy 7.1:* Create economic opportunities for the Willowbrook community by fostering a complementary variety of employment, retail, residential, and institutional uses.

*Policy 7.2:* Build on the Willowbrook community's economic base as a "healthcare cluster" by working with appropriate partners to provide workforce development opportunities for local residents.

*Policy 7.3:* Facilitate public-private partnerships to share responsibility for implementing this Specific Plan and achieving its goals.

*Policy 7.4:* Encourage a mix of national brand and local merchant businesses.

*Policy 7.5:* Efficiently manage the supply and demand of parking to accommodate customer, commuter, and resident parking, and encourage the use of shared parking where possible.

# Land Use and Urban Design Framework

## 2.1 Introduction

The goal of the Land Use and Urban Design Framework is to develop the Specific Plan area as a system of cohesive and integrated uses, spaces, structures, and environments. A clear framework provides visitors and residents with a strong sense of place, an understanding of how to easily and safely find their way around and to efficiently identify uses and activities. Arrangement of land uses into identifiable subareas, improved streetscapes, connected open spaces, and enhanced landmarks will help make the Specific Plan area accessible and distinctive.

This Chapter describes the approach to land use and urban design that will transform the Specific Plan area into a vibrant transit-oriented district, while strengthening its connections to the adjacent residential neighborhoods and the rest of the Willowbrook community. This chapter also presents the short and long term land use strategies for the various institutions and facilities in the Specific Plan area, which will be important in achieving the community goals established for the Specific Plan area.

## 2.2 Land Use and Urban Design Framework

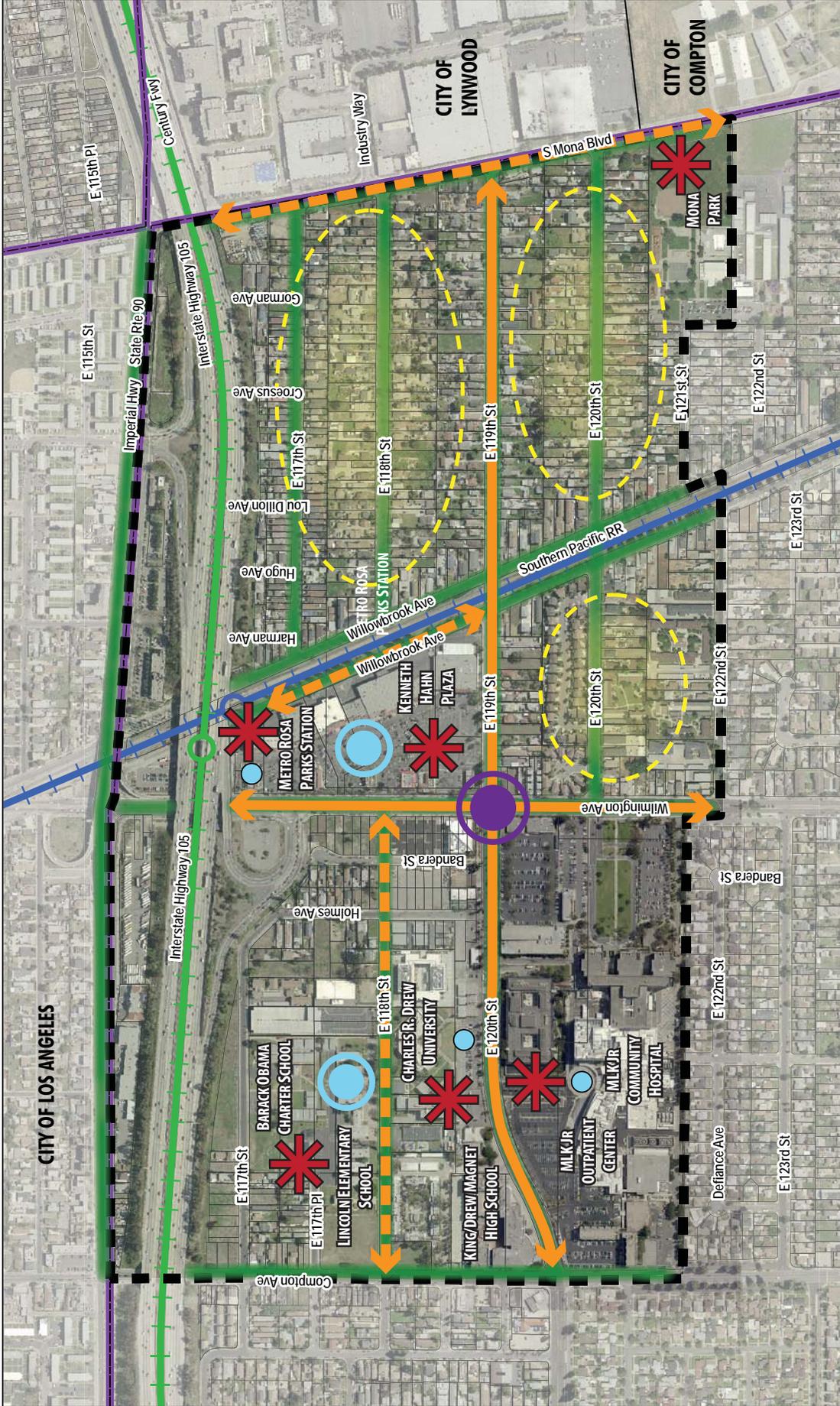
Elements of the Land Use and Urban Design Framework, as illustrated in Figure 2-1, include:

### Specific Plan Subareas

The major activity centers in the Specific Plan area include the Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, Martin Luther King Jr. (MLK) Medical Center Campus, Charles Drew University (CDU) Campus, and Mona Park. Section 2.3 describes the distinct subareas into which the Specific Plan area can be divided. The existing uses in and strategies for planned and future uses in these subareas are also discussed in this chapter.

### Mobility Improvements

Improvements to the vehicular, pedestrian, bicycle and transit network that enhance connectivity and ease of movements for non-auto transportation modes are further described in Chapter 4. Road diets for major pedestrian connections are further described in Chapter 4.



**Figure 2-1  
Urban Design Framework**

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Major Activity Centers
- Entry Plazas
- Future Open Space/Plaza
- Streetscape Improvements
- Primary Pedestrian Connections
- Major Intersection
- Protect and Enhance Residential Neighborhoods
- Secondary Pedestrian Connections

**WILLOWBROOK TOD SPECIFIC PLAN**



### **Pedestrian and Bicycle Connections**

The Urban Design Framework establishes enhanced pedestrian and bicycle connections along the street network between the Willowbrook/Rosa Parks Station and the activity centers. Additional bicycle facilities, such as a Class I bicycle path on Mona Boulevard, to complete the bicycle network with the Specific Plan area are also described in Chapter 4. Enhancements to the pedestrian experience such as pedestrian-oriented intersection improvements and wider sidewalks where feasible, are also included.

### **Major Intersection – Wilmington Avenue and E. 120th/E. 119th Street**

The two major streets within the Specific Plan area are Wilmington Avenue in the north-south direction and E. 120th/E. 119th Street in the east-west direction. These two streets connect the major activity centers to each other, as well as connect across the rail tracks with the residential neighborhoods to the east. They also intersect to form the pedestrian core or “heart” of the Specific Plan Area. Design concepts for special paving treatments at this intersection are described in Chapter 5.

### **Streetscape and Open Spaces**

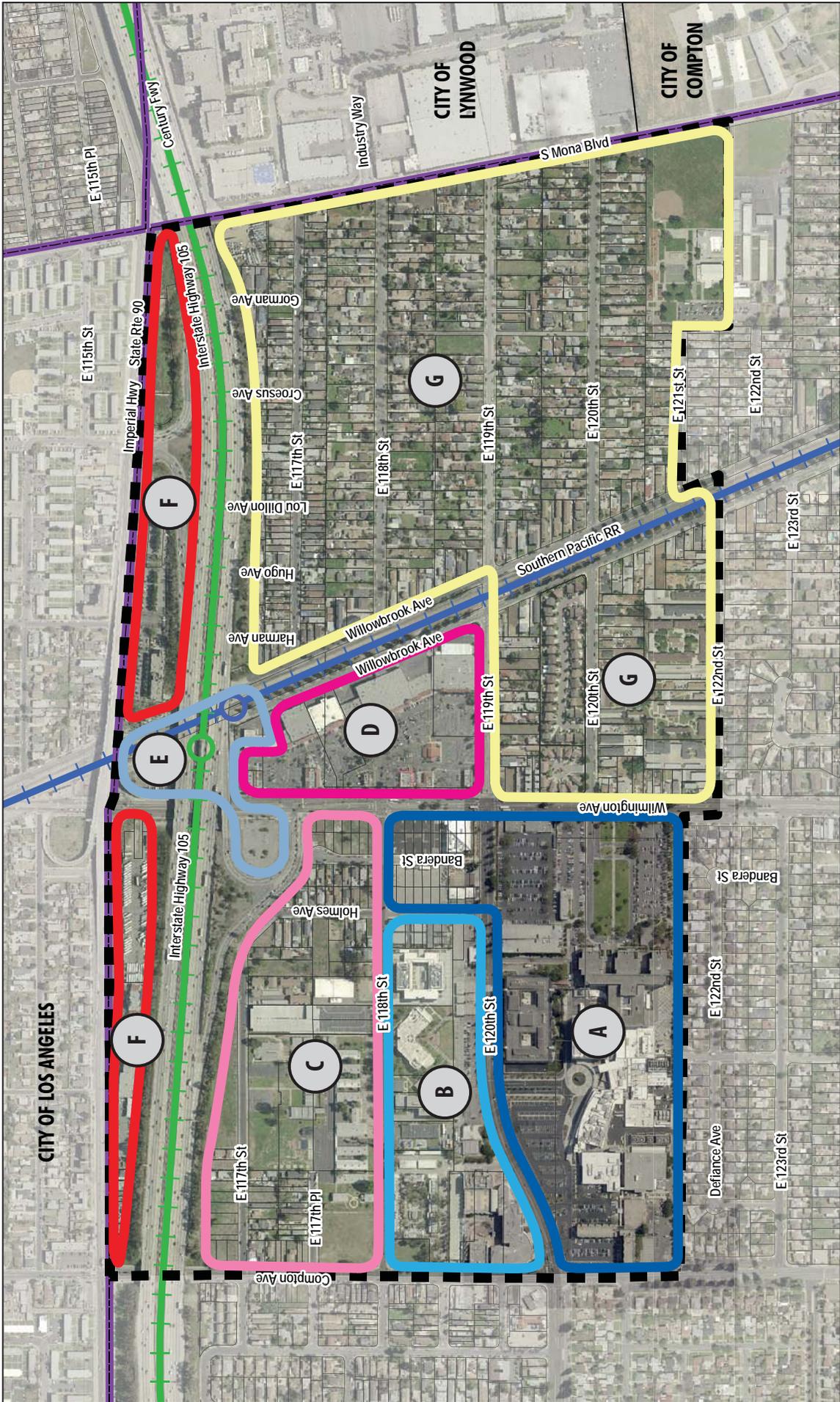
Another important element of the Urban Design Framework is improved streetscapes and connected open spaces. These streetscape improvements are intended to enhance and unify the visual and spatial experience of the driver, pedestrian, and the bicyclist, and help provide key linkages between the activity centers and the residential neighborhoods in the Specific Plan area. Recommended streetscape improvements consist of an interrelated palette of street trees, street furniture, street lighting, wayfinding signage and landscaping. These elements are described in Chapter 5.

Existing and planned open space resources in the Specific Plan area include Mona Park located along Mona Boulevard in the southeast corner of the Specific Plan area. Faith and Hope Park is planned for a mid-block location along E. 119th Street. Green alley improvements are also planned for the alley that runs adjacent to this park. Another important open space resource is the Fitness Garden, primarily located along 120th Street at the MLK Jr. Center for Public Health.

The Specific Plan encourages the creation of additional public open space as a part of new or expanded mixed use developments in the Specific Plan area.

### **Residential Neighborhoods**

The preservation and enhancement of the existing residential neighborhoods is an important element of the Urban Design Framework. Key strategies include maintaining the density and character of the neighborhoods (Chapters 2 & 3), improving safety and comfort for navigating the street network (Chapter 4), and improving connections to the activity centers in the Specific Plan area (Chapter 5).



**Figure 2-2  
Specific Plan Subareas**

**WILLOWBROOK TOD SPECIFIC PLAN**

0 325 650 1,300 1,500 Feet  
5 minute walk (1,200')

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- MLK Medical Center and Associated Facilities
- Charles Drew University
- Northwest Subarea
- Kenneth Hahn Plaza
- Metro Station
- Imperial Highway Corridor
- Residential Neighborhoods

## 2.3 Specific Plan Subareas

The Specific Plan area supports a range of land uses, including residential, retail, office, and other commercial, as well as educational and institutional facilities and services. It can be divided into the following seven subareas as shown in Figure 2-2:

- Martin Luther King Jr. (MLK) Medical Center campus and associated facilities
- Charles Drew University
- Northwest Subarea
- Kenneth Hahn Plaza
- Willowbrook/Rosa Parks Station
- Imperial Highway Corridor
- Residential Neighborhoods

The uses in these subareas are complementary and supportive of each other. These subareas connect through the street network and a system of pedestrian walkways and open space areas. This section describes the existing conditions and the strategies for planned and future development within each subarea.

### A. MLK Medical Center Campus and Associated Facilities

#### Existing Conditions

The Martin Luther King, Jr. Community Hospital (MLKCH) reopened in 2015 after being closed in 2007. It is located on approximately 38 acres of land, and the campus is bounded by Wilmington Avenue to the east, E. 120th Street to the north, Compton Avenue to the west, and the Specific Plan boundary to the south. The hospital is expected to serve 1.2 million residents in South Los Angeles, including Compton, Inglewood, Watts, Willowbrook and Lynwood. It is also expected to create more than 1,800 jobs in the area with approximately 900 directly at the hospital<sup>1</sup>.



*Martin Luther King Jr. Community Hospital (MLKCH)*



*Multi-Service Ambulatory Care Center (MACC)*

Several other facilities serving the healthcare needs of the community are located adjacent to the hospital, including the MLK Center for Public Health (MLKCPH), which focuses on preventative care, and the Martin Luther King, Jr. Outpatient Center, both operated by the County. The Outpatient Center operates over 70 primary care and specialty care clinics to serve the health care needs of surrounding

<sup>1</sup> *Martin Luther King, Jr. Community Hospital website*

communities. The MLKCPH includes a Fitness Garden, which is primarily located along the 120th Street frontage, and incorporates a walking path and par course fitness equipment for employees and the public.



*Martin Luther King Center for Public Health*



*Fitness Garden*

The County seeks to establish MLK Medical Center Campus as a center of excellence for health care delivery, urban health promotion and prevention, health workforce development, academic research and teaching, and economic development<sup>2</sup>. In 2009, the County commissioned the preparation of an EIR that analyzed the potential for significant environmental impacts in association with the proposed MLK Medical Center Campus Redevelopment Project (MLK EIR). The MLK EIR analyzes two tiers: Tier I,<sup>3</sup> project-level development, and Tier II,<sup>4</sup> program-level development. In addition, in 2009, the County Board of Supervisors approved a Categorical Exemption that allows for the repurposing of the existing facilities for hospital use without CEQA review.

Following the certification of the EIR, the County approved the *MLK Medical Center Campus Master Plan & The Willowbrook MLK Wellness Community Vision* document in 2012 as a guiding framework for future development. The Plan calls for a coordinated system of pedestrian and bicycle circulation within the campus and the surrounding area.

Tier I development, as described in the MLK EIR, has been completed. As part of Tier II, a 50,000 square feet medical office building and a 1,400 car parking garage are also in the planning and design stages. Other projects in the pipeline include expansion/construction of two clinics focused on providing services for children. The Los Angeles County Department of Children and Family Services (DCFS) operates the HUB Clinic, which provides mental health services to children and families who receive

<sup>2</sup> *Martin Luther King Jr. Medical Center Campus Redevelopment Draft EIR, August 2010*

<sup>3</sup> *Tier I entailed vacation of the emergency room, storage buildings, cooling towers, and existing MACC, and the development of two new environmentally sustainable buildings: the 132,000-square-foot new MACC and the 24,700-square-foot Ancillary Building. Tier I development consisted of approximately 170,332 square feet of new development and the vacation of approximately 509,018 square feet. In addition, tenant improvements in existing buildings, site improvements, and potential relocation of the MRI Building occurred in Tier I.*

<sup>4</sup> *Tier II would entail the development of a campus-wide Master Plan. It is anticipated that the development described in the Master Plan would seek to prepare the proposed project site for future mixed-use campus-related development that would provide the health services necessary to respond to and address the needs of the community. Tier II would have the potential to build out approximately 1,814,696 square feet of development on the proposed project site with mixed uses including medical office, commercial, retail, office space, recreation, and other development in support of the campus. The net new development of the proposed project would be approximately 1,476,010 square feet. Tier II would also entail the construction of up to 100 residential units, to be developed at a multi-family density consistent with surrounding residential area multi-family development densities. In addition, the Tier II components would entail the reuse or replacement of the existing MACC building, emergency room, storage building, and cooling towers.*

services through DCFS. Along with the Children's Behavioral Clinic, the two clinics will be sited directly to the north of the MLK Center for Public Health building, replacing the surface parking lot. Parking will be relocated in the block to the west.

### Specific Plan Strategy

The Specific Plan envisions that the MLK Jr. Medical Center campus will continue to grow and thrive as a regional destination for medical, health and associated services. The development on the campus will be guided by the *MLK Medical Center Master Plan and the Willowbrook MLK Wellness Community Vision*. Required parking for the medical uses will be provided on campus. The Specific Plan also builds upon the recommendations for enhanced pedestrian connections between the MLK Medical Center campus and the other activity areas in the Specific Plan area.



*MLK Medical Center Campus Master Plan & The Willowbrook MLK Wellness Community Vision (Source: Gensler).*

## B. Charles Drew University Campus Area

### Existing Conditions

Immediately north of the MLK Jr. Medical Center campus are Charles R. Drew University of Medicine and Science (CDU), a private university, and King/Drew Magnet High School, part of the Los Angeles Unified School District. These institutions are bounded by Holmes Avenue to the east, Compton Avenue to the west, 120th Street to the south and 118th Street to the north.

CDU has been in the Willowbrook community since the 1960s and is recognized by the Department of Education under Title III B as a Historically Black Graduate Institution. CDU provides training in medicine, medical technology, public health and nursing under the auspices of three schools: College of Medicine, College of Science and Health, and Mervyn M. Dymally School of Nursing.

Hospital occupations have been the focus of King/Drew High School’s curriculum since its establishment in the early eighties. The Medicine and Science Careers Program, the centerpiece of school’s offerings, provides an opportunity for students to access experience-based learning in environments including hospitals, clinics, research laboratories, and university facilities. The school is located on the northeast corner of Compton Avenue and 120th Street.

Other uses located within this area include a multi-family project on CDU-owned land on E. 118th Street, several surface parking lots that serve CDU, and other County facilities in the area that are located along E. 120th Street.



*Charles Drew Medical University and LAUSD King/Drew Magnet Highschool are anchors in this subarea.*

### **Specific Plan Strategy**

The Specific Plan strategy is to support CDU’s long range planning efforts. CDU is preparing a master plan (20-25 years) that plans for an increase in its student population from 625 to 1450. Today, CDU is primarily a commuter campus. The master plan includes new residential facilities (119 units) within the campus. The proposed housing types include dormitories for undergraduate students, shared housing for graduate student, and housing for visiting faculty. The master plan also provides conference facilities, incubator space, research labs and additional student classrooms and facilities. The growth of the CDU campus will contribute additional buying power of the campus community (faculty, students, workers, etc.).

With MLK Community Hospital, MLK Center for Public Health, and other health-related facilities in the area, CDU and King Drew High School play an important role in fostering Willowbrook as a “health focused cluster.” The Specific Plan reinforces the importance of these two facilities and builds upon the recommendations of the CDU master plan.

CDU’s master plan aims to unify the campus into a cohesive setting with the creation of quad spaces and new buildings. The planned buildings are taller – four to six stories in height. The master plan expands the parking structure and parking lot located on 118th Street to provide all required parking for the campus.

The Specific Plan requires new campus buildings to be set back from the adjoining streets. In order to be responsive to the scale of the residential uses across Compton Avenue, new buildings along Compton Avenue are limited to four stories in height and set further away from the Compton Avenue property line. Architects/designers for CDU will need to consider architectural cohesiveness in terms of materials and style as new buildings are designed and built. The quad spaces and the required yards will need to be extensively landscaped.

The CDU master plan also includes a pedestrian, bicycle and shuttle circulation network system that connects with the activity centers in the Specific Plan area. Campus pedestrian and bicycle paths are connected with the public street network and activity centers in the vicinity, including the hospital to the south, and Willowbrook/Rosa Parks Station and Kenneth Hahn Plaza to the east. The CDU shuttle system routes also connect the activity centers noted above. There is an opportunity to consolidate the various shuttles in the area to improve shuttle routes and service frequency.

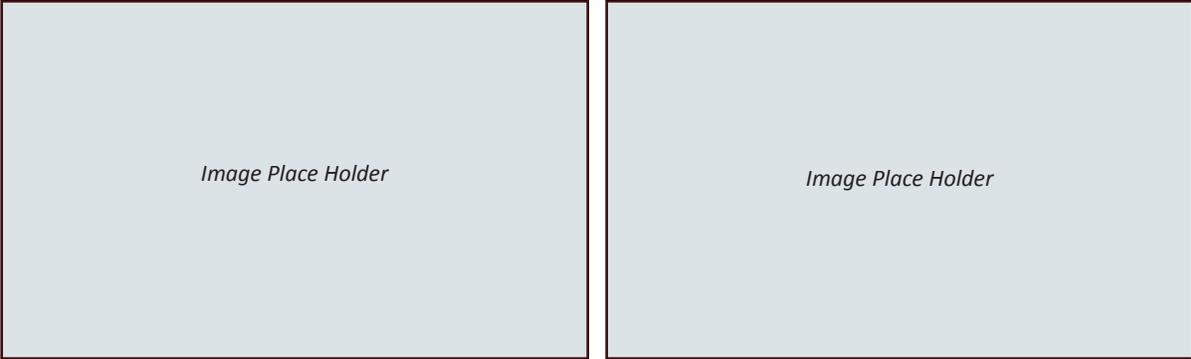
## C. Northwest Subarea

### Existing Conditions

The northwest subarea is an aggregation of a variety of uses – educational, retail, residential and institutional. Several vacant lots, owned by the Los Angeles Community Development Corporation (LACDC), are located along E. 117th Street. A large vacant site on the northeast corner of E. 118th Street and Compton Avenue is owned by the Compton Unified School District. Educational uses include Lincoln-Drew Elementary, a part of the Compton Unified School District (CUSD), and Barack Obama Charter School. Both are located north of E. 118th Street. Parking facilities for CDU are located adjacent to the school. Other uses in this subarea include a water well and reservoir operated by the Park Water Company. The residential uses are a mix of single family homes, duplexes and multifamily structures. The block between Wilmington Avenue and Bandera Street, north of E. 118th Street, is the site for an affordable senior housing project. The relocated Willowbrook Public Library will be located on the ground floor of this development.



Two elementary schools, CDU parking and a corner market are found in this subarea.



Wilmington + 118th Street Senior Housing and Library

**Specific Plan Strategy**

The northwest subarea has an advantageous location proximate to MLK Jr. Medical Center and CDU to the south and Willowbrook/Rosa Parks Station and Kenneth Hahn Plaza to the east. The Specific Plan envisions that over time, this subarea will capitalize on this proximity and become a mixed use district with employment generating uses and residential infill.

The Work Force Collaborative, among other groups and studies, has suggested that the Willowbrook community become a “health cluster” with MLK Jr. Medical Center campus and CDU as the anchors, leading to numerous opportunities for health related occupations at the two facilities. There is also opportunity for growth in related health fields. The Specific Plan encourages employment generating uses such as medical back office, laboratory facilities, hospital equipment facility, medical or university support businesses, etc. that provide job opportunities for both local residents and others. The Specific Plan also envisions residential infill that provides housing options for current and future workers, students, and residents in the area.

In order to achieve this long term vision, it is anticipated that the Specific Plan will facilitate the transition to more intensive uses. However, several uses within this subarea will need to remain or be re-accommodated. The CDU garage site will continue to serve the parking needs of the CDU campus. The existing single-family residential uses within the subarea will continue as legal non-conforming uses until they transition to other uses.

**Lincoln Elementary and Barack Obama Charter School**

The two school sites could be consolidated and reconfigured for more efficient use of the land. The California Department of Education Guide to School Site Analysis states that infill school sites can be much smaller than the previously typical recommended elementary school site size of 10 acres in urban areas. With the available CDC-owned and CUSD-owned properties in the subarea, a 15-acre development parcel could be assembled with minimal acquisitions (assuming a portion of the E. 117th Street right of way is vacated – see discussion below). A five to six acre portion of the assembled development parcel would be reserved for new school construction, with the rest of the parcel sold for development. The proceeds from the sale could be used to fund the reconstruction of the elementary and charter schools.

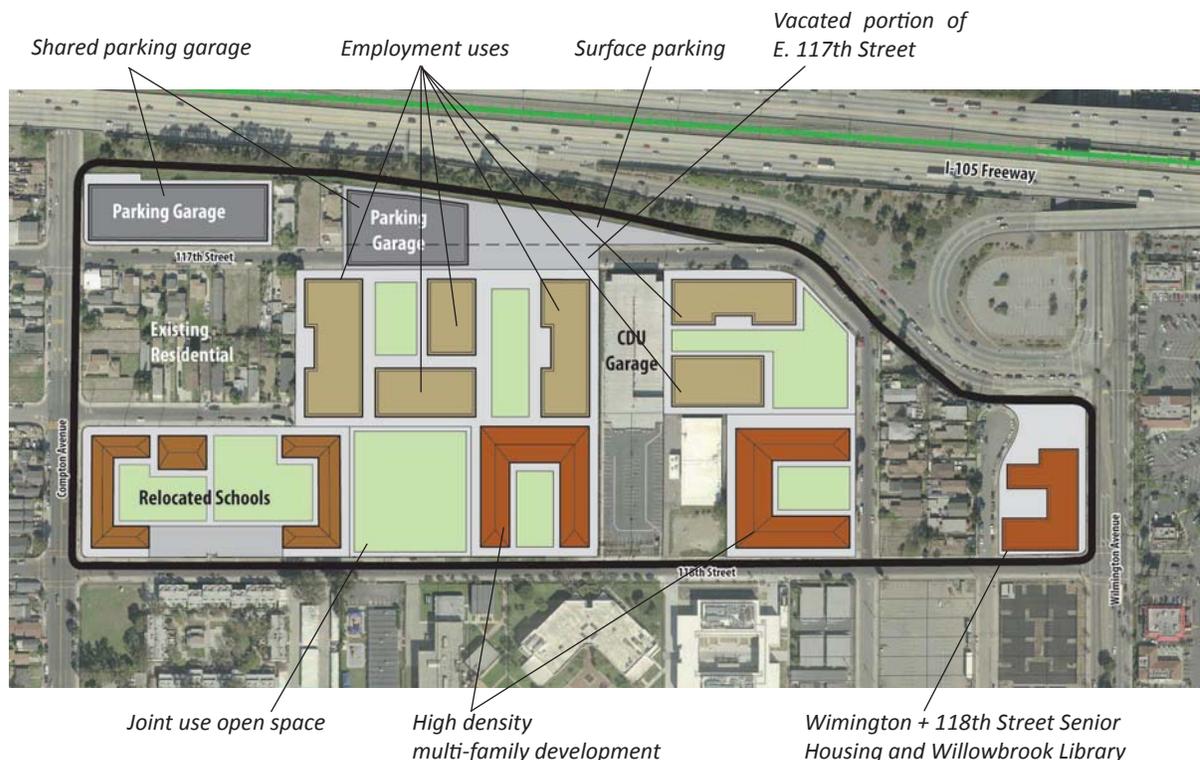
Commercial users would likely include corporations looking to associate with the new MLK Jr. Medical Center campus or CDU, and the parcel could be an ideal “back office” administrative location. The

parcel is large enough to accommodate a business park, which could attract a variety of healthcare related office uses, or function as a regional administrative facility for a healthcare provider. A portion of this parcel could also be allocated to a healthcare/wellness incubator campus.

The development parcel would also be very attractive to several types of residential uses. The development parcel's location near CDU makes it an attractive location for university housing and its proximity to the Willowbrook/Rosa Parks Station makes it a prime location for transit oriented development. Its location is also ideal for a sizeable workforce housing development.

If the CDC-owned and CUSD-owned properties are aggregated, there is an opportunity to create an even larger consolidated development site by vacating a portion of E. 117th Street that is west of the CDU garage site. It should be noted that any such vacation may only happen as long as there is no impact whatsoever on access to any of the adjacent properties. Any new large scale development should provide a public open space for the community.

This subarea will benefit from parking reductions in the Specific Plan. There is also the opportunity to explore the creation of a parking management district within the subarea. The CDC-owned properties between E. 117th Street and the 105 Freeway can be the location for parking, while at the same time serving the additional purpose of providing a buffer between freeway nuisance and new development.



**Figure 2-3: Illustrative Site Plan for the Northwest Subarea**

*This site plan shows one way the Northwest subarea could be developed under the development regulations and standards of the Specific Plan. Through lot consolidation and development of a unified project at higher densities, this subarea offers the potential for employment generating district with new residential opportunities. This plan is illustrative only; it is not intended to prescribe this particular site layout or use.*



*The Northwest subarea is envisioned to accommodate employment generating uses in office and/or business park settings. Multi-family developments are anticipated to provide housing options for workforce, students and transit-dependent users.*

## D. Kenneth Hahn Plaza

### Existing Conditions

Kenneth Hahn Plaza is a privately owned shopping center on County-owned land. It is located immediately south of the Willowbrook/Rosa Parks Station, and is bounded by Wilmington Avenue to the west, 119th Street to the south and Willowbrook Avenue to the east. The major anchor tenant is Food for Less, a grocery store. Other tenants include Rite-Aid, General Discount and DaVita Dialysis Center. Food establishments in the center include McDonald's, Taco Bell, Pizza Hut, and Denny's, which is the only sit-down restaurant in the area. The Plaza is also the site of the Willowbrook Library (soon to be relocated) and a Los Angeles County Sheriff substation.

The shopping center has a suburban site layout with buildings sited at the rear of the site and parking in front. Most of the fast food restaurants are located in smaller structures adjacent to Wilmington Avenue. The shopping center presents an uninviting and blank facade towards 119th Street, which has mostly single family uses along the south side. The loading areas are located along Willowbrook Avenue. An internal service road between the buildings and rear property line provides access for trucks and other service vehicles. The entire site is surrounded by a six-foot tall wrought iron security fence blocking a pedestrian connection between the Willowbrook/Rosa Parks Station and the shopping center.



*Kenneth Hahn Plaza currently has neighborhood serving retail as anchors. It is also isolated from its surroundings by fencing.*

The Kenneth Hahn Plaza site is owned by the Los Angeles County Community Development Corporation (CDC). Metro is acquiring approximately 1.5 acres of land on the northern end of the site for the expansion of the Willowbrook/Rosa Parks Station. The rest of the site is being purchased by the shopping center operator, Kimco.

### Specific Plan Strategy

The Kenneth Hahn Plaza site is the primary retail location within the Willowbrook TOD Specific Plan area. While the site is a prime opportunity for redevelopment as a TOD site, there is also strong community desire and a need to retain a grocery store and other neighborhood serving retail in this location. The Specific Plan sets forth the long term and short term goals for this area.

#### Long Term

Over the long term, the Specific Plan envisions the Kenneth Hahn Plaza will transform to a mixed use TOD project that takes advantage of the proximity of the Willowbrook/Rosa Parks Station. The MLK Medical Center Campus Master Plan, ULI Technical Advisory Panel study, and Rosa Park Station Study, all have also envisioned a mixed use TOD project on the site. The Specific Plan envisages that this site provides an integrated, complementary residential and commercial development with a continued emphasis on neighborhood serving retail, restaurant and service uses to serve local Willowbrook residents, employees, pedestrians and consumers. Development can be integrated vertically, with residential or office uses above retail uses, or horizontally on site.



*Over the long term, new mixed use buildings with neighborhood serving retail on the ground floor could transform Kenneth Hahn Plaza to a vibrant transit-oriented development.*

Densities allowed per this Specific Plan are appropriate for both vertical and horizontal mixed use projects. Buildings may be up to four stories tall. Residential uses will need to be designed to minimize the noise and vibration impacts of the rail traffic along Willowbrook Avenue. Parking may either be in consolidated lots, under podiums or in structures.

East 119th Street is the primary connection with the residential neighborhoods across the rail tracks within the Specific Plan area. As such, it is an extremely important connection that needs to be improved in terms of appearance, safety, visibility and amenities. Infill uses along East 119th Street will create a more active street, at least on the north side. These uses could either be storefronts with retail or service uses (child care, senior care, attorney offices etc.) or residential town-home/walk-ups facing the single family residential uses on the south side of the street. The massing and scale of these buildings will be lower than the rest of the site to respond to that of the single family residential uses on the south side of East 119th Street.



*Retail or service uses will activate the north side of East 119th Street, the primary east-west connection in the Specific Plan area.*



*New retail infill along Wilmington Avenue should activate the Wilmington frontage.*

As private development occurs on the Kenneth Hahn Plaza along the north side of E. 119th Street, buildings will be required to be set back from the E. 119th Street property line. This segment of E. 119th Street is an important pedestrian travel corridor and is a connection to the residential neighborhoods to the east. This setback area could become a part of the expanded sidewalk.

As a part of the site’s redevelopment, a large outdoor plaza or gathering space should be created, similar to other successful shopping centers in the region. Such a space should have amenities such as street furniture, landscaped open space, public art, a water feature, and concessions.



*The redevelopment of Kenneth Hahn Plaza should include a large outdoor plaza or gathering space.*

The site also has the potential for retail infill along Wilmington Avenue while maintaining views to the existing retail on the site. Additional massing along the Wilmington frontage will also help create a more pedestrian friendly environment along that street.

**Short Term**

In the short term, it is likely that the Plaza will continue as a shopping center that provides neighborhood-serving retail and service uses.

A better mix of retail tenants is needed to serve the shopping needs of Willowbrook area residents, employees of and visitors to the newly opened MLK Community Hospital and other existing facilities in the Specific Plan area. Active tenant recruitment will be necessary to improve the tenant mix at the Plaza.

Physical upgrades and improvements that can improve the environment in the Plaza include enhanced landscaping in the parking lot and along the periphery of the shopping center. Landscaping can also help create clearly identifiable paths of travel for pedestrians to navigate around the shopping center and minimize conflicts between the drive-thru lane entrances and pedestrian travel. The main entry drive on Wilmington Avenue should be improved with sidewalks on both sides of the driveway, better landscaping, and marked crosswalks for pedestrian safety.

Consideration should be given to the removal of the perimeter fence to better integrate the Plaza with its surrounding uses. At a minimum, pedestrian access to the Willowbrook/Rosa Parks Station should

be restored by removing the fence between the two facilities. Other improvements include better lighting in the parking lot, and improved pedestrian linkages with the surrounding activity centers, such as the hospital, CDU, mixed use areas, as well as the residential neighborhoods.

## E. Willowbrook/Rosa Parks Station

### Existing Conditions

The Specific Plan area has many options for public transportation, including bus service and the Willowbrook/Rosa Parks Station, which is a major transfer point between the Metro Blue Line and Green Line light rail lines. The Green Line station is located in the median of the I-105 Freeway (Glenn Anderson) and the Blue Line station is at grade, one level below. In addition to the two rail lines, the Willowbrook/Rosa Parks Station is served by six Metro bus lines, as well as several municipal bus lines and community shuttles.

A Metro public parking lot is located north of the I-105 freeway at Wilmington Avenue. Another Metro park-and-ride lot is located across Wilmington Avenue, within the off-ramp loop.

The physical environment at the Willowbrook/Rosa Parks station is in need of major improvements. It is poorly lit as well as difficult to access and navigate, and it is poorly connected to its surrounding environment. The Rosa Parks station is blocked off from Kenneth Hahn Plaza by a fence, and access to the residential neighborhoods to the east of the tracks is limited. This station also has the one of the highest crime rates in the Metro system.



*The Metro Rosa Parks Station environment is inhospitable and unattractive.*

Metro is working on the Willowbrook/Rosa Parks Station Improvement Project that will solve many of the above identified issues. The design, still preliminary at the time of writing this Plan, includes relocating the Blue Line platform south, restructuring the entry area for automobiles, buses and pedestrians, and creating better connections for all modes of transportation. Acquisition of approximately 1.5 acres of the Kenneth Hahn Plaza is a part of this plan. The following improvements are included:

- Extend the Blue Line platform to reduce crowding;
- Create a new southern at-grade pedestrian crossing and entrance to the extended Blue Line platform;
- Upgrade the elevators, escalator and stairs to the Metro Blue and Green Line platforms;

- Improve the East Bus Depot to create a safer and more comfortable area for patrons;
- Reconfigure the West Bus Depot to relocate the bays from underneath the freeway;
- Reconfigure both Park & Ride lots for optimal circulation and pedestrian safety;
- Improve pedestrian circulation through the Station by creating a new Transit Hall;
- Add a new Sheriff's facility;
- Add a new Mobility Hub, including a Bike Hub;
- Add a new Metro Customer Service Center;
- Add new canopies on the extended Blue Line platform and at the open-air bus bays; and
- Improve lighting, landscaping and signage throughout the site.

As of September 2014, the total cost for these improvements is approximately \$53 million.



*These images illustrate the reconceived entry plaza and entrance booths for the Metro Rosa Parks Station upgrades. (Source: Metro)*

### Specific Plan Strategy

The Specific Plan supports the improvements planned at the Willowbrook/Rosa Parks Station. Several of the improvements planned at the Willowbrook/Rosa Parks Station are critical to improving pedestrian access and connectivity in the Specific Plan area. However, additional improvements are necessary to meet the goals of the Specific Plan.

The rail ROW is a barrier between the eastern neighborhoods and institutional uses to the west. Improvements to the at-grade crossing at the north end of the Blue Line platform will greatly enhance pedestrian connectivity to the residential neighborhoods to the east of the rail tracks. Another at-grade crossing is planned at the south end of the Blue Line platform, but only across the western tracks. If this crossing could extend eastward across the tracks, it would further enhance connectivity for residents to the east.

Improvements to the intersection of Wilmington Avenue and the I-105 Freeway off-ramps are also necessary to improve access across Wilmington Avenue and to the Metro parking lot that is located in the ramp loop. These are included in the improvements described in Chapter 4 of the Specific Plan.

Another key improvement in this Specific Plan is enhanced bicycle access to the station – a Class I bicycle path or Class IV cycle track is planned along Willowbrook Avenue West between Imperial Highway and E. 119th Street, completing a missing connection within the bicycle network in the Specific Plan area.

## F. Imperial Highway Corridor

### Existing Conditions

Imperial Highway is one of the busiest east-west streets in Los Angeles County. It is generally three lanes in each direction within the Specific Plan area and is also grade separated through the central portion across Wilmington Avenue. Frontage access roads provide access to some of the properties. The streetscape environment is generally uninviting to pedestrians.

The uses along Imperial Highway are generally a mix of auto repair, retail, residential, Metro facilities, and underutilized and vacant lots. A school bus parking lot and a Metro maintenance yard are located to the west of Wilmington Avenue. A barber shop and auto shop (towing) are located further west, near Compton Avenue. A couple of new homes have been built recently in this section as well. Other Metro facilities, including a parking lot, are located to the east of Wilmington Avenue. A vacant site owned by the Housing Authority for the City of Los Angeles (HACLA) is also located along this segment of Imperial Highway. On the north side of Imperial Highway is Imperial Courts, a public housing project. While Imperial Courts is not in the Specific Plan area, there is interest in creating a pedestrian connection across the street to the HACLA site.

### Specific Plan Strategy

The parcels within the Specific Plan area along Imperial Highway are sandwiched between the street and the I-105 Freeway. This corridor is suited to less intensive, commercial and service uses. Some of the existing uses, such as Metro maintenance yards and parking facilities, fall within this category. Other similar uses, such as self-service public storage facilities, are permitted by the Specific Plan. HACLA has tentative plans to use their vacant parcels to house a WorkSource center, as well as temporary staging space during Metro's station improvements. Another use under consideration is to house a communications equipment building for high-speed Internet service for the City of Los Angeles.

While new residential uses along this corridor are prohibited, existing residential uses will be allowed to continue as non-conforming uses.



*Low intensity commercial and service uses are appropriate for the Imperial Highway corridor.*

## G. Residential Neighborhoods

### Existing Conditions

Residential uses within the Specific Plan area are a mix of single family, two-family units and apartments, condominiums and townhouses. The residential area south of Kenneth Hahn Plaza, east of Wilmington Avenue and west of Willowbrook Avenue is primarily multi-family, with a mix of apartments and detached town homes. Most of these multi-family developments are two stories in height.

The area bounded by Mona Boulevard, I-105 Freeway, Willowbrook Avenue and 121st Street has mostly single-family uses, with the exception of the Willowbrook Avenue East frontage along the Metro Blue Line tracks. The latter is primarily multi-family uses. The single family homes are predominantly single-story with an occasional two-story portion. The homes are fenced along the front property line. Many of the homes have landscaped front yards. Parking and property upkeep are identified problems, with vehicles often parked in the front setback and on sidewalks.



*Single family uses are the basis of the stable residential neighborhood in the Willowbrook community.*

Residential parcel configurations vary dramatically across the Specific Plan area. In a majority of the residential blocks, parcels are 90 feet wide and over 200 feet deep. However, some of the parcels are as narrow as 30 feet wide and approximately 100 feet deep. Many of the larger parcels have two (or more) units constructed on them, some illegally.

Some of the other issues identified for the residential areas include:

- Issues related to second units include height restrictions and parking.
- Illegal conversions/construction.
- Lack of street amenities.
- The Willowbrook Community Standards District (CSD) has several restrictions in place that need to be addressed.
- This subarea includes an industrially zoned property at the northeastern corner of the residential neighborhoods on Mona Boulevard and I-105 Freeway. The site is just over an acre in size and is approximately 225 feet in depth. The current use is a commercial vehicle parking lot. The community has indicated a desire for better aesthetic screening and long-term conversion to residential use.



Several important community churches are located in the Specific Plan area.

### Specific Plan Strategy

The Specific Plan preserves and enhances existing residential uses in this area. In keeping with the community's desires, the Specific Plan does not increase base residential densities. However, the Specific Plan increases options for adding additional living quarters as living suites or relaxing the requirements for constructing second units.

#### Living Suites

A "living suite" is a portion of a residence that provides flexible living arrangements for the household which occupies the residence. A living suite typically includes a living room with a kitchenette (microwave and refrigerator but no stove), a bathroom, and one or more bedrooms. A living suite includes an interior connection (typically a door) to the remainder of the residence and may also have a separate exterior entrance. Living suites are well-suited to multi-generational households and provide an additional housing resource for families. Living suites will be permitted in the Specific Plan. (See Chapter 3)

#### Industrial Property on Mona Boulevard

As described earlier, the only industrially zoned property in the Specific Plan area is currently being used as a truck parking lot, which is an incompatible use in a single and two-family residential neighborhood. The Specific Plan rezones this property as residential in keeping with its surroundings.

The current use may continue as a non-conforming use on the site per the provisions for non-conforming uses in Title 22 of the County Code and Chapter 3 of this Specific Plan.

#### Residential Street Improvements

The Specific Plan lays out the street improvements for the residential neighborhoods including a street tree program, pedestrian-scaled street lighting, and other street amenities. The plan also improves pedestrian and bicycle connectivity for the residents to the major activity centers in the Specific Plan area.

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# Specific Plan Zones

## 3.1 Introduction

This Chapter establishes the zones, use regulations, development and design standards for the Willowbrook Transit Oriented District (TOD) Specific Plan area. The zones with accompanying use regulations, development and design standards are intended to provide property owners, business owners, developers, and their designers with basic development and design criteria that are intended to reinforce the desired building and district character.

This Chapter shall be used in conjunction with all other County of Los Angeles relevant adopted plans and ordinances, including:

- Los Angeles County 2035 General Plan
- Title 22 of the Los Angeles County Municipal Code (Title 22)

## 3.2 Applicability

Per California State law, specific plans must be internally consistent with the jurisdiction's general plan. The County General Plan regulates the land uses in the Willowbrook TOD Specific Plan area.

The provisions of this Specific Plan shall apply to all properties included in the Willowbrook TOD Specific Plan area. No construction, modification, addition, placement or installation of any building or structure shall occur, nor shall any new use commence on any lot, on or after the effective date of this Specific Plan, except in conformity with the provisions of this Specific Plan and except as noted in section 3.2A below.

### A. Applications filed prior to Specific Plan Adoption

The following provisions shall apply to complete applications filed prior to the effective date of this Specific Plan and accompanying General Plan amendment.

The applicant may choose whether the application will be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan or with this Specific Plan and accompanying General Plan amendment. In either case, approval of the application is not guaranteed.

If the applicant chooses to have the application be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan, the application may be modified prior to consideration by the Los Angeles County Board of Supervisors, the Regional Planning Commission, the Hearing Officer or the Director, and still be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan as long as the requested modification does not:

- Change the project's housing type (e.g., from single-family to two-family or multi-family);
- Increase the project's residential density;
- Increase the project's floor area or lot coverage for non-residential space;
- Increase the amount of grading for the project; or
- Increase the area of ground disturbance resulting from the project.

Such a modification may necessitate submittal by the applicant of revised, updated, or additional materials, including, but not limited to, site plans, elevations, and oak tree reports. If the requested modification does not meet all of the criteria listed above, the modified project shall be reviewed for consistency with this Specific Plan and accompanying General Plan amendments.

An application for a modification to an approved but not used permit, that is valid on the effective date of this Specific Plan and accompanying General Plan amendment, where the modification will result in a project that substantially conforms with the previously approved project, as determined by the Director, may, at the election of the applicant, be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, an application for a modification to such permit shall be considered a new application and shall be reviewed for consistency with this Specific Plan and accompanying General Plan amendment.

If an approved permit has been used prior to the effective date of this Specific Plan and accompanying General Plan amendment and the permit contains a grant term, the approved use may continue until the end of the grant term, and, at the end of the grant term, shall be subject to the Specific Plan and General Plan policies in effect at that time. If, during the grant term, a request for a modification to the previously approved permit is made, and the modification will result in a use that substantially conforms with the previously approved permit, as determined by the Director, the modified permit shall be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, a request for a modification to the previously approved permit shall be subject to the Specific Plan and General Plan policies in effect at the time of filing the application for the modification.

If an approved permit has been used prior to the effective date of this Specific Plan and accompanying General Plan Amendment, and the permit does not have a grant term, the approved use may continue indefinitely without regard to the policies in this Specific Plan and accompanying General Plan Amendment. Notwithstanding the preceding sentence, all applicable non-conforming use provisions of the Zoning Code shall apply to the previously approved permit. If a request for a modification to the previously approved permit is made, and the modification will result in a use that substantially conforms with the previously approved permit, as determined by the Director, the modified permit shall be reviewed for consistency with the land use designations and zoning regulations in effect prior to the adoption of this Specific Plan. In all other cases, a request for a modification to the previously approved permit shall be subject to the Specific Plan and General Plan policies in effect at the time of filing the application for the modification.

## **B. Consistency with Title 22 of the Los Angeles County Municipal Code**

The Specific Plan does not convey any rights not otherwise granted under the provisions and procedures contained in Title 22 and other applicable ordinances, except as specifically provided herein. Where general land use regulations and/or development standards of Title 22 are inconsistent with this Specific Plan, the standards and regulations of the Specific Plan shall prevail and supersede the applicable provisions of Title 22. The provisions of this Specific Plan shall also supersede the provisions of the Willowbrook Community Standards District (Chapter 22.64 of Title 22).

Any issue not specifically covered in the Specific Plan shall be subject to Title 22 and/or other titles of the Los Angeles County Municipal Code, or to interpretation by the Director if not specifically covered in the County's existing regulations.

### **3.3 Nonconforming Uses**

Single family residences located in zones that do not allow such uses may be continuously maintained as such, pursuant to the provisions of Chapter 22.174 (Nonconforming Uses, Buildings and Structures) of Title 22.

Existing legally established M-1 uses at the time of adoption of this Specific Plan may be continuously maintained as such, pursuant to the provisions of Chapter 22.174 (Nonconforming Uses, Buildings and Structures) of Title 22.

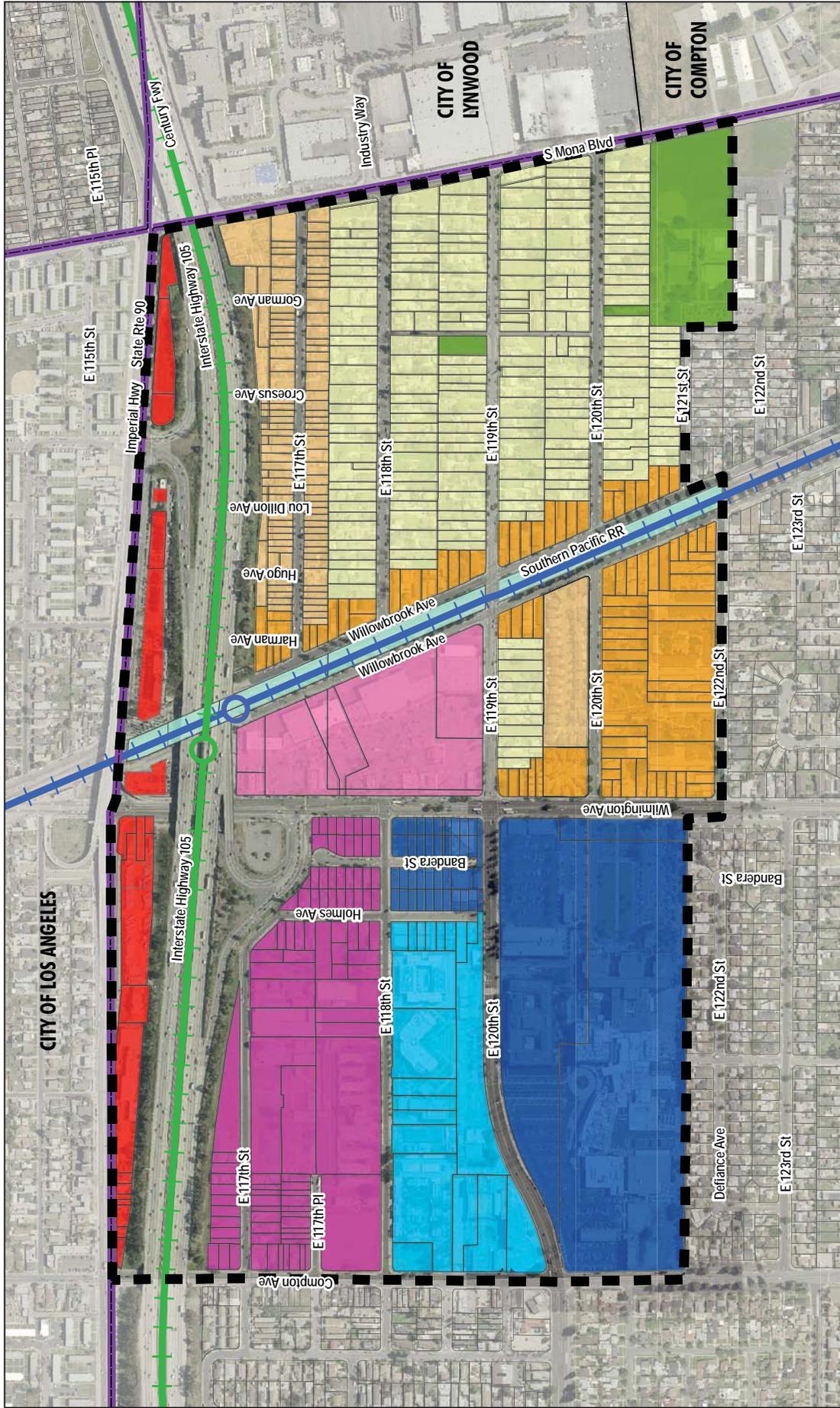
Any other use within the Specific Plan area that is nonconforming to the requirements and standards of this Plan shall be subject to Chapter 22.174 (Nonconforming Uses, Buildings and Structures) of Title 22.

### **3.4 Specific Plan Zones**

The Willowbrook TOD Specific Plan establishes zoning for parcels within the Specific Plan boundary as identified in Figure 3-1. The zones for the Specific Plan area are as follows:

- Mixed Use 1 (MU-1)
- Mixed Use 2 (MU-2)
- MLK Medical
- Drew Educational
- Imperial Commercial
- Willowbrook Residential 1
- Willowbrook Residential 2
- Willowbrook Residential 3
- Open Space (O-S)

The following sections of this Chapter identify the land use regulations and development and design standards for each Specific Plan zone. Parking requirements for uses in the Specific Plan are contained in Section 3.13 of this Chapter. Additional design guidelines are contained in Sections 3.14-3.18 of this Chapter.



**Figure 3-1  
Specific Plan Zones**

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Mixed Use 1 (FAR 1.5, 30 du/acre)
- Mixed Use 2 (FAR 3.0, 60 du/acre)
- MLK Medical (FAR 1.65)
- Drew Educational (FAR 1.5)
- Imperial Commercial (FAR 1.0)
- Willowbrook Residential 1 (9 du/acre)
- Willowbrook Residential 2 (18 du/acre)
- Willowbrook Residential 3 (30 du/acre)
- Open Space
- Rail Right-of-Way

**WILLOWBROOK TOD SPECIFIC PLAN**



## 3.5 Mixed Use Zones

### A. Purpose

The Mixed Use Zones are established to promote walking, bicycling, recreation, transit use and community reinvestment and are intended to facilitate integrated commercial and residential development through optimal site planning and efficient use of land.

**1. Mixed-Use 1 (MU-1) Zone.** The Mixed Use 1 (MU-1) zone is intended to provide commercial and residential development, with an emphasis on neighborhood serving retail, restaurant and service uses. The area is appropriate for a large retail or mixed use center, with a neighborhood plaza or community gathering space as a focal point and strong pedestrian connections to the Willowbrook/Rosa Parks Station, as well as the educational and medical campuses to the west.

**2. Mixed-Use 2 (MU-2) Zone.** The Mixed Use 2 (MU-2) zone is intended to provide commercial and residential development, with an emphasis on employment generating uses and residential infill development. The area is appropriate for office, business park, or mixed use developments, with an open space components and pedestrian connections to the Willowbrook/Rosa Parks Station, and the educational and medical campuses to the south.

### B. Land Use Regulations

- 1. General.** This section prescribes the land use regulations for the Mixed Use zones.
- 2. Permit and review requirements.** Table 3-1 identifies the permit or review required to establish each use.

**Table 3-1:  
Permit and Review Requirements**

Abbreviation	Permit or Review Requirement	Reference
--	Not Permitted	
P	Permitted	
SPR	Ministerial Site Plan Review	Chapter 22.188 of Title 22
CUP	Conditional Use Permit	Chapter 22.158 of Title 22
SEP	Special Events Permit	Chapter 22.192 of Title 22

- 3. Principal uses.** Table 3-2 identifies the principal uses and the permit or review required to establish each use. Additional regulations contained in Title 22 and/or this Specific Plan are also identified. Principal uses identified in Table 3-2 below may be established on a single site either as an integrated project, or as stand-alone use, subject to the provisions of this Chapter.

**Table 3-2:  
Principal Use Regulations for Mixed Use Zones**

Use	MU-1	MU-2	Additional Regulations
<b>Agricultural and Resource-Based Uses</b>			
Community gardens	SPR	SPR	
<b>Animal-Related Uses</b>			
Veterinaries, small animal			
A. Clinics	SPR	--	
B. Hospitals	CUP	--	
<b>Cultural, Educational and Institutional Uses</b>			
Churches, temples or other places used exclusively for religious worship, including accessory educational and social activities	SPR	CUP	
Community centers, including accessory facilities	SPR	SPR	
Libraries	SPR	SPR	
Museums	SPR	SPR	
Schools, including accessory facilities			
A. College, university, professional	CUP	CUP	
B. Elementary and secondary (public)	SPR	SPR	
C. Elementary and secondary (private)	--	CUP	
D. Vocational, technical and trade schools	SPR	CUP	
E. Tutorial, dance, art, martial arts, drama, etc.	SPR	SPR	
Theaters and other auditoriums	CUP	--	
<b>Industrial Uses</b>			
Laboratories, research and testing	--	CUP	
<b>Lodging Uses</b>			
Hotel	--	CUP	
<b>Recreational Uses</b>			
Billiards or pool halls	CUP	--	
Bowling alleys	CUP	--	
Parks, playgrounds and open space (plazas, etc.)	SPR	SPR	
<b>Residential Uses</b>			
Adult residential facility			
A. Facilities serving six or fewer persons	P	P	Chapter 22.140 of Title 22
Foster family home, in an approved residential use	p	p	
Group homes for children			
A. Facilities serving six or fewer persons	P	P	Chapter 22.140 of Title 22
Joint live and work units	SPR	SPR	Chapter 22.140 of Title 22
Multi-family housing			
A. Apartment houses	SPR	SPR	
B. Townhouses	SPR	SPR	Chapter 22.140 of Title 22
Small family homes for children	P	P	

Table 3-2 (continued)

Use	MU-1	MU-2	Additional Regulations
<b>Retail/Commercial Uses</b>			
Art gallery	SPR	SPR	
Drug store or pharmacy	SPR	SPR	
Farmers' market	SPR	SPR	Chapter 22.140 of Title 22
<b>Food and Beverage Sales</b>			
A. Grocery stores and markets	SPR	SPR	
B. Specialty stores (deli, coffee, bakery, produce)	SPR	SPR	
Health clubs	SPR	CUP	
<b>Home improvement, retail sales and service (hardware, lumber and building material stores)</b>			
A. Under 10,000 sf	SPR	--	
B. 10,000 sf or More	CUP	--	
Office machines and equipment sales	SPR	SPR	
Pet store, including the sale of pets (sales and grooming, no boarding)	SPR	--	
<b>Retail stores</b>			
A. 10,000 sq. ft. and under	SPR	SPR	
B. More than 10,000 sq. ft.	SPR	--	
Secondhand stores	SPR	--	
<b>Service Uses</b>			
Banks and financial institutions	SPR	SPR	
Business support services & facilities (including graphic reproduction, computer services, etc.)	SPR	SPR	
Catering establishments	SPR	SPR	
Check cashing	SPR	--	
Conference facilities	--	CUP	
<b>Day care</b>			
A. Adult day care	CUP	CUP	
B. Child day care center	SPR	SPR	
C. Large family child care home	--	--	
D. Small family child care home	P	P	
Health retreat	--	CUP	
Medical services - clinic, medical/dental offices, medical laboratory, and urgent/express care	SPR	SPR	
Offices, business and professional	SPR	SPR	
Personal services (barber, beauty Salon, spa, tailor, dry cleaner, self-service laundry, etc.)	SPR	SPR	
Parking lots and parking buildings (stand alone)	SPR	SPR	Chapter 22.112 of Title 22
Repair shops, household and fix-it	SPR	--	
Restaurants and other eating establishments, including food take-out and outdoor dining	SPR	SPR	Chapter 22.140 of Title 22; Section 3.5.C.5 of this Chapter

Table 3-2 (continued)

Use	MU-1	MU-2	Additional Regulations
Transportation, Electrical, Gas, Communication Utilities and Public Service Uses			
Comfort stations	SPR	SPR	
Communications equipment buildings	SPR	SPR	
Earth station	CUP	CUP	
Electric distributions substations, including related microwave facilities	SPR	SPR	Chapter 22.140 of Title 22
Fire station	SPR	SPR	
Gas metering and control stations, public utility	SPR	SPR	
Microwave stations	SPR	SPR	
Police station	SPR	SPR	
Post office	SPR	SPR	
Publicly owned uses, other than those already listed, that are necessary to maintain the public health convenience or general welfare	CUP	CUP	
Public utility service center	SPR	SPR	
Stations - bus, rail or taxi	SPR	--	
Telephone repeater station	SPR	SPR	
Water reservoirs, dams, treatment plants, gauging stations, pumping stations, tanks, wells and any use normal or accessory to the storage and distribution of water	CUP	CUP	

4. **Accessory uses.** Table 3-3 identifies uses accessory to an established principal use listed in Table 3-2.

**Table 3-3:  
Accessory Use Regulations for Mixed Use Zones**

Use	MU-1	MU-2	Additional Regulations
Accessory buildings and structures	SPR	SPR	Chapter 22.110 of Title 22
Alcoholic beverage sales for off-site consumption	CUP	--	Chapter 22.140 of Title 22
Alcoholic beverage sales for on-site consumption	CUP	--	Chapter 22.140 of Title 22
Home-based occupations	P	P	Chapter 22.140 of Title 22
Live entertainment	SPR	SPR	Chapter 22.140 of Title 22
Outdoor storage and display	SPR	SPR	Chapter 22.140 of Title 22
Signs	As specified in Chapter 22.114 of Title 22 and Section 3.5.C.4 of this Chapter		

5. **Temporary uses.** Table 3-4 identifies uses that are temporary and accessory to an established principal use listed in Table 3-2.

**Table 3-4:  
Temporary Use Regulations for Mixed Use Zones**

Use	MU-1	MU-2	Additional Regulations
Holiday and seasonal sales	SPR	SPR	Chapter 22.106 of Title 22
Special events	SEP	SEP	
Storage of materials and construction equipment used in construction or maintenance of streets and highways, sewers, storm drains, underground conduits, flood control works, pipelines and similar uses for up to one year	CUP	CUP	

6. **Uses not listed.** If a use is not specifically listed in Tables 3-2, 3-3 or 3-4, the Director shall have the authority to determine whether a use is permitted if that use is comparable to a particular use permitted in the zone.
7. **Prohibited uses.** Uses expressly prohibited in the Mixed Use Zones are as follows:
- a) Bars and cocktails lounges.
  - b) Drive-through establishments.
  - c) Liquor stores.
  - d) Pawn shops.
  - e) Tobacco and vape shops.
  - f) Vehicle-related uses, including vehicle sales, rentals, washing and service.

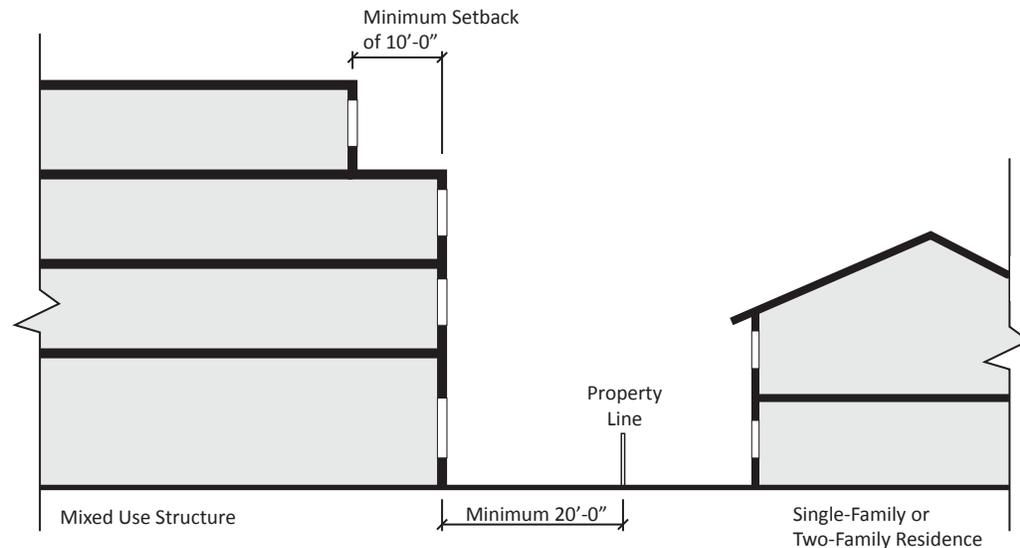
### C. Development and Design Standards

1. **General.** Table 3-5 identifies the development standards applicable to all development in the Mixed Use Zones. Additional regulations contained in Title 22 and/or this Specific Plan are also identified.

**Table 3-5:  
Development Standards for Mixed Uses**

Development Standards	MU-1	MU-2	Additional Regulations
Lot Area - Minimum	1 acre	1 acre	For newly created lots only. Individual or out-lots may be created for financing/sale purposes.
Minimum Street-fronting Yard Depth			Additional setback may be required depending on existing street frontage.
E. 119th Street	10 ft	n/a	
Willowbrook Avenue	20 ft	n/a	
Wilmington Avenue	5 ft	4 ft	
E. 117th Street	n/a	10 ft	
E. 117th Place	n/a	10 ft	
E. 118th Street	n/a	10 ft	
Bandera Street	n/a	10 ft	
Holmes Avenue	n/a	10 ft	
Compton Avenue	n/a	15 ft	
Minimum Interior Yard (side or rear)	0 ft	0 ft	
Minimum Interior Yard (side or rear) from Residential Use	n/a	20 ft	
Building Height - Maximum	50 ft and 4 stories	50 ft and 4 stories	Section 3.5.C.2. of this Chapter; Chapters 22.04 and 22.110 of Title 22
Floor Area Ratio - Maximum	1.5	3.0	
Residential Density - Maximum	30 du/ac	60 du/ac	
Non-Residential Common Open Space Area (courtyard or plaza) - Minimum	1,000 sf	1,000 sf	Minimum dimension shall be 25 feet.
Residential Common Open Space Area - Minimum	50 sf/du	50 sf/du for at least 50% of the units	Required for residential uses only. Minimum dimension shall be 25 feet.
Residential Private Open Space Area - Minimum	50 sf/du	50 sf/du	Required for residential uses only. Minimum dimension shall be 5 feet.
Residential Floor Areas - Minimum			
Efficiency	500 sf	500 sf	
One Bedroom	700 sf	700 sf	
Two Bedroom	900 sf	900 sf	
Three Bedroom	1,100 sf	1,100 sf	
Each Additional Bedroom	200 sf	200 sf	
Live/work unit	1,000 sf	1,000 sf	Chapter 22.106 of Title 22

- 2. Building height adjacent to residential zone or use.** At residential-adjacent locations, buildings shall maintain lower profiles to provide a transition between mixed use developments and residential zones and uses. The following standards apply:
- New development in the MU-1 Zone fronting E. 119th Street shall not exceed 40 feet and 3 stories for a depth of 50 feet from E. 119th Street property line.
  - New development in the MU-2 Zone fronting Compton Avenue shall step back the fourth story of the building by a minimum of 10 feet.
  - New development sharing an interior property line with an existing single or two family residence shall step back the fourth story of the building by a minimum of 10 feet (see Figure 3-2).



**Figure 3-2: Building height step back adjacent to residential zone or use**

- 3. Minimum transparency requirements.**
- Windows and openings facing streets shall constitute a minimum of 50% of street-level building facades, with the exception of residential units with individual entries, in which case windows and openings facing streets shall constitute a minimum of 30% of street-level building facades.
  - On upper-floors, windows and openings facing streets shall constitute a minimum of 40% of building facades for commercial uses, and 20% of building facades for residential uses.
- 4. Minimum floor-to-ceiling height.** The floor-to-ceiling height of the ground floor for a street-facing retail component of a mixed use development shall be a minimum of 15 feet.
- 5. Signs.** The provisions of Chapter 22.114 (Signs) of Title 22 for signs in the C-3 Zone shall apply to all development in the Mixed Use Zones, with the following exceptions:
- Roof signs shall be prohibited.
  - Signs on perimeter fences shall be prohibited.

**6. Standards for outdoor dining.** Outdoor dining is encouraged in the mixed use zones and may be provided either in private patios or within the public sidewalk right-of-way. Standards for outdoor sidewalk dining are provided in Chapter 16.27 (Outdoor Sidewalk Dining) of Title 16. The following standards apply to outdoor patio dining spaces.

- a) **Patio dining locations.** Patio dining shall be permitted within private property adjacent to the streets or public alley walkways and shall be used exclusively for dining, drinking and circulation therein. Outdoor patio dining is allowed within the street yard area.
- b) Patio dining spaces may be open or covered with temporary structures such as trellises, umbrellas or permanent structures such as overhangs and upper floors of the building.
- c) **Demarcation of patio dining spaces.** The patio dining spaces adjacent to streets or alley walkways shall be demarcated by either temporary or permanent boundary definers such as railings, fencing, or a combination of railings or fencing, and landscaping in planter boxes, or movable bollards.
- d) **Parking Requirements.** The area used for patio dining shall be included in the eating or drinking establishment's area for the purposes of calculating the establishment's parking requirement.
- e) **Hours of operation.** The outdoor dining space hours of operation shall be limited to the hours of operation of the associated dining establishment.
- f) **Awnings.** All awnings shall conform to the requirements of Title 26 (building Code) for roof coverings.
- g) **Music.** There shall be no amplified sound or music in the outdoor patio dining areas.
- h) **Display of outdoor menus.** A single-sided framed menu attached to a moveable barrier that defines the patio dining space is permitted within the patio dining space. The size of the frame shall not exceed three square feet. Freestanding pedestal menus, sandwich boards or A-frame displays are prohibited.
- i) All applicable provisions of Title 11 (Health and Safety Code) shall be observed in all areas of the restaurant.

**7. Residential open space.**

- a) The minimum required common and private open space area for residential uses is listed in Table 3-5. More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.
- b) Open space areas shall have no parking, driveway or right-of-way encroachments.
- c) Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy. Private open space areas that are intended for use by residents only shall not be accessible from the commercial portion of the mixed-use development.

**8. Site landscaping.**

- a) A minimum of 10 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.
- b) Chapter 22.126 (Tree Planting Requirements) of Title 22 establishes site tree requirements, including those for surface parking lots.

c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

**9. Pedestrian circulation.**

- a) Pedestrian walkways shall be a minimum of four feet in width.
- b) The use of asphalt for paving walkways is prohibited.

**10. Vehicular circulation.**

- a) Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, turning, or loading shall occur onsite and not within the public right-of-way.

**11. Parking lots.**

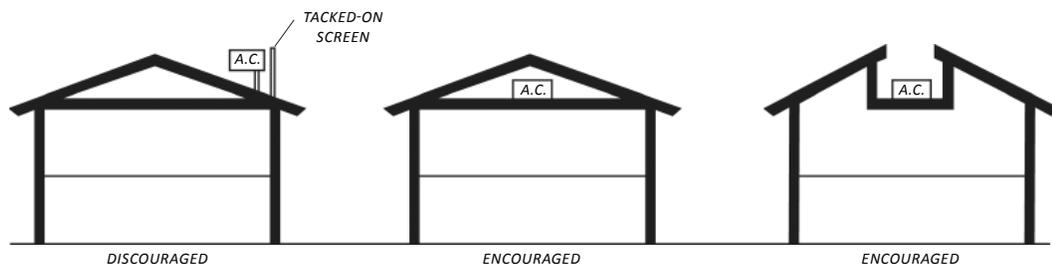
- a) Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.
- b) Parking is not permitted in the required street-fronting yard.
- c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.
- d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.
- e) Separate parking facilities shall be provided for residential uses and commercial uses.

**12. Fences, walls, gates and hedges.**

- a) Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:
  - A minimum 2-ft. change in horizontal plane for at least 10 feet.
  - A minimum 18-inch change in height for at least 10 feet.
  - A minimum 18-inch high raised planter for at least half the length of the wall.
  - Use of pilasters at 25-foot maximum intervals and at changes in wall planes.

**13. Utility and mechanical equipment.**

- a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being “tacked on.”



**Figure 3-3: Equipment Screening**

**14. Refuse and recyclable collection facilities.**

- a) Separate refuse collection facilities shall be provided and maintained for the residential units in a mixed use development from those for the commercial uses. It shall be clearly marked for residential use only.

## D. Performance Standards

The purpose of this section is to ensure that residential uses in mixed-use zones are not adversely impacted by adjacent commercial uses, including but not limited to traffic, noise, light, and safety impacts.

1. **Hours of operation.** The hours of operation for commercial uses shall be no earlier than 6:00 a.m. and no later than 11:00 p.m. daily, unless modified by a conditional use permit.
2. **Loading.** Loading, unloading and all maintenance activities shall be conducted within the hours of operation noted above, and in such a fashion so as to prevent annoyance to adjacent residents and tenants.
3. **Noise.** Noise generated by activities on the premises shall be controlled in such a manner so as not to create a nuisance or hazard on any adjacent property, in accordance with the Noise Ordinance in Title 12 (Environmental Protection) of the County Code. Residential units shall be constructed and designed to reduce the noise, particularly when located proximate to the Southern Pacific Rail Road (SPRR). Proper design may include, but shall not be limited to, building orientation, double windows, wall and ceiling insulation and orientation of vents. Common walls between residential and non-residential uses shall be constructed to minimize the transmission of noise and vibration.
4. **Light and Glare.** All outdoor lighting associated with commercial uses adjacent to or within the immediate vicinity of residential uses shall be designated with fixtures and poles that illuminate commercial uses, while minimizing light trespass into residential areas. An unacceptable level of light trespass shall be 0.8 foot-candles or greater when the light trespass falls onto an adjoining residentially-zoned lot, or open space zoned lot.
5. **Operating activities prohibited.** The following operating activities shall be prohibited in commercial uses located within mixed use developments:
  - a) Storage or shipping of flammable liquids or hazardous materials beyond that normally associated with a residential use; and
  - b) Welding, machining, or open flame work.
6. **Graffiti.** To encourage the maintenance of exterior walls free from graffiti that would impact pedestrian views, all structures, walls, and fences open to public view shall remain free of graffiti. In the event of such graffiti occurring, the property owner, tenant, or their agent shall remove or cover said graffiti within 72 hours, weather permitting. Paint utilized in covering such graffiti shall be a color that matches, as closely as possible, the color of the adjacent surfaces.
7. **Security.** The residential units shall be designed to ensure the security of residents through the provision of separate and secured entrances and exits that are directly accessible to secured parking areas. Where residential units are in the same structure as a commercial use, access to residential units shall be from a secured area located on the first floor at the ground level. Nonresidential and residential uses located on the same floor shall not have common entrance hallways or common balconies.

## 3.6 MLK Medical Zone

### A. Purpose

The MLK Medical Zone is established to meet the existing and future needs of the Martin Luther King Jr. Medical Center campus, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote medical, clinic, medical office, and associated supportive uses such as incidental retail, supportive residential and parking, and expand pedestrian linkages and connectivity between the MLK Medical Center, Charles R. Drew University of Medicine and Science (CDU), the Willowbrook/Rosa Parks Station, and the Willowbrook community.

### B. Land Use Regulations

The land use regulations for the Institutional (IT) Zone contained in Chapter 22.24 of Title 22 shall apply to all development in the MLK Medical Zone with the following addition:

1. **Senior citizen residences.** Senior citizen residences shall be permitted in the MLK Medical Zone.

### C. Development and Design Standards

The standards for the IT Zone contained in Chapter 22.24 of Title 22 shall apply to all development in the MLK Medical Zone.

## 3.7 Drew Educational Zone

### A. Purpose

The Drew Educational Zone is established to meet the existing and future needs of the Charles R. Drew University of Medicine and Science (CDU) and King Drew Magnet High School, while ensuring compatibility with adjacent land uses. The intent is to create a medical university campus for CDU by maintaining and promoting educational and associated support uses, while maintaining sensitivity to surrounding development; as well as expand pedestrian linkages and connectivity between the CDU campus, Martin Luther King Jr. Medical Center, Willowbrook/Rosa Parks Station, Kenneth Hahn Plaza, and the Willowbrook community. The future CDU Master Plan shall comply with the regulations and standards contained in this Section 3.7.

### B. Land Use Regulations

The land use regulations for the Institutional (IT) Zone contained in Chapter 22.24 of Title 22 shall apply to all development in the Drew Educational Zone with the following addition:

1. **Student and faculty housing.** Student and faculty housing shall be permitted in the Drew Educational Zone.

### C. Development and Design Standards

The standards for the IT Zone contained in Chapter 22.24 of Title 22 shall apply to all development in the Drew Educational Zone, with the following modifications:

1. **Street-fronting yard depths.**
  - a) The minimum street-fronting yard depths shall be 15 feet from Compton Avenue and 10 feet from E. 118th Street, E. 120th Street, and Holmes Avenue.
2. **Maximum height.**
  - a) The maximum building height shall be 75 feet and 6 stories. New development fronting Compton Avenue shall be 50 feet and four stories for a depth of 100 feet from the Compton Avenue property line. Development standards related to height per Chapter 22.110 of Title 22 shall also apply.
3. **Required residential open space.**
  - a) The minimum open space area requirements for multi-family developments in the Drew Educational Zone shall be as follows:
    - i. 50 square feet/dwelling unit of common open space area with a minimum dimension of 25 feet; and
    - ii. 50 square feet/dwelling unit of private open space area with a minimum dimension of 5 feet.
  - b) More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.
  - c) Open space areas shall have no parking, driveway or right-of-way encroachments.

- d) Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy.
- e) Private open space areas that are intended for use by residents only shall not be accessible from the non-residential portion of the development.

#### **4. Site landscaping.**

- a) A minimum of 20 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.
- b) Chapter 22.126 (Tree Planting Requirements) of Title 22 establishes site tree requirements, including those for surface parking lots.
- c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

#### **5. Pedestrian circulation.**

- a) Pedestrian walkways shall be a minimum of four feet in width.
- b) The use of asphalt for paving walkways is prohibited.

#### **6. Vehicular circulation.**

- a) Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, loading, or turning shall occur onsite and not within the public right-of-way.

#### **7. Parking lots.**

- a) Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.
- b) Parking is not permitted in the required street-fronting yard.
- c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.
- d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.
- e) Separate parking facilities shall be provided for residential uses and institutional uses.

#### **8. Fences, walls, gates and hedges.**

- a) Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:
  - A minimum 2-ft. change in horizontal plane for at least 10 feet.
  - A minimum 18-inch change in height for at least 10 feet.
  - A minimum 18-inch high raised planter for at least half the length of the wall.
  - Use of pilasters at 25-foot maximum intervals and at changes in wall planes.

#### **9. Utility and mechanical equipment.**

- a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."

## 3.8 Imperial Commercial Zone

### A. Purpose

The Imperial Commercial Zone is established to meet the commerce and service needs of the resident and business communities, while ensuring compatibility with adjacent land uses. The intent is to maintain and promote commercial uses between Imperial Highway and the I-105 Freeway. The Imperial Commercial Zone provides for the development of a broad range of retail and service uses, as well as freeway-oriented, regional-serving retail, office complexes, and light manufacturing businesses.

### B. Land Use Regulations

The land use regulations for the Unlimited Commercial (C-3) Zone contained in Chapter 22.20 of Title 22 shall apply to all development in the Imperial Commercial Zone, with the following additions and exceptions:

1. **Self service storage facilities.** A self service storage facility shall be permitted in the Imperial Commercial Zone with a Conditional Use Permit (CUP) application (Chapter 22.158 of Title 22), and subject to the provisions of Section 22.140.570 of Title 22.
2. **Limitations on use.** The following uses shall not be permitted in the Imperial Commercial Zone:
  - a) Liquor stores.
  - b) Tobacco and vape shops.
  - c) New residential uses.

### C. Development and Design Standards

The standards for the C-3 Zone contained in Chapter 22.20 of Title 22 shall apply to all new development in the Imperial Commercial Zone, with the following modifications:

1. **Maximum lot coverage.**
  - a) The maximum lot coverage by structures of any type in the Imperial Commercial Zone shall be 50 percent.
2. **Maximum height.**
  - a) The maximum building height in the Imperial Commercial Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.80 of Title 22 shall also apply.
3. **Maximum floor area ratio (FAR).**
  - a) The maximum FAR in the Imperial Commercial Zone shall be 1.0.
4. **Site landscaping.**
  - a) A minimum of 10 percent of the lot shall be landscaped with trees, ground cover, shrubbery and flowers, and shall be continuously maintained in good condition. Parking lot landscaping does not count towards this requirement. Incidental walkways may be developed in the landscaped area.
  - b) Chapter 22.126 (Tree Planting Requirements) of Title 22 establishes site tree requirements, including those for surface parking lots.
  - c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

**5. Pedestrian circulation.**

- a) Pedestrian walkways shall be a minimum of four feet in width.
- b) The use of asphalt for paving walkways is prohibited.

**6. Vehicular circulation.**

- a) Vehicular access, drives and circulation routes shall be designed so that all movements involved in parking, loading or turning shall occur onsite and not within the public right-of-way.

**7. Parking lots.**

- a) Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.
- b) Parking is not permitted in the required street-fronting yard.
- c) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.
- d) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.

**8. Utility and mechanical equipment.**

- a) All mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."

## 3.9 Willowbrook Residential 1 Zone

### A. Purpose

The Willowbrook Residential 1 Zone is established to preserve and enhance desirable characteristics of single family residential areas. The Willowbrook Residential 1 Zone provides for primarily detached single-family dwelling units, and is intended to provide Willowbrook residents with a comfortable, healthy, and safe living environment.

### B. Land Use Regulations

The land use regulations for the Single Family Residence (R-1) Zone contained in Chapter 22.18 of Title 22 shall apply to all development in the Willowbrook Residential 1 Zone, with the following additions:

1. **Living suites.** A living suite, as defined in Chapter 22.14 (Definitions) of Title 22, shall be permitted as an accessory use in the Willowbrook Residential 1 Zone with a Ministerial Site Plan Review (SPR) application (Chapter 22.146 of Title 22), and subject to the following:
  - a) A living suite shall not be permitted on a lot or parcel of land where any of the following exists:
    - i. A guest house, as defined in Chapter 22.14 of Title 22; or
    - ii. A second unit, as defined in Chapter 22.14 of Title 22.
2. **Noncommercial keeping of chickens.** The noncommercial keeping of chickens shall be permitted as an accessory use in the Willowbrook Residential 1 Zone, subject to the following provisions:
  - a) No more than five chickens per residence are allowed, located within the rear yard and kept outside the dwelling unit.
  - b) All such chickens shall be kept in an enclosure within the rear yard and located a minimum of 35 feet from any habitable building.
  - c) Roosters shall not be permitted.
  - d) For the purposes of this section, the occasional sale of eggs or chickens, which is incidental to the keeping of chickens as a hobby, does not constitute a commercial use.

### C. Development and Design Standards

The standards for the R-1 Zone contained in Chapter 22.18 of Title 22 shall apply to all development in the Willowbrook Residential 1 Zone, with the following modifications:

1. **Maximum height.**
  - a) The maximum height permitted in the Willowbrook Residential 1 Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.80 of Title 22 shall also apply.
2. **Minimum floor area.**
  - a) The minimum floor area of a new single family residence in the Willowbrook Residential 1 Zone shall be 1,200 square feet.
3. **Second units.**
  - a) The maximum height of a second unit in the Willowbrook Residential 1 Zone shall be as follows:
    - i. 24 feet for detached units; and

- ii. 24 feet for attached units, with the following exceptions:
  - Any portion of the structure that is set back more than 20 feet from the front property line may have an additional foot in height for every additional foot of setback, up to a maximum of 35 feet in height; and
  - Any portion of the structure that is set back more than five feet from the side property line may have an additional foot in height for every additional foot of setback, up to a maximum of 35 feet in height.
- iii. In no case shall a second unit be placed within a required side yard, or closer than five feet to any lot line.
- iv. All other provisions of Section 22.106.550 of Title 22 shall apply to second units.

**4. Driveways and walkways.**

- a) The use of asphalt for paving driveways and walkways is prohibited.

**5. Front yard landscaping.**

- a) With the exception of the required paved driveway and walkway having a width not to exceed four feet, all areas within the street-fronting yard shall be landscaped and maintained.
- b) Where possible, existing trees shall be maintained.
- c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

**6. Fences.**

- a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review.
- b) Those portions of fences more than three and on-half feet high must be substantially open (75% transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction.
- c) Barbed wire and chain link fencing are prohibited.
- d) Drying clothes on fencing is not permitted.

**7. Clotheslines.**

- a) Clotheslines or clotheslines structures are permitted, provided they are located to the rear of the structure, and not visible from adjoining streets when viewed at ground level.

**8. Utility and mechanical equipment.**

- a) All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or they shall be screened from view. Satellite dishes shall also be located away from public view.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."

## 3.10 Willowbrook Residential 2 Zone

### A. Purpose

The purpose of the Willowbrook Residential 2 Zone is to preserve and enhance single family neighborhood characteristics, while also providing an environment suitable for two-family residences. The intent is to promote desirable characteristics of low to medium density neighborhoods.

### B. Land Use Regulations

The land use regulations for the Two-Family Residence (R-2) Zone contained in Chapter 22.18 of Title 22 shall apply to all development in the Willowbrook Residential 2 Zone in the Willowbrook TOD Specific Plan area, with the following additions:

1. **Living suites.** A living suite, as defined in Chapter 22.14 (Definitions) of Title 22, shall be permitted as an accessory use in the Willowbrook Residential 1 Zone with a Ministerial Site Plan Review (SPR) application (Chapter 22.146 of Title 22), and subject to the following:
  - a) A living suite shall not be permitted on a lot or parcel of land where any of the following exists:
    - i. A guest house, as defined in Chapter 22.14 of Title 22; or
    - ii. A second unit, as defined in Chapter 22.14 of Title 22.

### C. Development and Design Standards

The standards for the R-2 Zone contained in Chapter 22.18 of Title 22 shall apply to all development in the Willowbrook Residential 2 Zone in the Willowbrook TOD Specific Plan area, with the following additions and exceptions:

1. **Maximum height.**
  - a) The maximum height permitted in the Willowbrook Residential 2 Zone shall be 35 feet and two stories. Development standards related to height per Chapter 22.80 of Title 22 shall also apply.
2. **Second units.**
  - a) The maximum height of a second unit as an accessory use to a single family residence in the Willowbrook Residential 2 Zone shall be as follows:
    - i. 24 feet for detached units; and
    - ii. 24 feet for attached units, with the following exceptions:
      - Any portion of the structure that is set back more than 20 feet from the front property line may have an additional foot in height for every additional foot of setback, up to a maximum of 35 feet in height; and
      - Any portion of the structure that is set back more than five feet from the side property line may have an additional foot in height for every additional foot of setback, up to a maximum of 35 feet in height.
    - iii. In no case shall a second unit be placed within a required side yard, or closer than five feet to any lot line.
    - iv. All other provisions of Section 22.106.540 of Title 22 shall apply to second units.
3. **Driveways and walkways.**
  - a) The use of asphalt for paving driveways and walkways is prohibited.

**4. Front yard landscaping.**

- a) With the exception of the required paved driveway and walkway having a width not to exceed four feet, all areas within the street-fronting yard shall be landscaped and maintained.
- b) Where possible, existing trees shall be maintained.
- c) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

**5. Fences.**

- a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review.
- b) Those portions of fences more than three and on-half feet high must be substantially open (75% transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction.
- c) Barbed wire and chain link fencing are prohibited.
- d) Drying clothes on fencing is not permitted.

**6. Clotheslines.**

- a) Clotheslines or clotheslines structures are permitted, provided they are located to the rear of the structure, and not visible from adjoining streets when viewed at ground level.

**7. Utility and mechanical equipment.**

- a) All utility and mechanical equipment shall be placed in locations that are not exposed to view from the street or they shall be screened from view. Satellite dishes shall also be located away from public view.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."

### 3.11 Willowbrook Residential 3 Zone

#### A. Purpose

The Willowbrook Residential 3 Zone is established to provide opportunities for developments containing multiple units, such as apartments or condominiums. The intent is to promote desirable characteristics of medium density neighborhoods, and provide a variety of housing options to serve the needs of the Willowbrook community.

#### B. Land Use Regulations

The land use regulations for the Limited Multiple Residence (R-3) Zone contained in Chapter 22.18 of Title 22 shall apply to all development in the Willowbrook Residential 3 Zone in the Willowbrook TOD Specific Plan area, with the following additions:

1. Living suites. A living suite, as defined in Chapter 22.14 (Definitions) of Title 22, shall be permitted as an accessory use in the Willowbrook Residential 1 Zone with a Ministerial Site Plan Review (SPR) application (Chapter 22.146 of Title 22), and subject to the following:
  - a) A living suite shall not be permitted on a lot or parcel of land where any of the following exists:
    - i. A guest house, as defined in Chapter 22.14 of Title 22; or
    - ii. A second unit, as defined in Chapter 22.14 of Title 22.

#### C. Development and Design Standards

The standards for the R-3 Zone contained in Chapter 22.18 of Title 22 shall apply to all development in the Willowbrook Residential 3 Zone in the Willowbrook TOD Specific Plan area, with the following modifications:

1. **Maximum height.**
  - a) The maximum height permitted in the Willowbrook Residential 3 Zone shall be 35 feet and three stories. Development standards related to height per Chapter 22.80 of Title 22 shall also apply.
2. **Second units.**
  - a) The maximum height of a second unit as an accessory use to a single family residence in the Willowbrook Residential 3 Zone shall be as follows:
    - i. 24 feet for detached units; and
    - ii. 24 feet for attached units, with the following exceptions:
      - Any portion of the structure that is set back more than 20 feet from the front property line may have an additional foot in height for every additional foot of setback, up to a maximum of 35 feet in height; and
      - Any portion of the structure that is set back more than five feet from the side property line may have an additional foot in height for every additional foot of setback, up to a maximum of 35 feet in height.
    - iii. In no case shall a second unit be placed within a required side yard, or closer than five feet to any lot line.
    - iv. All other provisions of Section 22.106.540 of Title 22 shall apply to second units.

**3. Required residential open space.**

- a) The minimum open space area requirements for multi-family developments in the Willowbrook Residential 3 Zone shall be as follows:
  - i. 50 square feet/dwelling unit of common open space area with a minimum dimension of 25 feet; and
  - ii. 50 square feet/dwelling unit of private open space area with a minimum dimension of 5 feet.
- b) More than one open space area may be provided on a lot. The sum of square footages for all eligible open space areas on a lot shall comprise the total open space area for that lot. Required side or rear yard areas may be included in the calculated open space area, but a required front yard area shall not be included.
- c) Open space areas shall have no parking, driveway or right-of-way encroachments.
- d) Private useable open space shall be contiguous to the residential unit served and screened from public view for privacy. All patios that front a public street shall be substantially enclosed for screening and privacy. All balconies that front a public street shall have solid rails for screening and privacy.

**4 Site landscaping.**

- a) A minimum of 20 percent of the lot shall be landscaped or hardscaped with open, useable outdoor space. Required common open space areas may be counted in the calculated landscaped area.
- b) With the exception of the required paved driveway and walkway(s), all areas within the street-fronting yard shall be landscaped and maintained.
- c) Chapter 22.126 (Tree Planting Requirements) of Title 22 establishes site tree requirements, including those for surface parking lots.
- d) The provisions of Section 4.106.5 (Landscape Design) of Title 31 shall also apply.

**5. Pedestrian circulation.**

- a) Pedestrian walkways shall be a minimum of four feet in width.
- b) The use of asphalt for paving walkways is prohibited.

**6. Vehicular circulation.**

- a) Vehicular access, drives and circulation routes shall be designed so that all movements involved in loading, parking, or turning shall occur onsite and not within the public right-of-way.
- b) Principal vehicular access into multi-family developments shall be through an entry drive (with no direct access to parking spaces) rather than a parking aisle (direct access to parking spaces).

**7. Parking.**

- a) Parking for multi-family developments shall be provided on site in on-grade or underground structures, surface parking lots, carports, or attached garages.
- b) Parking shall be located proximate to the building and located to the rear of the parcel wherever possible. Parking shall be designed to avoid awkward turning maneuvers and the backing of vehicles into public streets. Parking areas shall be designed with end-stall turnarounds or a continuous circulation pattern.
- c) Parking shall not occupy more than 30% of any linear street frontage.
- d) Parking is not permitted in the street-fronting yard.
- e) Continuous concrete curbs shall be provided as wheel stops where parking adjoins landscaping.
- f) Parking areas shall be separated from buildings by a pedestrian walkway and/or landscape strip.
- g) Carports and parking structures shall be architecturally compatible with the design of the main structures in the project.

**8. Fences, walls, gates and hedges.**

- a) If street fencing is required for security reasons, wrought iron style fences which do not obscure views may be permitted to the maximum height of six feet within front yards and corner side yards, subject to Director's review. Those portions of fences more than three and on-half feet high must be substantially open (75% transparency), except for pillars used in conjunction with wrought iron fences and shall not cause a significant visual obstruction. Barbed wire and chain link fencing are prohibited. Drying clothes on fencing is not permitted.
- b) Wall sections greater than 50 feet in length fronting a street shall incorporate at least two of the following design features, in proportion to the length of the wall:
  - A minimum 2-ft. change in horizontal plane for at least 10 feet.
  - A minimum 18-inch change in height for at least 10 feet.
  - A minimum 18-inch high raised planter for at least half the length of the wall.
  - Use of pilasters at 25-foot maximum intervals and at changes in wall planes.

**9. Utility and mechanical equipment.**

- a) All utility and mechanical equipment shall be screened from public view. This includes all ground, wall, and roof mounted equipment. Satellite dishes shall also be located away from public view.
- b) Screening elements shall be an integral part of the building; no screening method shall give the appearance of being "tacked on."

**10. Refuse and recyclable collection facilities.**

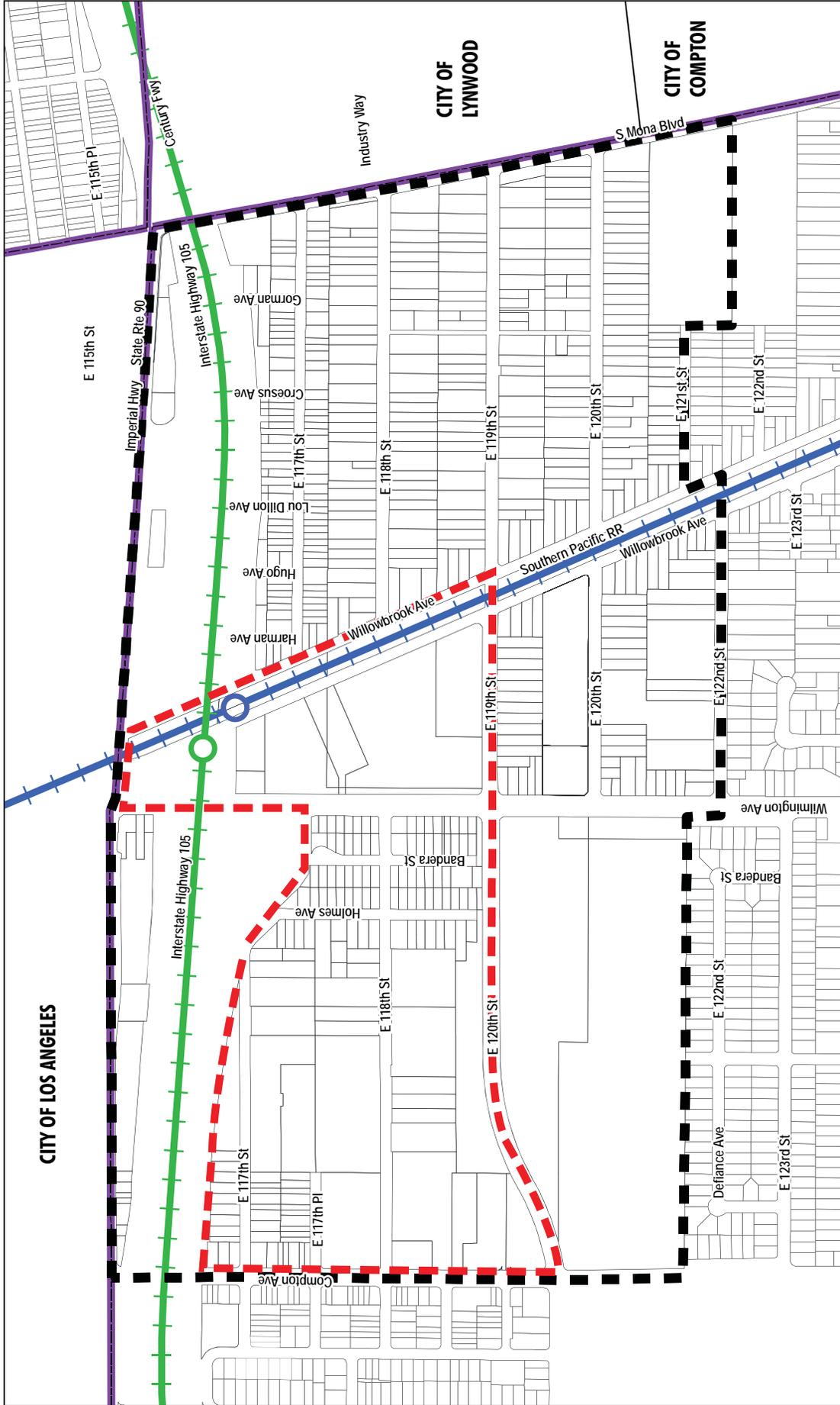
- a) Refuse enclosures shall be located for the convenience of the residents and be easily accessible for refuse and recyclables collection, but shall not impede circulation during loading operations.

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## **3.12 Open Space (O-S) Zone**

### **A. General**

The provisions of Chapter 22.16 of Title 22 shall apply to all development in the Open Space (O-S) Zone in the Willowbrook TOD Specific Plan area.



**Figure 3-4  
TOD Parking Reduction  
Overlay Zone**

-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  TOD Parking Reduction Overlay Zone

**WILLOWBROOK TOD SPECIFIC PLAN**

5 minute walk (1,200')



### 3.13 Parking

#### A. Introduction

The Willowbrook TOD Specific Plan provides modifications to the parking requirements contained in Chapter 22.112 of Title 22. These modifications are intended to provide a parking supply that is more closely tailored to transit-oriented development and to allow greater flexibility in the provision of required parking.

#### B. Parking Requirements

The standards for parking requirements contained in Chapter 22.18 of Title 22 shall apply to all development in the Willowbrook TOD Specific Plan area with the following additions and exceptions:

1. This Specific Plan establishes a TOD Parking Reduction Overlay Zone with reduced parking requirements and maximum parking standards. The boundaries for this zone are shown in Figure 3-4. Minimum and maximum parking standards shall be as set forth in Table 3-6.
2. Parking requirements for non-residential uses may be met within 1,500 feet from the site of the development.
3. A Transportation Demand Management Program shall be implemented for all new non-residential uses in the Specific Plan Area exceeding 50,000 sq. ft. to reduce vehicle trips and reduce parking needs in the TOD (as identified in Chapter 4, Section 4.8).

**Table 3-6:  
Parking Requirements**

Land Use	Minimum	Maximum
Residential		
Single Family	2.0/DU	2.0/DU
Bachelor Apartment	0.60/DU	1.0/DU
1-Bed Apartment	0.90/DU	1.5/DU
2-Bed Apartment	1.20/DU	2.0/DU
Guest <sup>1</sup>	0.15/DU	0.25/DU
Non-Residential		
Office	1.50/1,000 sf	2.5/1,000 sf
Medical Office	2.40/1,000 sf	4.0/1,000 sf
Clinic	2.40/1,000 sf	4.0/1,000 sf
Retail <sup>2</sup>	2.40/1,000 sf	4.0/1,000 sf
Restaurant	6.80/1,000 sf	17.0/1,000 sf <sup>3</sup>
Light Industrial	1.20/1,000 sf	2.0/1,000 sf

1. Guest parking is not required if the number of required guest parking is one (1).
2. TOD reductions are allowed for specific specialty retail/service uses.  
However, because land use forecasts are not by specific use, only 40% TOD reduction applied for general retail category.
3. Rate for 1,000 sq ft estimated.

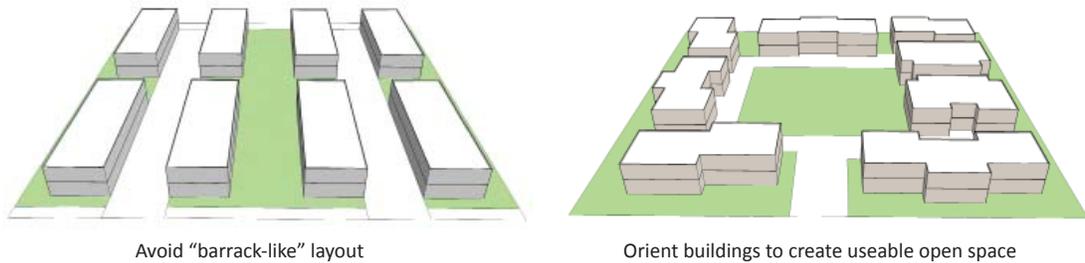
4. Required parking shall be provided on site; and is not permitted within the required front yard or corner side yard. Required parking for new single family uses may be uncovered. A Ministerial Site Plan Review (SPR) application (Chapter 22.146 of Title 22) is required for the conversion of covered parking to uncovered parking in existing single family uses.
5. Tandem parking is allowed for single family uses. A parking space for a second unit may be located in tandem with a space serving the existing single-family residence if the design is necessary to provide the required number of spaces for both units.
6. The provisions regarding shared parking contained in Sections 22.138.050 B2, 3, 4 and 5 in Chapter 22.180 (Parking Permits) of Title 22 shall apply.

### 3.14 Design Guidelines for Mixed Use, Commercial and Institutional Uses

The purpose of this section is to facilitate high quality development for mixed use, institutional and commercial uses within the Willowbrook TOD Specific Plan area. These design guidelines address site planning and building design and are in addition to the above land use regulations and development standards contained in this chapter.

#### 1. Building siting and orientation.

- a) Buildings should be sited to avoid random and irregular building relationships, and arranged to create a sense of unity and overall harmony. To the maximum extent possible, new structures should be clustered to create plazas and open space areas and avoid creating of “barrack-like” rows of structures. Where clustering is impractical, a visual link between separate structures should be established. This link can be accomplished through the use of an arcade system, trellis or other open structure.



Avoid “barrack-like” layout

Orient buildings to create useable open space

**Figure 3-5: Site layout to create useable space**



*Arcades and trellises provide shade, connections and define usable space.*

- b) Commercial facilities in mixed-use projects should be oriented to the street, with parking generally located in the rear or side of buildings. The perimeter of parking areas and driveways adjacent to streets and sidewalks should be screened with an attractive low wall, berm, fence or landscaping.
- c) Existing residential uses should be buffered from incompatible commercial development to mitigate negative impacts due to noise, vibration, shading, light and glare, and aesthetics. Intensified landscaping, increased yard depths and/or appropriate building orientation should be utilized as a means of providing adequate separation between such land uses.
- d) Entry and edge design features such as landscaping, architectural signage and monumentation, and/or enhanced paving should be incorporated.

## 2. Building modulation and detailing.

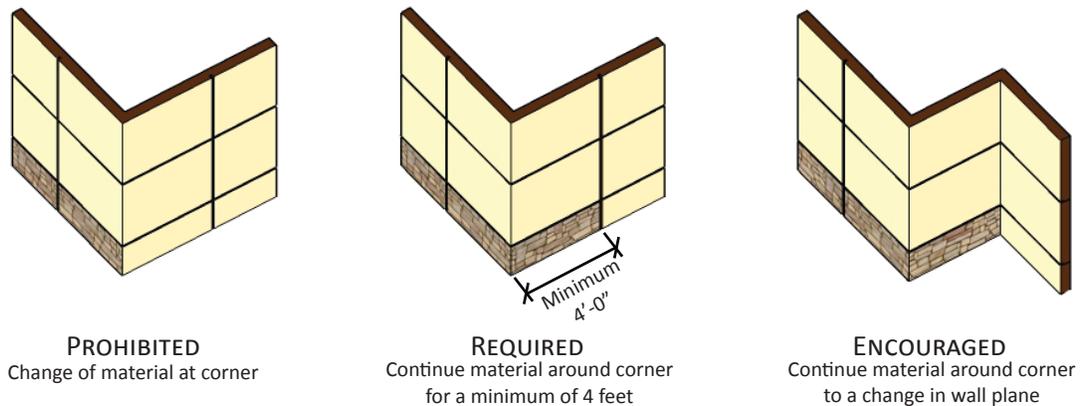
- a) Building design should avoid large monotonous facades, long straight-line building fronts, plain box shapes, and barren exterior treatment. All building elevations visible from a public way, including freeways, should be highly articulated and incorporate the chosen design theme in a consistent manner.



*Varied building forms, volumetric and planar changes, and variations in roof forms and height, contribute to a well-articulated building massing.*

- b) Commercial facades of mixed-use projects should be modulated at least every 50 feet by changes in building mass or facade treatment, such as projected entrance windows, roof form or other architectural features.
- c) Building facades should be designed to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; varying color or materials of each individual module within a harmonious palette of colors and materials, etc.
- d) Building form and articulation should be used to accentuate public entrances, de-emphasize service areas, and define and shelter pedestrian walkways and exterior spaces.
- e) Awnings, canopies and arcades should be used as appropriate to provide visual interest and shade.
- f) Buildings should be treated as a whole and finished appropriately on all sides to provide continuity. Backs of buildings shall use similar materials; however, less expensive and more utilitarian substituted materials are acceptable, provided they are compatible with the overall design.

- g) Material changes shall not occur at corners. The same material shall continue around corners for a minimum distance of 4 feet. If feasible, the same material should continue to the next change in the wall plane.



**Figure 3-6: Treatments for material changes at corners**

### 3. Building entrances.

- In mixed use projects, when residential and commercial uses are combined in the same structure, separate entrances shall be provided for each use.
- The main entrance or entrances should be emphasized and oriented to the street or major plazas or open space. Main entries to buildings should be clearly demarcated, visible and accessible from the street and/or pedestrian walkways. Examples include the use of special materials, color, detailing, or equivalent architectural treatment.
- Secondary entries may be from parking areas.



*Entrances should be clearly demarcated. Entrances may be emphasized with building elements, material and color variations and signage.*

### 4. Transparency.

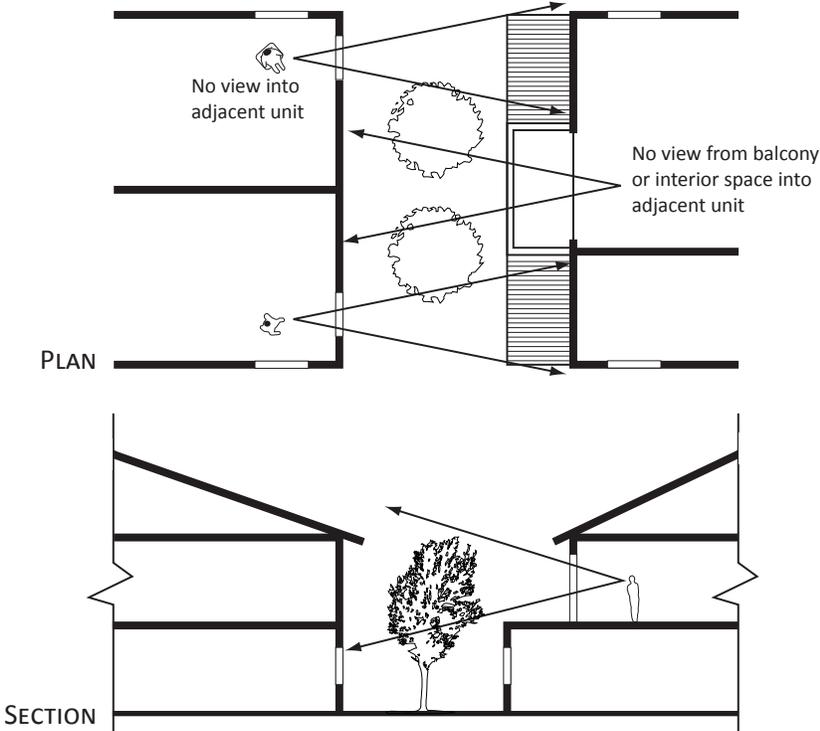
- Placement and orientation of doorways and windows should create strong, direct relationships with the street. Street-facing facades of all buildings shall incorporate windows and openings providing light to adjacent spaces and rooms, and to create visual interest and provide "eyes on the street."
- Windows and openings shall provide clear and transparent view into the ground floor retail uses or shall display merchandise to reinforce a pedestrian scale. Dark tinted, reflective or opaque glazing is not permitted for any required wall opening along street level retail façades.



Retail uses shall have transparent windows that help activate the street.

**5. Privacy for residential units.**

- a) Buildings should be oriented to promote privacy to the greatest extent possible. In mixed use projects, residential windows should face away from loading areas and docks. To the extent residential windows face the windows of an adjacent unit; the windows should be offset to maximize privacy.
- b) Windows, balconies or similar openings should be oriented so as not to have a direct line-of-sight into adjacent units within the development. In addition, units above the first story should be designed so that they do not look directly onto private patios or backyards of adjoining residential property or units.



Plan appropriate trees and offset windows and balconies (or patios) to maintain privacy between residential units

**Figure 3-7: Privacy for Residential Units**

## 6. Vehicle circulation and access.

- a) Site access and internal circulation in all developments shall be safe, efficient, and convenient. Vehicular traffic should be adequately separated from pedestrian circulation. Vehicular entrances shall be clearly identified to minimize pedestrian/vehicle conflict.
- b) The number of site access points or driveway aprons should be minimized for aesthetic purposes, to achieve efficient and productive use of paved pathways, and to eliminate traffic hazards. They should be located as far as possible from street intersections and be coordinated with existing or planned median openings and driveways on the opposite side of the roadway. Common driveways that promote access to more than one site are encouraged.

## 7. Parking lots.

- a) Parking lots should be designed with a clear hierarchy of circulation: major access drives with no direct access to parking spaces; major circulation drives with little or no parking; and parking aisles for direct access to parking spaces. Loading and service areas should be provided with separate access and circulation whenever possible.
- b) The visual impact of parking lots should be mitigated with landscaping or screened by buildings.

## 8. Parking structures.

- a) The massing, height and architectural design of the parking structure should be consistent and compatible with the rest of the development within which the parking structure is to be located.
- b) Vehicular access to parking structures should be designed so as not to obstruct free flow of traffic on adjoining public streets. Vehicular entrances and exits should be located so as to minimize turning movement conflicts as vehicles enter and exit the structure.
- c) Pedestrian access points to the parking structure should be located to avoid pedestrian/vehicle conflicts.



*Parking structures should be designed to integrate with the area they serve. Well designed structures, ground-level storefronts, and enhanced landscape treatments can minimize the visual impact of parking structures.*

**9. Pedestrian circulation.**

- a) All developments shall be designed to enhance pedestrian movement within the project and to adjacent uses. The design of new projects shall promote pedestrian walkability and connectivity.
- b) Pedestrian walkways between parking areas and buildings shall be provided. Pedestrian walkways should also be provided to link dwelling units with commercial or institutional uses in a mixed-use or institutional development respectively, as well as with common open space, plazas and courtyards, and public sidewalks.
- c) Where pedestrian paths or walkways cross parking areas or driveways, decorative paving or some other method shall be used to define the pedestrian space and delineate crossings.
- d) Pedestrian paths or walkways to building entrances shall be paved with high quality paving materials such as but not limited to, stone pavers, decorative concrete pavers and brick.

**10. Residential open space.**

- a) Common open space areas shall be convenient to the residential portion of a mixed-use development, separate and secure from the non-residential component, and clearly marked for residential use only.
- b) Common open space areas shall contain amenities appropriate to the project’s size; i.e. pools, spas, recreation buildings are encouraged for larger projects, while barbeque areas and gazebos may be more appropriate for smaller projects. Children’s play areas shall be sited to be visible from residential units and not visible from the public right-of-way.



*Play areas can be incorporated into the common areas.*



*Pools, spas and other recreational amenities are encouraged for larger projects.*



- c) In mixed-use projects, accessible rooftop open space may be used as common useable open space by the residents. It may also be used for private useable open space, when the space is directly accessible from the unit(s).

#### 11. Fences, walls, gates and hedges.

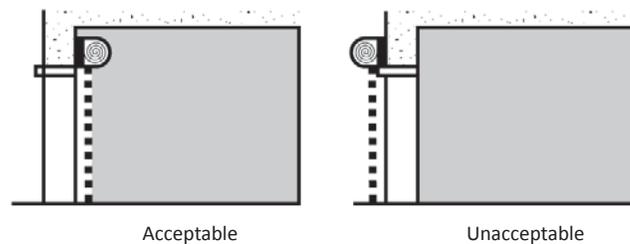
- a) Walls and fences shall be planned and designed as integral parts of all projects as necessary to screen vehicles, loading and storage areas, and utility structures, and shall be consistent with the landscaping and building design. Decorative types of view fencing, such as wrought iron, are generally encouraged for street fencing.

#### 12. Exterior lighting.

- a) Exterior lighting shall be used to provide illumination for the security and safety of on-site areas such as building entrances, parking, loading, shipping and receiving, walkways, and working areas. The design of light fixtures and their structural support shall be architecturally compatible with the main buildings on-site.
- b) Site, parking lot and building security lighting shall not impact surrounding or neighboring properties. The type and location of such lighting shall preclude direct glare onto adjoining property, streets, or skyward. All lighting fixtures must be shielded to confine light spread on-site.
- c) In mixed use projects, all site, parking lot and building security lighting shall be appropriately shielded so as not to spill over into or otherwise adversely impact the residential portion of a mixed-use development.

#### 13. Loading areas.

- a) Loading areas should be designed to prevent interference with vehicular circulation and parking, and to provide an unobstructed area for trucks to maneuver when accessing loading spaces.
- b) Loading areas should be located away from main customer entrances and the street, preferably toward the rear of the property.
- c) Overhead (roll-up) doors shall not be directly open to public view, and shall be substantially screened from the street and residentially zoned properties. Screening may be accomplished in a variety of ways, including the use of wing walls, the recessing of overhead doors (building articulation), landscaping, or a combination of these techniques. Fixed hardware for roll up doors shall be located on the inside of buildings to minimize visual clutter. If located adjacent to a residential use, the design of overhead doors shall minimize noise through devices such as rubber seals and/or other dampening features.



**Figure 3-8: Roll-up doors**

**14. Outdoor storage and service areas.**

- a) Outdoor storage and service areas (including, but not limited to, service entrances, loading docks and bays, outdoor storage of commercial vehicles) shall be clearly defined and designated for convenient access. They shall not conflict with vehicular access, on-site parking facilities, pedestrian walkways, and customer entrances.
- b) All outdoor storage and service areas for commercial uses shall be located so as to be convenient to the commercial users and where associated odors and noise will not adversely impact the residential units in a mixed-use project, or adjacent residential uses.
- c) Outdoor storage and service areas shall not be visible from public right-of-ways and shall be screened from on-site and off-site public view with a combination of building features, decorative walls and landscaping consistent with the architectural style of the building.

**15. Refuse and recyclable collection facilities.**

- a) Refuse and recyclable storage shall be enclosed within or adjacent to the main structure or located within separate freestanding enclosures. The location of refuse and recyclable collection facilities shall be coordinated with the location of loading/service areas, and not be readily visible to public view.
- b) In mixed use developments, all refuse collection facilities for commercial uses shall be located so as to be convenient to the commercial users and where associated odors and noise will not adversely impact the residential uses.
- c) Screening for refuse and recyclable collection facilities shall use a combination of building features, decorative walls and landscaping consistent with the architectural style of the building.

### 3.15 Design Guidelines for Multi-Family Residential Uses

The purpose of this section is to facilitate high quality multifamily residential development within Willowbrook TOD Specific Plan area. These design guidelines address site planning and building design and are in addition to the above land use regulations and development standards for multifamily uses in this chapter.

#### 1. Scale and mass.

- a) Multi-family residential projects should avoid massive apartment-like structures. Multi-family units in large projects should be clustered, or broken up into groups of structures. Windows should be provided facing the street for all units adjacent to the street to break up massing and to contribute to “eyes on the street” which helps neighborhood security.

#### 2. Building Orientation and Entrances.

- a) Multi-family developments should have a strong street presence with public entrances oriented towards the street. The majority of unit entrances must be accessed from the street or from the main open space. Where there is a common building entrance for all units, it should be accessed from the street or main open space.
- b) Multi-family developments should have transitional spaces in the form of stoops, overhangs and porches between public areas and entrances to the units.

#### 3. Building articulation and detailing.

- a) Multi-family building design should avoid large monotonous facades, long straight line building fronts, plain box shapes, and barren exterior treatment. All building elevations visible from a public right-of-way should be articulated and incorporate the chosen design theme in a consistent manner.
- b) Multi-family building facades should be designed so as to give individual identity to each vertical module of residential units, using techniques such as providing a deep notch between the modules; varying architectural elements between units (e.g., window color, roof shape, window shape, stoop detail, railing type); providing porches and balconies; varying color or materials of each individual module within a harmonious palette of colors and materials, etc.



*Articulate buildings with architectural detailing, staggered building planes, material and color changes, and varied roof lines.*

**4. Privacy for residences.**

- a) Multi-family buildings should be oriented to promote privacy to the greatest extent possible. Windows, balconies or similar openings above the first story should be oriented so as not to have a direct line-of-sight into adjacent units within the development.
- b) Multi-family units above the first story should be designed so that they do not look directly onto private patios or backyards of adjoining residential property or units. Landscaping should be used to aid in privacy screening in ground floor units.

**5. Vehicle circulation and access.**

- a) Site access and internal circulation in multi-family developments shall be safe, efficient, and convenient. Vehicular traffic shall be adequately separated from pedestrian circulation. Vehicular entrances shall be clearly identified and easily accessible to minimize pedestrian/vehicle conflict.
- c) The number of site access points or driveway aprons should be minimized for aesthetic purposes, to achieve efficient and productive use of paved accessways, and to eliminate traffic hazards. They should be located as far as possible from street intersections and be coordinated with existing or planned median openings and driveways on the opposite side of the roadway.

**6. Parking.**

- a) Parking for multi-family developments shall be provided on site in on-grade or underground structures, parking courts, carports, or attached garages. Adverse visual impacts from parking lots, carports, structures, and garage doors on the residential character of the street or project site shall be minimized through proper siting and design.

**7. Pedestrian Circulation.**

- a) Pedestrian circulation shall be adequately separated from vehicular traffic. Pedestrian entrances and walkways should be clearly identified and easily accessible to minimize pedestrian/vehicle conflict.
- b) Pedestrian walkways should link dwelling units with common open space and recreation areas, parking areas, and the street at the project entries. Where possible, multi-family developments shall incorporate pedestrian connections to adjoining residential and commercial projects and other compatible land use facilities.
- c) Pedestrian walkways shall be safe, visually attractive, and well defined by landscaping and lights.

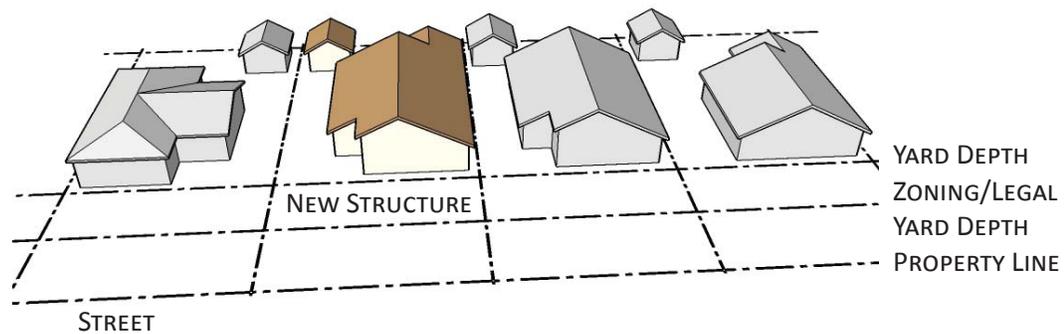
**8. Exterior lighting.**

- a) Every multi-family project should have adequate lighting to provide for security and visibility, particularly along walkways and driveways, entrances to parking areas, and open space areas.
- b) Site lighting shall not impact surrounding or neighboring properties. The type and location of site and building lighting shall preclude direct glare onto adjoining property, streets, or skyward. All lighting fixtures must be shielded to confine light spread on-site.
- c) The design of all exterior light fixtures should be compatible with the building's architecture.

### 3.16 Design Guidelines for Single Family Residential Uses

The purpose of this section is to facilitate high quality single family residential development within the Willowbrook Residential 1 and Willowbrook Residential 2 Zones. These design guidelines address site planning and building design and are in addition to the above land use regulations and development standards for the Willowbrook Residential 1 and Willowbrook Residential 2 Zones.

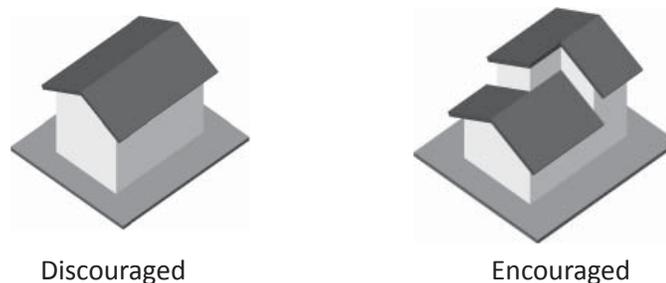
- 1. Yard areas.** While meeting the minimum front and side yard depths established in Chapter 22.18 of Title 22, a new dwelling's front and side yard depths should approximate that of adjacent residences. The front and side yard pattern on the block shall be identified and respected to help unify the neighborhood.



*New structure respects the yard depths established along a street*

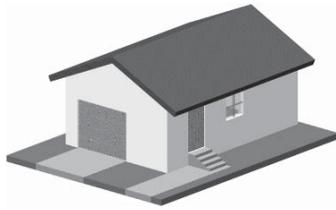
**Figure 3-9: Yard depths**

- 2. Scale and mass.** The scale and mass of a new dwelling should be similar to that of neighboring houses and not overwhelm them with disproportionate size or a design that is out of character.
- 3. Height and rooflines.** Height and rooflines should be consistent with the style of architecture of the house, and complement qualities of neighboring residential structures such as type, slope, size, material and color.
- 4. Additions, accessory buildings and second units.** Additions, accessory structures and second units should maintain the look and appearance of the existing primary structure so that they do not appear as an addition or new building. They should respect the architectural style, scale, rhythm, and building elements of the existing primary structure. An addition should complement and balance the overall form, mass, and composition of the existing primary structure on the property. An addition shall maintain the same floor-to-floor height of the original structure.

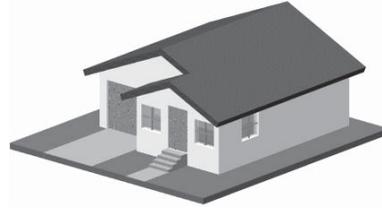


**Figure 3-10: Massing**

5. **Orientation.** Dwellings shall orient themselves to the street with a clearly identifiable front door and windows that face the street. Front windows and the front door contribute to “eyes on the street” which helps neighborhood security.



Discouraged



Encouraged

*Orient the front of the house and entry to face the street*

**Figure 3-11: Orientation**

6. **Privacy for residences.** New houses and/or accessory buildings should minimize their visual impact on adjacent properties. New windows and second floor balconies should be placed where they promote maximum privacy between properties. Windows should be offset or staggered from neighboring windows to maintain privacy between houses. Avoid locating balconies so that they look directly overlook a neighboring patio or yard.
7. **Exterior lighting.** Exterior lighting includes all lighting fixtures on front facades, security lighting, and landscape lighting. Adequate exterior lighting shall be provided on the front of the house to ensure neighborhood safety and security. Recessed porches shall be lit. Exterior lighting shall be positioned so that no direct light extends into neighboring properties or public rights-of-way.

### 3.17 Sustainable Design Guidelines

Building green means reducing the use of resources, minimizing harmful impacts to the environment, and creating healthier environments for people. Green buildings can incorporate both passive, low-tech design, such as daylighting; and active, high-tech strategies and systems, such as photo voltaic panels. To be most effective, green building strategies should be incorporated into all phases of a project from early programming and budgeting, to design and construction, to commissioning, operations, and maintenance and post-use demolition/recycling.

#### Site Design and Passive Solar Design

- Buildings should be sited and designed to maximize the use of sunlight and shade for energy savings, and respect the solar access of adjacent buildings.
- Buildings should be clustered for shade, and incorporate protective courtyards, recessed windows and doors, and insulated walls.
- To reduce energy use, the east and west walls of buildings should be shaded with evergreen trees to reduce summer heat gain. South walls should be shaded with deciduous trees.
- Walkways and plazas should be designed to collect stormwater, where feasible.

#### Water Efficiency

- To reduce water use and maintenance costs, the majority of plant materials should be drought tolerant and require relatively low maintenance.



*Landscape materials should be drought-tolerant and low maintenance. Pervious, non-vegetative ground cover interspersed with native plantings can provide a sustainable, attractive landscaped area.*

### Building Design

- The provision of a green roof should strongly be considered to reduce solar gain and to reduce the quantity of water entering the storm drain system.
- Solar panels on roofs should be considered to capture solar energy for internal use of the project.
- Arcades, covered walkways, trellises and passages should be incorporated to provide sheltered areas for pedestrian circulation, as well as to shade the buildings to reduce energy usage.



*Green roofs serve several purposes for a building, such as absorbing rainwater, providing insulation, and helping to lower urban air temperatures and mitigate the heat island effect. Parking structures and other buildings offer potential for solar roofs, which capture sunlight and transform it into energy.*

### 3.18 Street and Sidewalk Standards and Guidelines

The Specific Plan area has a mix of commercial, institutional and residential uses. While both paved sidewalks and sidewalks with parkways can be found within the area, this Specific Plan differentiates the sidewalk treatment between residential and non-residential frontages as follows:

- All existing parkways facing residential uses shall be retained.
- All other street segments (with institutional, commercial or mixed use zone frontages) shall have paved sidewalks instead of parkways.



*Existing parkways along residential frontages include turf and drought tolerant landscaping.*



*Existing sidewalks along non-residential parkways have a mix of paving materials.*

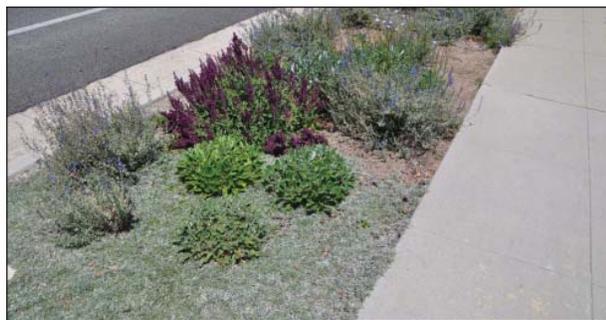
Each property owner is responsible for the maintenance of the parkway that fronts their lot. Drought-tolerant landscaping is encouraged in the residential parkways.



*Sidewalks along non-residential frontages should have attractive paving materials and patterns. Tree wells can also become a part of a Low Impact Development strategy.*

As new development occurs in the mixed use zones, the adjoining property owner/developer shall follow the following standards and guidelines for upgrading roadways and sidewalks. Property owners in other areas are also encouraged to following these standards and guidelines.

- Standards in Section 22.84.070.F. in Chapter 22.84 (Transit Oriented Districts) shall apply to all new or reconstructed streets and sidewalks.
- New or reconstructed roadways and sidewalks shall be designed in accordance with Public Works' standards for engineering construction, LID (Low Impact Development) and Green Streets recommendations.
- Green Streets and Low Impact Development (LID) strategies, such as the use of vegetated swales and decomposed granite, shall be utilized to manage stormwater, improve water quality, reduce flows and enhance watershed health.
- New sidewalks should be graded to allow stormwater run-off into adjacent unpaved areas, such as planter strips and parkways.
- Sidewalks should be designed or modified such that street furniture, signage, lighting, landscaping, etc. do not interfere with a continuous pedestrian path. The pedestrian path should be clearly identified in project plans.
- Street treatments in which stormwater runoff is captured and retained shall be located within street medians and parkways. Stormwater Best Management Practices per the California Stormwater Quality Association's Stormwater Best Management Practice Handbook shall be applied.
- Decorative non-vegetative groundcover, such as river cobble, pebbles, and crushed stone, should be used in streetscape composition. Non-vegetative groundcovers shall allow proper drainage. Any material, such as crushed tires, that may leach harmful chemicals into the soil or generate heat is not permitted.
- Sidewalks shall comply with all ADA standards.



*Drought-tolerant landscaping in the parkways is an attractive replacement for turf.*

## 4.1 Introduction

This Chapter describes the Mobility Strategy for the Willowbrook TOD Specific Plan, which includes recommendations for the roadway network as well for improving pedestrian, bicycle and transit circulation.

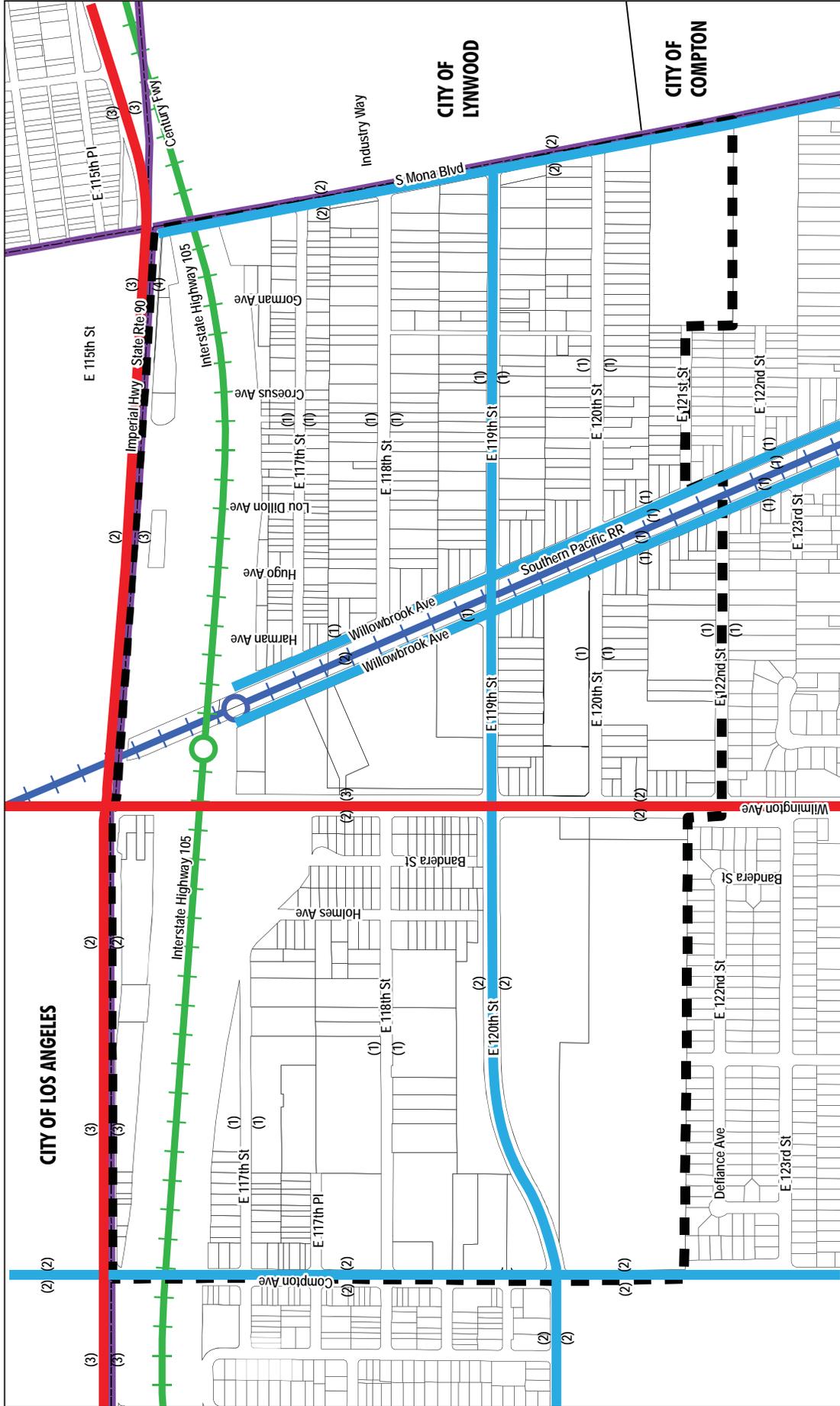
The overall goal for mobility in the Specific Plan area is to enhance connectivity and the ease of movements for non-auto transportation modes, particularly pedestrians and bicyclists. These enhancements will support healthy transportation options.

## 4.2 Background

The Willowbrook TOD Specific Plan area is centered on the Willowbrook/Rosa Parks Metro Station with stops for both the Metro Blue Line and the Metro Green Line.

Generally, the existing circulation system in the area is automobile-oriented and auto-dominated. The rail station is generally difficult to access, and the station area is inhospitable to transit users. There are however numerous shuttles and bus routes serving the Specific Plan area and connecting to the rail station.

The local community has expressed concerns over the high speed of vehicle traffic on key roadways and the poor pedestrian environment at most intersections and on many of the sidewalks. There are few locations to conveniently cross streets, intersections without a clear roadway and pedestrian markings, and a poor pedestrian sidewalk environment – particularly on the east side of Wilmington Avenue by the Willowbrook/Rosa Parks Station. There is a lack of pedestrian infrastructure in some areas, with unpaved paths and poor lighting. Furthermore, there is a lack of bicycle infrastructure on streets in the Specific Plan area, and inadequate bicycle parking.



**Figure 4-1  
Street Network**

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Major Roadway
- Secondary Roadway
- (x) Number of Lanes

**WILLOWBROOK TOD SPECIFIC PLAN**



### 4.3 Roadway Network

The roadway system, shown on Figure 4-1, provides the backbone circulation system for all modes of transportation. While historically street systems have been oriented to serving the automobile, the roadway system should provide a network of complete streets that provides for the safe and efficient circulation of transit, bicycles, and pedestrian as well as automobiles.

The existing street system will be largely maintained in its current configuration, with some changes/improvements designed to improve access and circulation, and walkability. The Major Roadways (as identified in the General Plan) will continue to be Wilmington Avenue and Imperial Highway. Secondary Roadways (as identified in the General Plan) will continue to be Compton Avenue, Willowbrook Avenue and Mona Boulevard in the north-south direction, and 120th Street/119th Street in the east-west direction. The number of traffic lanes and roadway lane configurations will generally remain the same, except where otherwise noted in the Specific Plan (for example where road diets will be implemented).

The following street enhancements, shown in Figure 4-2, are intended to improve circulation for bicycles and pedestrians in the Specific Plan area. These are also described in more detail in sections 4.4 and 4.5.

#### **Road Diet and Bicycle Lanes on 120th Street**

The section of 120th Street between Compton Avenue and Wilmington Avenue will be reduced from four lanes to three lanes, with a bicycle lane in each direction. This is part of the Willowbrook Area Access Improvement project.

#### **Willowbrook Avenue West**

The section of Willowbrook Avenue West between the Rosa Parks Metro Station and 119th Street, will be reduced from two lanes southbound to one lane southbound, and a bike path installed on the west side of the street.

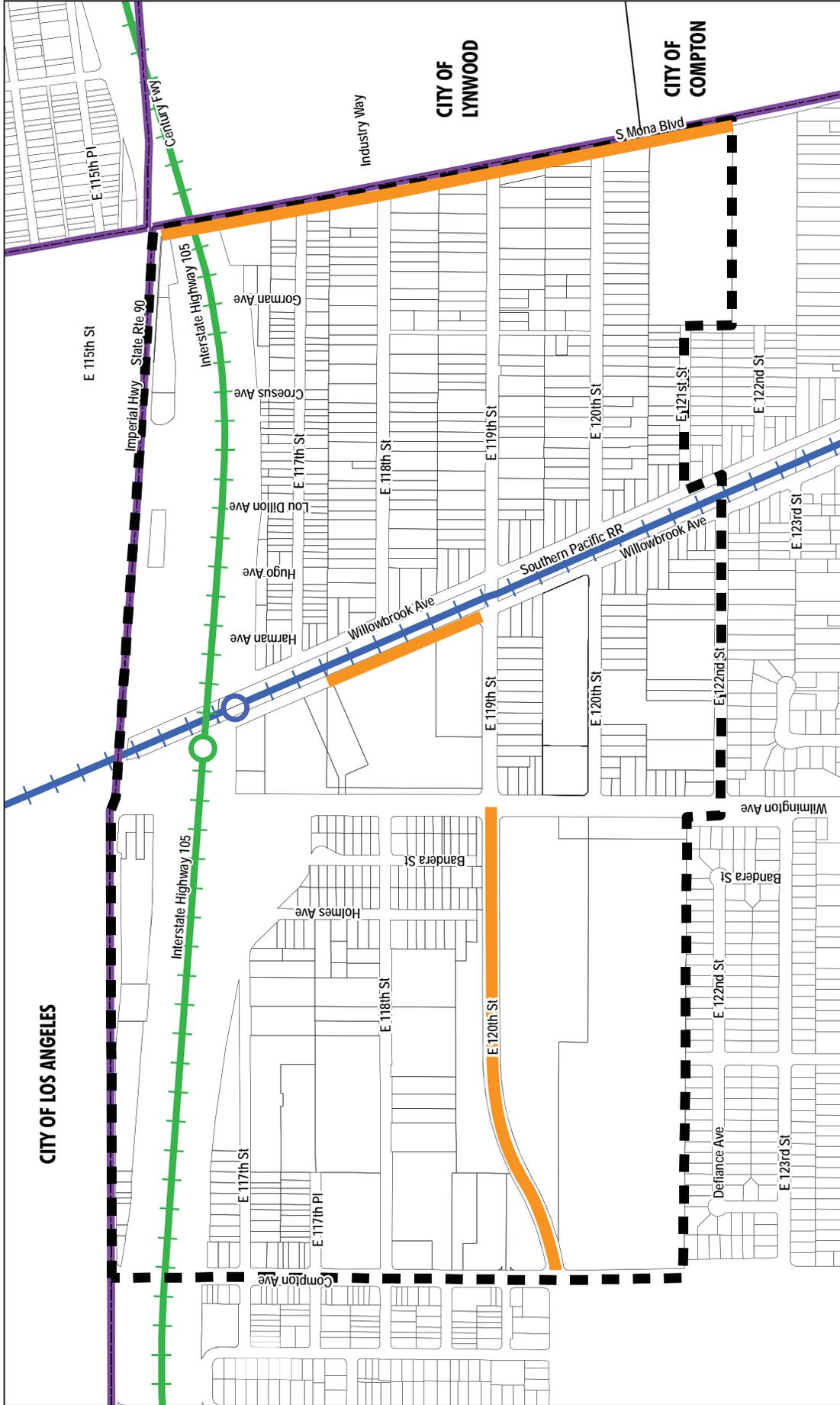
#### **Road Diet and Bicycle/Pedestrian Trail on Mona Boulevard**

Mona Boulevard from the I-105 Freeway to 124th Street will be converted from a four lane street to a three lane street, and a pedestrian/bicycle trail installed on the west side of the street. The pedestrian/bicycle trail shall include a paved section to meet existing Class I or upcoming Class IV bikeway design guidelines. The design of the trail will be subject to the requirements of Los Angeles County Department of Public Works (LACDPW). Figure 4-3 illustrates a typical cross section of reconfigured Mona Boulevard.

The majority of properties on the west side of Mona Boulevard are accessed from the east-west streets. However, as there are some driveways and curb cuts on Mona Boulevard (approximately twelve along the 3,000 foot section), the pedestrian/bicycle trail will need to be designed to safely accommodate those driveways, with adequate signage and surface material details. (It is noted that typical street conditions often require driveway access across sidewalks and across bike lanes).

#### **Rename 120th Street between Compton and Wilmington Avenues**

Currently, the segment of 120th Street west of Wilmington Avenue (between Compton and Wilmington Avenues) aligns with 119th Street east of Wilmington Avenue, which causes confusion. This segment of 120th Street, which fronts the MLK Medical Center, should be renamed. A community contest should be organized to select an appropriate street name, similar to that for “Healthy Way” on the Medical Campus.

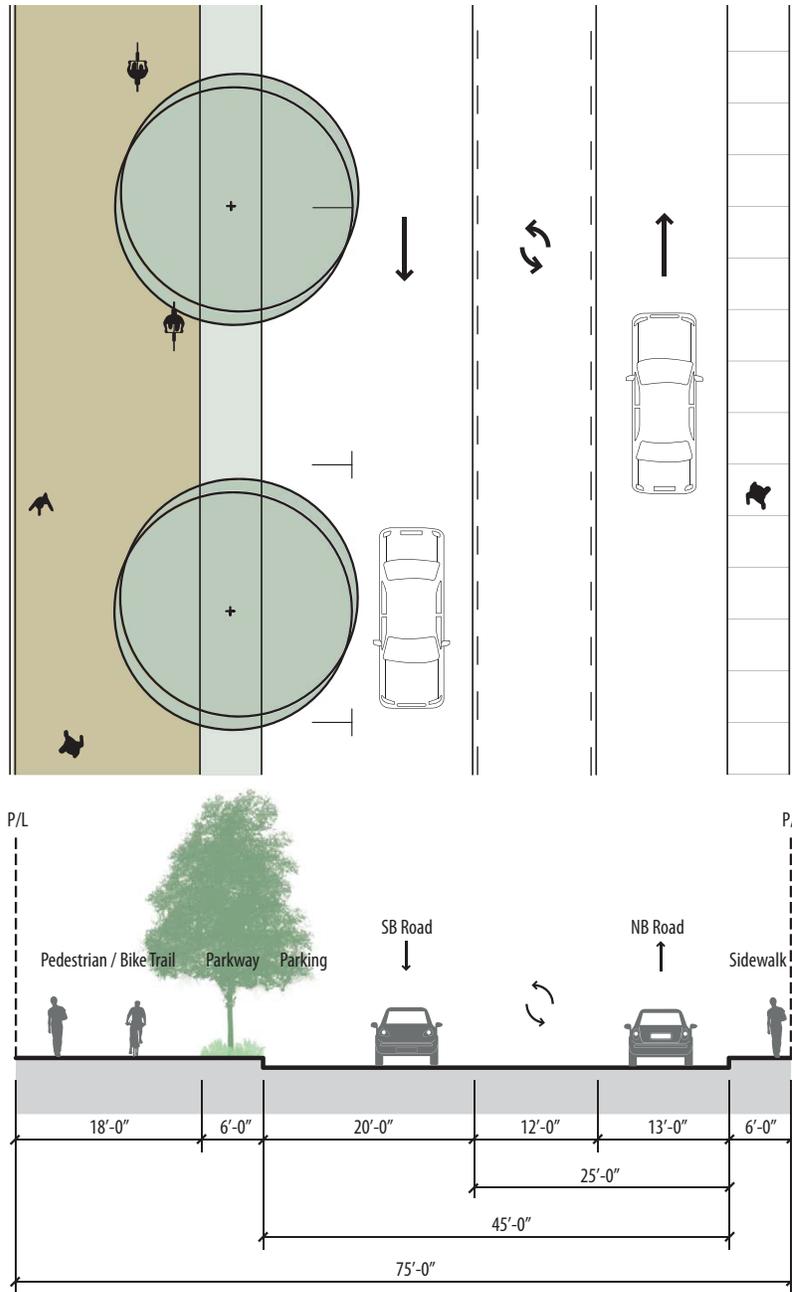


**Figure 4-2  
Street System Changes -  
Road Diets**

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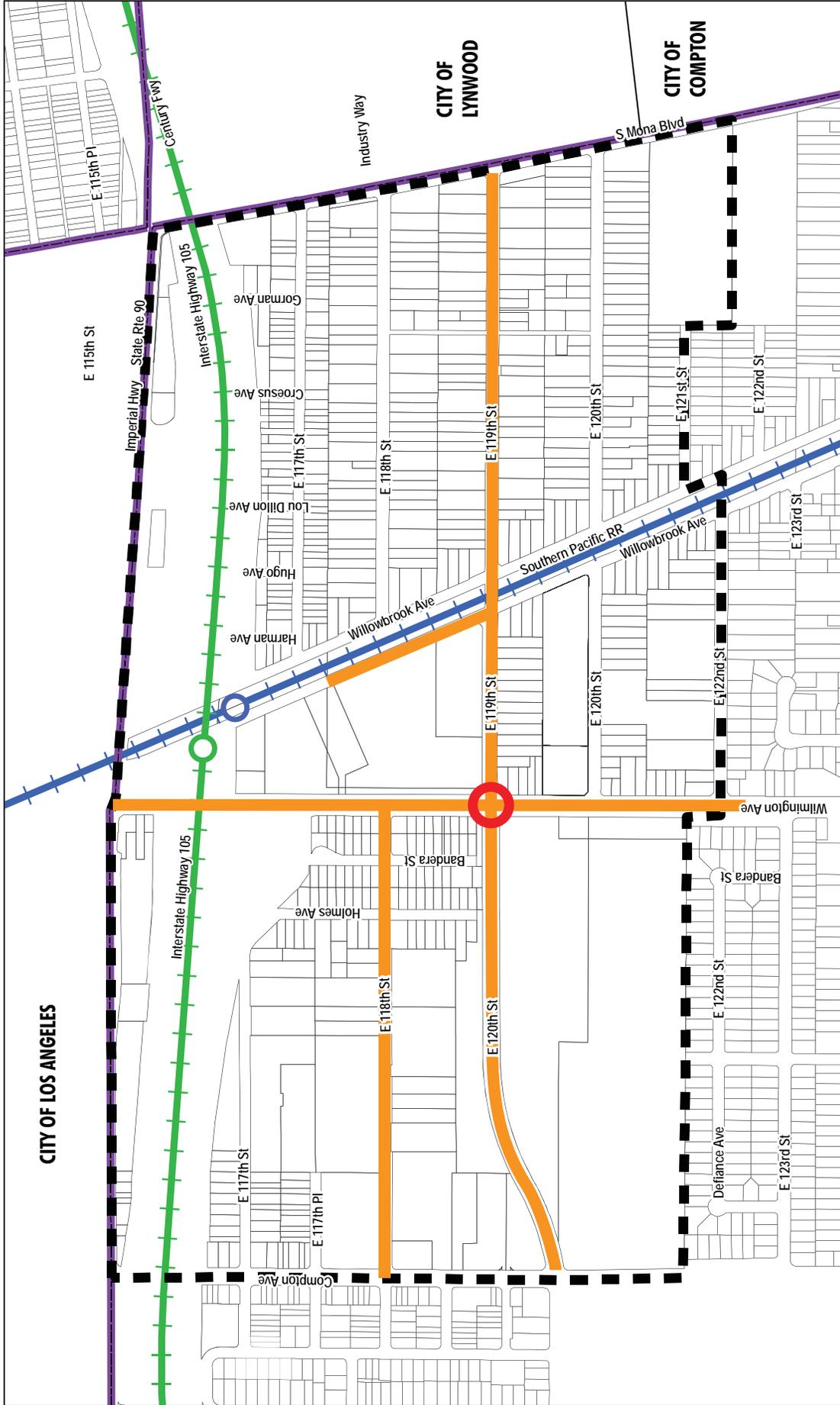
-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  Reduce Lanes



**Figure 4-3: Proposed Mona Boulevard Section**



*Examples of a Class I bicycle path and pedestrian trail*



**Figure 4-4  
Key Pedestrian Routes**

-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  Key Pedestrian Routes
-  Pedestrian Hub

**WILLOWBROOK TOD SPECIFIC PLAN**



## 4.4 Pedestrian Circulation

The key pedestrian routes in the Specific Plan area are shown in Figure 4-4. The backbone of the pedestrian system is formed by Wilmington Avenue in the north-south direction and 120th/119th Street in the east-west direction. These corridors connect activity centers of the Willowbrook/Rosa Parks Station, the Kenneth Hahn Plaza, and the Martin Luther King Jr. Medical Center Campus. They also cross at the intersection of Wilmington Avenue and 120th/119th Street – which is the functional pedestrian hub of the Specific Plan Area. Section 5.2C in Chapter 5 discusses and illustrates concepts for special treatment for this intersection.

Additional key elements of the pedestrian system are 118th Street between Compton Avenue and Wilmington Avenue – which connects the Charles Drew University campus to the rest of the Specific Plan Area, Willowbrook Avenue West between 119th Street and the Willowbrook/Rosa Parks Metro Station – providing access from residential areas to the station, and 119th Street between Willowbrook Avenue and Mona Boulevard – which provides access from the residential areas to the activity centers within the Specific Plan area. Mona Boulevard also provides north-south pedestrian access on the east side of the Specific Plan area including access to Mona Park, the Martin Luther King Elementary School, and the Dr. Ralph Bunche Middle School.

### Pedestrian Sidewalk Improvements

Sidewalks currently exist on most streets in the Specific Plan area, although some are narrow or substandard in quality. Sidewalks will be improved as new development occurs, and through the landscaping provisions contained in this Specific Plan.

Specific recommendations include:

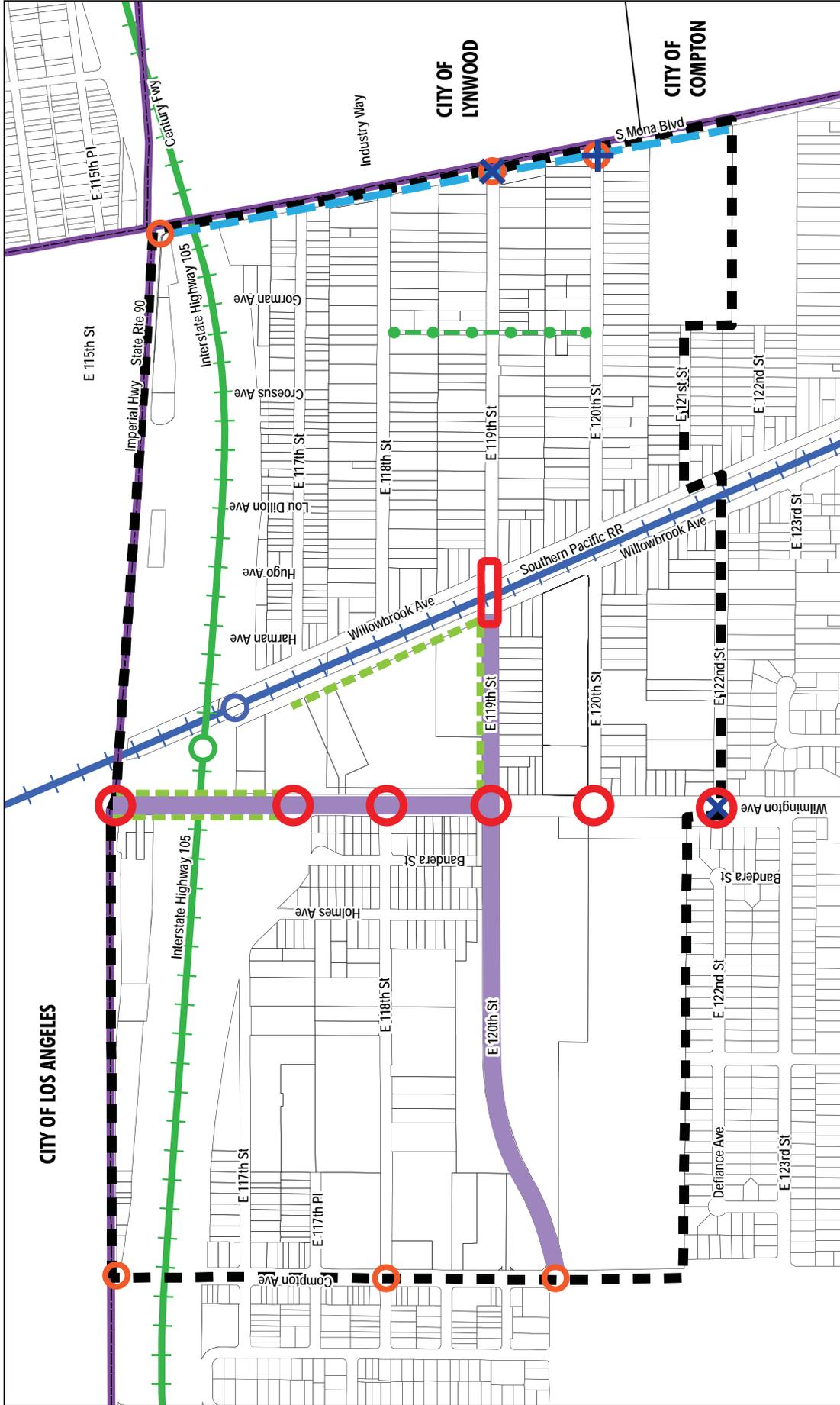
- A sidewalk will be added to the currently unpaved west side of Willowbrook Avenue West between the Metro Station and 119th Street.
- The sidewalks on Wilmington Avenue between the I-105 Freeway Off-Ramps and Imperial Highway will be improved through widening and streetscape improvements, including improved street lighting.
- Streetscape standards will provide for wider sidewalks where feasible as redevelopment occurs, for example the north side of 119th Street between Wilmington Avenue and Willowbrook Avenue.

### Pedestrian Oriented Intersection Improvements

In order to enhance the pedestrian environment and to calm traffic, a number of pedestrian oriented intersection improvements will be implemented throughout the Specific Plan area. These will be based on a menu of improvements that includes the following:

- Adding high visibility crosswalks at intersections.<sup>1</sup>
- Adding passive pedestrian detection and pedestrian push buttons for crosswalks at traffic signals at intersections.
- Adding pedestrian countdown pedestrian signals and audio signals to crosswalks at intersections.
- Adding advance stop bars to signalized intersection approaches.
- Adding sidewalk bulb-outs and extensions, or reducing curb returns, on intersection corners where feasible.

<sup>1</sup> Design of any high visibility crosswalks should consider ADA access, maintenance cost, and feasibility. Pavers or textured surfaces should not be considered in the pedestrian walking path of the crosswalks.



**Figure 4-6  
Pedestrian Improvements**

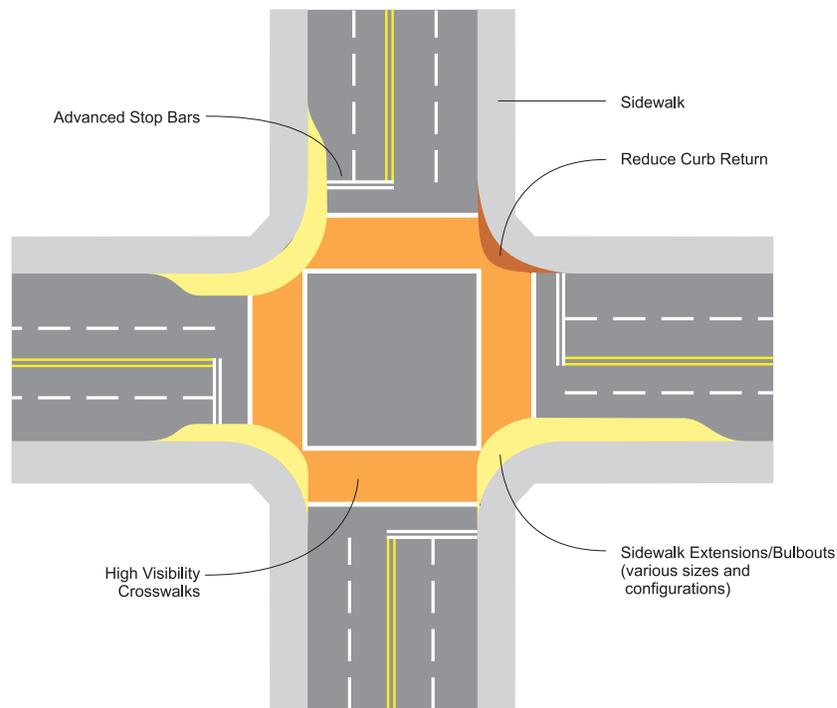
- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- New Traffic Signal
- New Pedestrian Signal
- Pedestrian Improvements - Key Location
- Pedestrian Improvements - Key Location
- Add Pedestrian Trail
- Pedestrian Improvements - Secondary Location
- County Willowbrook Area Access Improvements
- County Willowbrook Green Alley

**WILLOWBROOK TOD SPECIFIC PLAN**



For all bulb-out locations where two curb ramps are proposed per corner, curb ramps shall be situated close enough to the intersection so that pedestrian visibility is not hindered. Any proposed landscaping shall not interfere with sight distance of pedestrians waiting to cross. The intersection geometrics shall be evaluated to determine if bulb-outs can fit within the proposed curb to curb width.

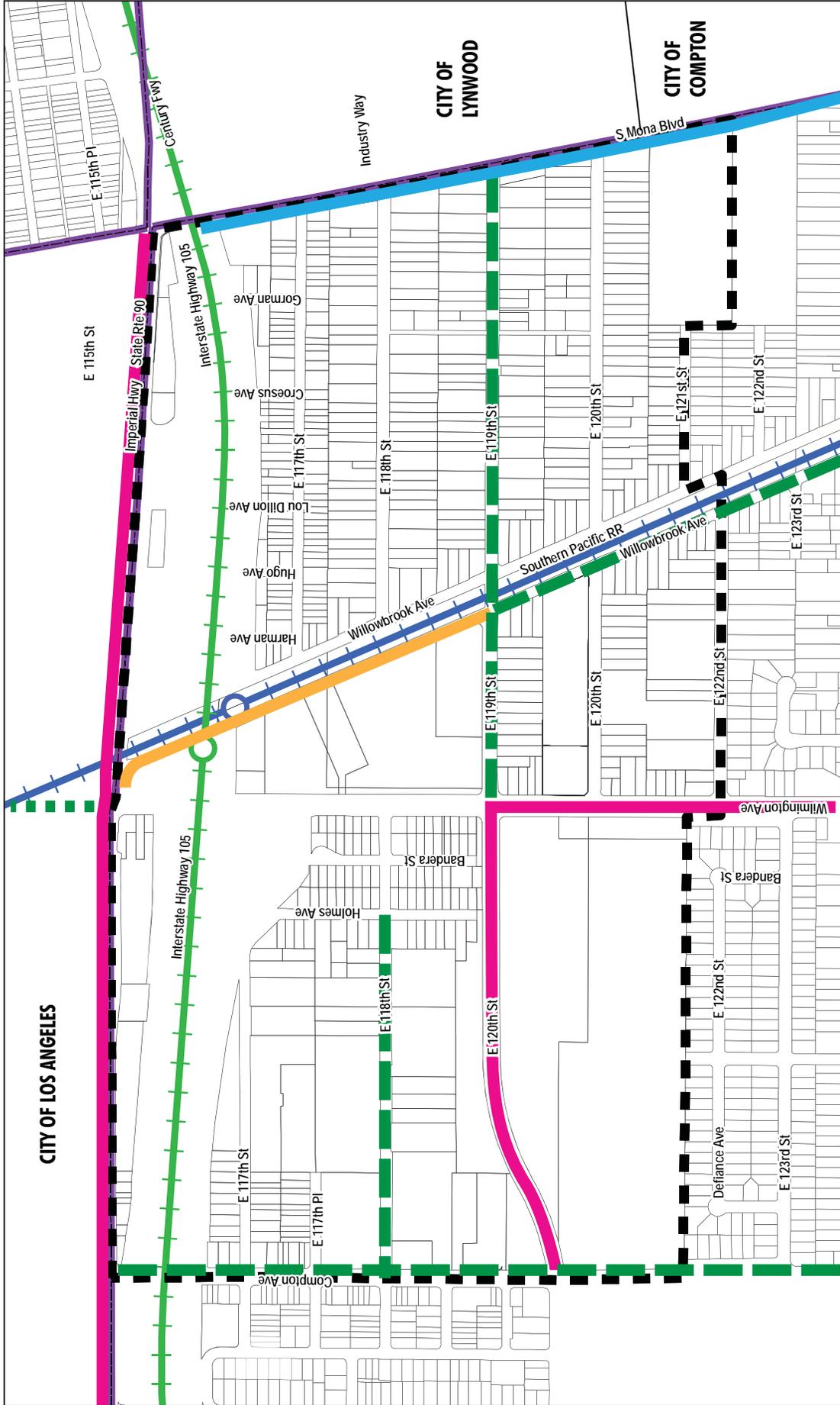
These measures will facilitate pedestrian circulation, by reducing the width of roadway for pedestrians to cross, providing additional sidewalk space, and making pedestrian crossings more visible to pedestrians and motorists. A typical diagram is illustrated in Figure 4-5.



**Figure 4-5: Pedestrian oriented intersection improvements**

The locations for such improvements are shown in Figure 4-6, and listed below.

- Wilmington Avenue and Imperial Highway
- Wilmington Avenue and I-105 Eastbound Ramps
- Wilmington Avenue and 118th Street
- Wilmington Avenue and 120th/119th Streets
- Wilmington Avenue and 120th Street
- Wilmington Avenue and 122nd Street
- Willowbrook Avenue West and 119th Street
- Willowbrook Avenue East and 119th Street
- Mona Boulevard and Imperial Highway
- Mona Boulevard and 119th Street
- Mona Boulevard and 120th Street
- Compton Avenue and Imperial Highway
- Compton Avenue and 118th Street
- Compton Avenue and 120th Street



**Figure 4-7  
Bicycle Network**

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Class I Bike Path
- Class II Bike Lane
- Class III Bike Route
- Class IV Bikeway or Class I Bike Path
- Bike Friendly Street

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The type of improvements will follow the concepts identified in the “Los Angeles County Transit Oriented Districts Access Study, SCAG and County of Los Angeles.” The specific improvements to be implemented at each location will be determined following detailed design studies to determine applicability and feasibility and the ultimate configuration. However, curb extensions should not restrict the circulation of buses, trucks, emergency vehicles, and bicycles.

The improvements at Wilmington Avenue and I-105 eastbound ramps will add a crosswalk across Wilmington Avenue to facilitate access to the Willowbrook/Rosa Parks Metro Station. If found to be warranted by LACDPW analyses, other specific improvements include:

- New traffic signals at Wilmington Avenue and 122nd Street, and at Mona Boulevard and 119th Street, to facilitate pedestrian crossings on long stretches of both streets currently without signalized crosswalks.
- A signalized pedestrian crosswalk on Mona Boulevard and 120th Street, to facilitate pedestrians crossing to the Dr. Ralph Bunche Middle School.

## 4.5 Bicycle Circulation

The Specific Plan Bicycle Network, shown in Figure 4-7, includes a combination of Class I, Class II, Class III and Class IV facilities that connect activity centers and neighborhoods to the rail station, connect to adjacent communities, and provide a dedicated network for bicyclists to use safely and efficiently.

### Bicycle Facilities

- A Class I Bike Path consists of an exclusive bicycle facility, separated from roadways and traffic.
- A Class II Bicycle Lane consists of a designated striped lane along the curb lane of a street. It provides for one-way travel and is generally delineated with special striping and signage.
- A Class III Bicycle Route is for shared use of bicycles with automobile traffic. Signs are posted which indicate the road also serves as a bike route, but no special lane for bicycles is striped. There may however be painted bicycle symbols on the roadways surface (known as sharrows) to indicate to motorists that bicycles also use the street.
- A Class IV Bikeway, or cycle track, provides a right-of-way designated exclusively for bicycle travel within or adjacent to a roadway and which is protected from vehicular traffic. Types of separation include, but are not limited to, grade separation, flexible posts, inflexible physical barriers, or on-street parking.

Specific recommendations include:

- Class I bike paths will be implemented on Willowbrook Avenue West between 119th Street and Imperial Highway — to provide access to the rail station, and on Mona Boulevard (west side) between Imperial Highway and 124th Street. The facility on Willowbrook Avenue West could also be a Class IV cycle track.
- Class II bike lanes will be implemented on 120th Street between Compton Avenue and Wilmington Avenue, on Wilmington Avenue between 124th Street and 120th Street, and on Imperial Highway between Compton Avenue and Mona Boulevard.



**Figure 4-8  
Key Transit Streets**

-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  Transit Streets (Multiple Routes)

**WILLOWBROOK TOD SPECIFIC PLAN**



- Not all streets can support bicycle lanes due to insufficient width or the presence of on-street parking, which is also an important asset to the function and economic well-being of the adjacent commercial uses or neighborhoods. Where there is insufficient roadway width to stripe bicycle lanes and/or to retain on-street parking, a connected network is achieved through the designation of Class III Bike Routes. Class III bike routes will be implemented on Compton Avenue, Willowbrook Avenue West south of 119th Street, 119th Street between Wilmington Avenue and Mona Boulevard, and on 124th Street throughout the Specific Plan area.

The Bicycle Circulation System includes elements from the County's Bicycle Plan and the City of Los Angeles Bicycle Plan.

### **Bicycle Amenities**

Bicycle parking can include bike racks and lockers, bike corrals (larger areas dedicated to bike parking), and at bike stations. Bike stations can include repair facilities and a small bike shop, as well as secure bike parking. Bike share programs can provide bikes for sharing by the community where people can rent a bike for a period of time and pick it up or drop it off at any bike share stop in an area.

#### **Bicycle Parking and Stations**

Bike parking should be provided according to the Title 22 of the Los Angeles County Municipal Code for all new developments. Additional bike parking should be provided at key public locations to also serve existing uses. This includes the Kenneth Hahn Plaza and the Willowbrook/Rosa Parks Metro Station. Metro is planning to provide a bike station at the rail station. Bike stations should also be provided on the Martin Luther King Jr. Medical Center Campus and the Charles Drew University Campus, and in the Northwest Quadrant area of office uses. These additional bike stations should be provided by those facilities and uses.

#### **Bike Share Program**

A Bike Share Program should be implemented in the Specific Plan Area to encourage the use of bicycling – which would reduce vehicle trips and provide a healthy alternative method of transportation. It would also improve the first and last mile connections to the rail station. While such a program could primarily be internal to the Specific Plan area, it should also be coordinated with any other existing or planned bike share programs in adjacent areas or the County.

## **4.6 Transit Circulation**

### **Station Access**

Metro is currently working on improvement plans for the Willowbrook/Rosa Parks Station. These are expected to include enhancements to the station facilities and platforms, the provision of a Shuttle Stop and Bike Station, and improvements to station access, including a crosswalk across the tracks south of the station to Willowbrook Avenue East.

### **Transit Service**

Key transit streets (with multiple bus routes) in the Specific Plan area are shown in Figure 4-8. It is anticipated that current bus routes will continue to serve the Specific Plan area focusing on the rail station.

### Shuttle Routes

Existing shuttle routes that are operated by the County, the Martin Luther King Jr. Medical Center and Charles Drew University should be continued to facilitate alternative modes of transportation, and provide critical access to the Medical Center for those without a car.

Additional shuttle routes should be added to serve new development in the Northwest Quadrant and connect the land uses to the Willowbrook/Rosa Parks Metro Station. These new shuttle services could be provided by the private sector as part of a comprehensive Transportation Demand Management Program (see Section 4.8 below).

The potential combination of the various shuttle routes could be considered, which improves the overall effectiveness of service and avoids duplication between routes, as long as the needs of individual land uses and institutions continues to be met.

## 4.7 Transportation Demand Management

A Transportation Demand Management Program will be developed by the County to take advantage of the high level of transit service, and to reduce both vehicle trips and the number of parking spaces provided. Such programs include the encouragement of use of transit, bicycling, walking, and ridesharing. These types of programs are generally most suitable and most effective for large employers, institutional uses, and office uses and could be attractive to employers in new office type land uses in the Northwest Quadrant as they could reduce the capital costs needs of building parking. The Program should include the Northwest Quadrant, Charles Drew University, and the Martin Luther King Jr. Medical Center.

Transportation demand management and trip reduction strategies could include but not be limited to:

- Encouraging use of transit, including subsidizing transit passes
- Parking cash out programs
- Encouraging rideshare
- Providing preferential parking for carpools
- Facilitating formation of carpools and vanpools
- Site and building design to facilitate use of transit, bicycling and walking

A Transportation Management Organization (TMO) could also be established to facilitate these programs at an area wide level and support individual employers and/or buildings in participating to the fullest extent possible.

# Streetscape Improvements

## 5.1 Introduction

The purpose of this chapter is to describe recommended streetscape improvements in the Specific Plan area that serve to fulfill the Specific Plan goal of improving the quality of life for residents with improvements to the public realm. Some of these recommendations are conceptual and the County will need to further assess and supplement these recommendations, as noted, on a case-by-case basis.

Streetscape improvements are intended to enhance and unify the visual and spatial experience of the driver, pedestrian, and the bicyclist, and help provide key linkages between the activity centers and neighborhoods in the Specific Plan area. The streetscape gives the user a sense of direction and place within the Specific Plan area. These enhancements also enhance connectivity and the ease of movement for pedestrians and bicyclists, supporting healthy transportation options.

## 5.2 Streetscape Improvements

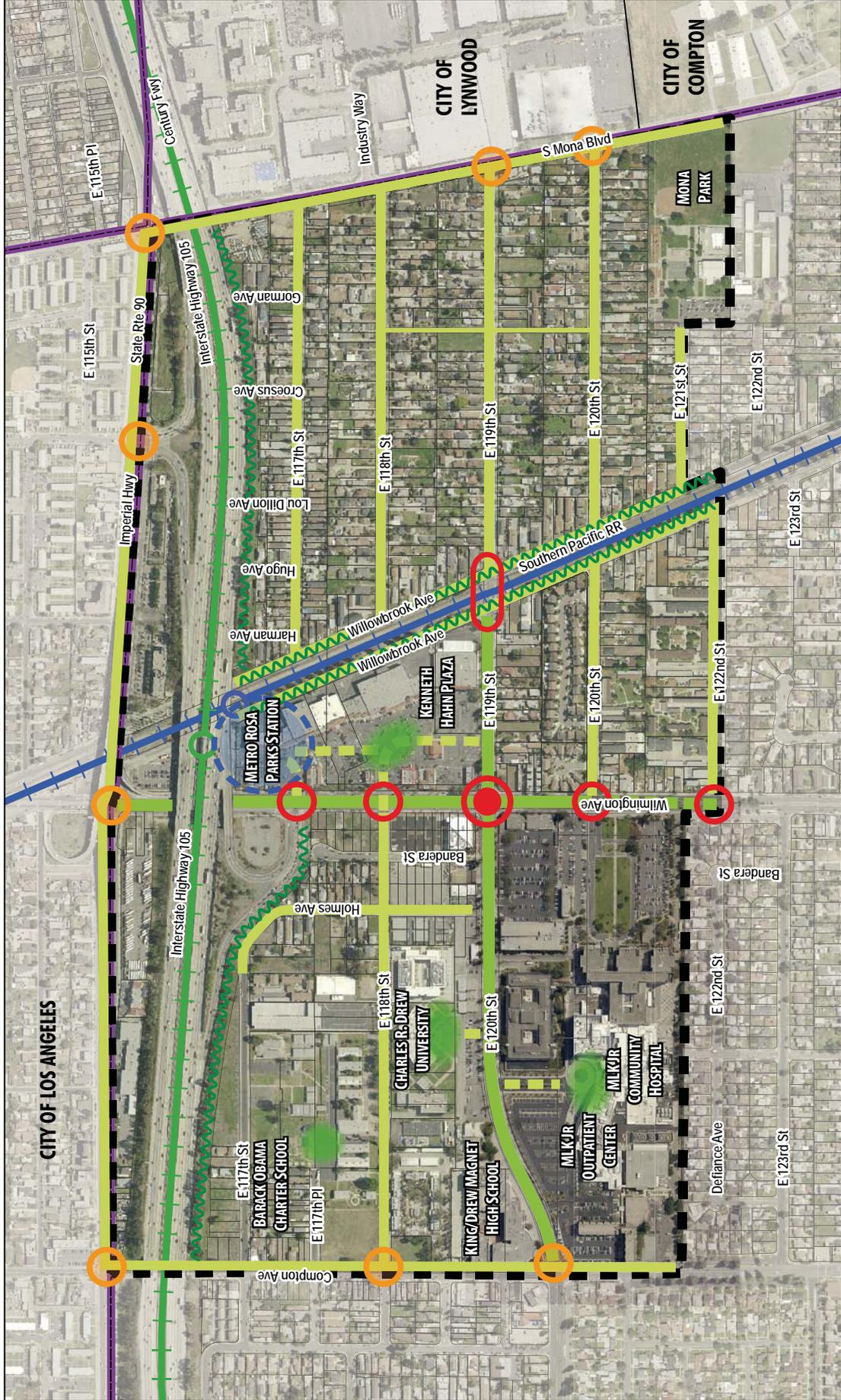
Recommended streetscape improvements consist of an interrelated palette of street trees, street furniture, street lighting, signage and landscaping. Public art can also have a role in the streetscape palette. These elements are illustrated in Figure 5-1 and described in more detail in the following sections:

### A. Willowbrook Area Access Improvements Project

The Los Angeles County Department of Public Works (LACDPW) has streetscape improvements planned for Wilmington Avenue (Imperial to E. 120th Street) and E. 120th/E. 119th Street (Compton to Willowbrook) as a part of the Willowbrook Area Access Improvements Project (WAAI Project). The streetscape design includes new sidewalks, Class II bicycle lanes, pedestrian lights, street trees, street furnishings, and bus shelters. The WAAI Project also includes signage and wayfinding services with the theme of “willow and brook.” The Specific Plan builds on this project by continuing the design themes of the WAAI Project to the rest of the Specific Plan area.

### B. Green Alley Project

LACDPW is developing design concepts for a green alley in the Specific Plan area. The project limits are between E 118th Street and E 120th Street. The project consists of colored concrete pavement; curb and driveway reconstruction at the alley entrance; crosswalk enhancements; pedestrian lighting; monument sign; and decorative walls that hold potted plants.



**Figure 5-1  
Streetscape Improvements**

- |  |  |   |
|--|--|---|
| <ul style="list-style-type: none"> <li> County of Los Angeles</li> <li> Willowbrook TOD Specific Plan Area</li> <li> Metro Green Line</li> <li> Metro Blue Line</li> <li> Pedestrian Connections</li> <li> New Street Lighting &amp; Street Trees</li> </ul> | <ul style="list-style-type: none"> <li> DPW Roadway Improvements Project <ul style="list-style-type: none"> <li>- Roadway pavement improvements</li> <li>- Trees</li> <li>- Area signage</li> <li>- Landscaping</li> </ul> </li> <li> Extend DPW Roadway Improvements to Specific Plan boundary</li> <li> Landscape Screening</li> </ul> | <ul style="list-style-type: none"> <li> Metro Rosa Station Improvements</li> <li> Major Intersection Improvements</li> <li> Minor Intersection Improvements</li> <li> Major Pedestrian Hub</li> <li> Park/Open Space/Plaza</li> </ul> |
|--|--|---|

**WILLOWBROOK TOD SPECIFIC PLAN**

5 minute walk (1,200')

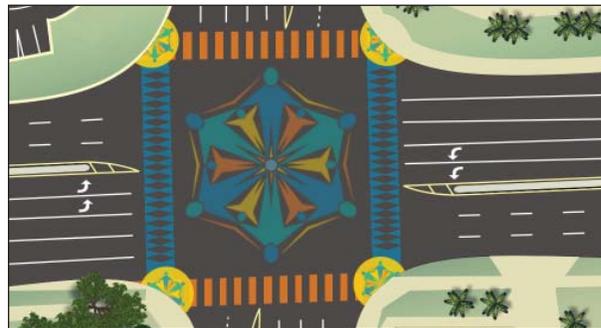
Feet  
0 325 650 1,300 1,500

### C. Streetscape Improvements at the Intersection of Wilmington Avenue and East 120th / East 119th Street

The two major pedestrian streets within the Specific Plan area are Wilmington Avenue in the north-south direction and E. 120th/E. 119th Street in the east-west direction. These two streets connect the major activity areas as well as connect across the rail tracks with the residential neighborhoods to the east. They also intersect to form the pedestrian core of the Specific Plan Area. This intersection is the center of the Specific Plan area and should have special treatments to mark it as such. Building on the improvements planned per the WAAI Project, the Specific Plan proposes additional improvements at this intersection as illustrated in Figure 5-2. These include a bulb-out on the southeast corner of the



Figure 5-2: Wilmington Avenue and E. 120th/E. 119th Street Intersection



*Examples of paving designs that highlight special intersections.*

intersection, and additional paving designs to mark this intersection as special. A gateway structure across Wilmington Avenue could also be explored. Examples of gateway structures from other communities are shown below.



Gateway structures that mark entries to special areas can also be used to celebrate the heart of a community.

## D. Pedestrian Amenities

Elements of an engaging, pedestrian-oriented streetscape include: street trees (described in the Section 5.3E), street lights, street furniture, wayfinding, and landscaped open space. In some instances, public art and water features may also be appropriate where it serves to interpret and draw attention to the history or culture of the area. In general, pedestrian amenities should be focused along the major streets and locations that have the highest amount of pedestrian activity.

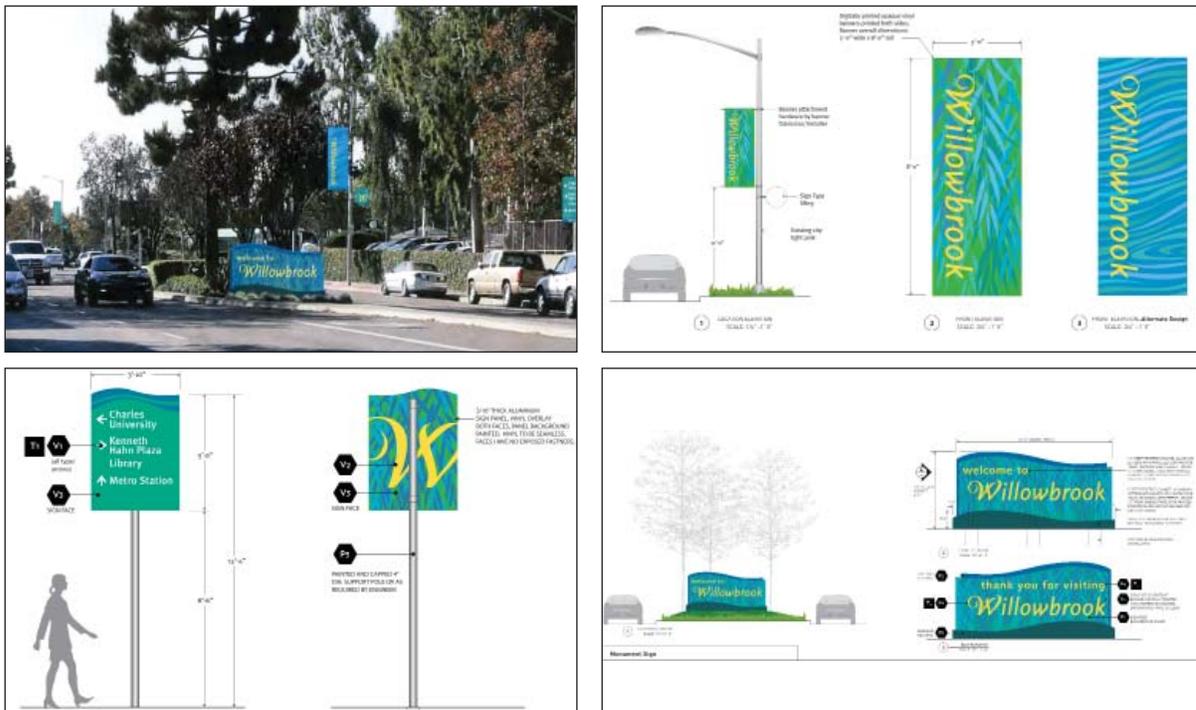
- 1. Street Furniture.** Various styles of street furniture can be found in the Specific Plan area. A consistent palette of street furniture will help unify the area, as well as provide essential seating, trash receptacles and bicycle racks. The WAAI Project is a first step in this process. As a part of this project, existing bus shelters, benches, trash receptacles and bicycle racks will be replaced or refinished to match the selected design theme. The WAAI street furniture palette is shown below. Trash receptacles similar to those proposed by the WAAI Project should be added at all intersections in the Specific Plan area. The feasibility of installation will depend on the site conditions at each intersection. ADA accessibility requirements must be followed to ensure that a clear path of travel is not impeded. Additional trash receptacles may be installed as pedestrian activity levels increases.

Site furnishings (benches, trash receptacles and bicycle racks, etc.) are also encouraged in setback areas, paseos, plazas, and courtyards. The selected site furnishings should be compatible in style with the buildings and selected to bring comfort, scale and design expression to the streetscape. It also must be highly durable and easy to maintain. All elements of the furniture palette should be uniform.



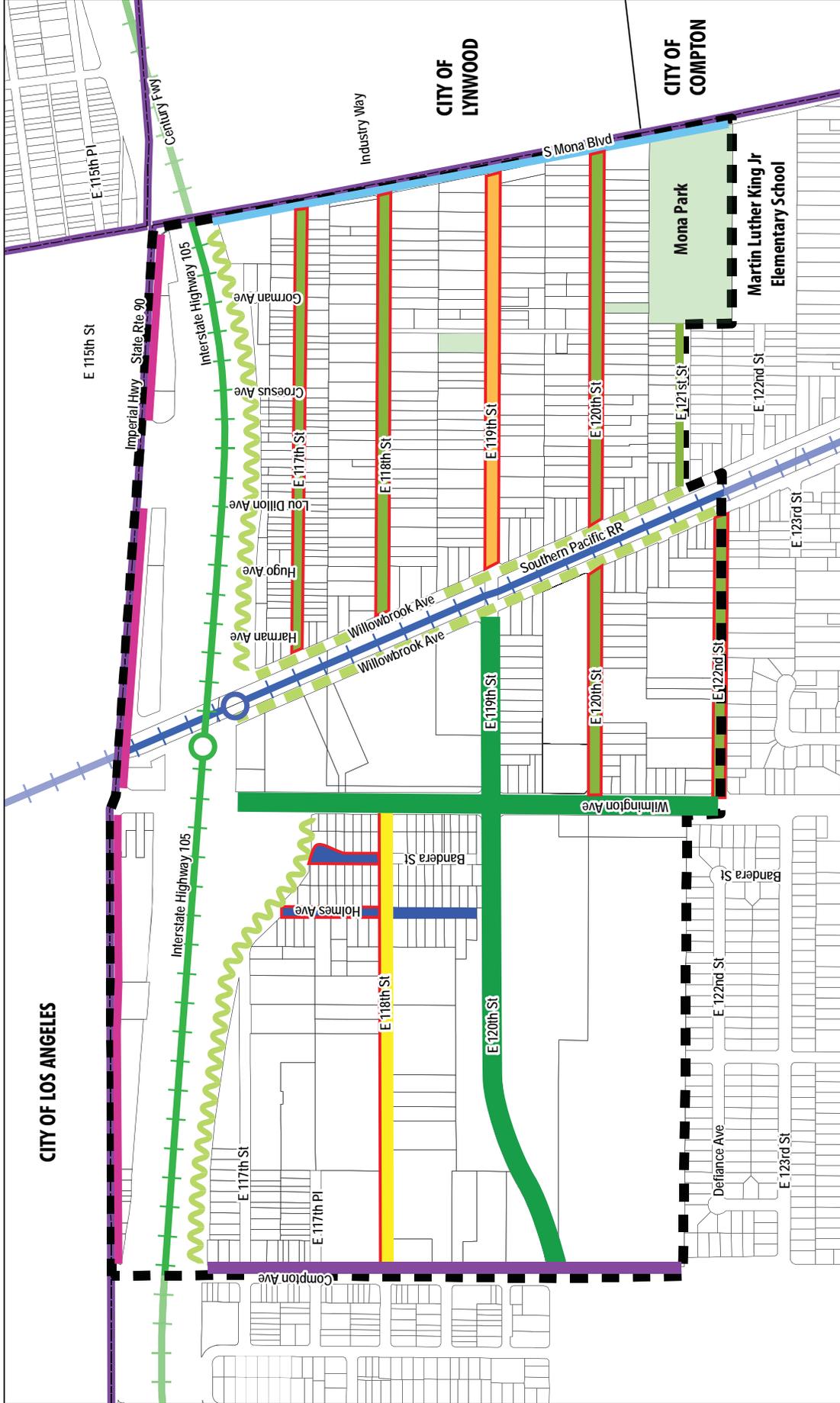
The selected street furniture palette in the Willowbrook Area Access Improvements project includes Landscape Forms' Presidio bench and trash receptacle in a silver metallic finish. (Source: County of Los Angeles Department of Public Works)

2. **Wayfinding.** Wayfinding helps people orient themselves in physical space and navigate from place to place through effective signage, markers, and/or monuments. It is how people choose a path within the built environment. A system of wayfinding signs and entry monument signs is a part of the WAAI Project. As shown below, these signs have a consistent design with a coordinated color palette that creates a unique theme (willow and brook) that is both recognizable and sensitive to its context. The Specific Plan recommends expanding this wayfinding system to the entire Specific Plan area. One of the key locations for this expansion is at the Willowbrook/Rosa Parks Station. Directional signage at this location should direct motorists and pedestrians to the rest of MLK Jr. Medical Center, CDU, Kenneth Hahn Plaza, as well as other public facilities and public parking.



Banners, directional signs and monument signs are a part of the Willowbrook Area Access Improvements project. (Source: County of Los Angeles Department of Public Works)

3. **Street Lighting.** Lighting promotes safety and helps to create an appealing walkable environment. Lighting provides spatial definition to the sidewalk, adds ambiance to neighborhood settings, and affords a sense of security to users. The WAAI Project adds pedestrian-scaled lights to the key pedestrian streets. The Specific Plan recommends adding pedestrian-scaled lighting spacing



**Figure 5-3  
Street Tree Designations**

	County of Los Angeles		Callery Pear
	Willowbrook TOD Specific Plan Area		Chinese Pistache
	Metro Green Line		Existing Screening Vegetation
	Metro Blue Line		Bronze Loquat or Pink Trumpet Tree
	Fraser's Photinia alternating with existing Canary Island Pine		Natchez Grape Myrtle
			Chinese Flame Tree
			Brisbane Box
			Holly Oak
			Pattern of - Natchez Grape Myrtle - Brisbane Box - London Plane Tree
			Turf Parkways (maintain existing)

**WILLOWBROOK TOD SPECIFIC PLAN**



to the remainder of the streets in the Specific Plan area. The selected light fixtures should also adhere to guidelines set forth by the Dark Sky Association to protect the area's view of stars. Light fixtures in the public right-of-way shall also follow the Southern California Edison (SCE) standards for maintenance.

- 4. Public Art.** Public art can complement and enhance the Specific Plan area, and make positive contributions to the public's experience and understanding of life in the area. Arts and culture help to enrich everyday life, stimulate the local economy, enhance the environment, celebrate the nature, engage citizens, and foster neighborhood pride. Art brings the community together, creating situations not merely for enjoyment and beauty, but also for dialogue, thought, and growth.

There are opportunities to encourage this evolving identity of the area by further integrating public art into the fabric and streetscape, such as at the Metro Station, along major streets and activity centers.

## E. Street Trees

The Specific Plan area has an inconsistent palette and pattern of street trees. Except for a few street segments, a mix of street tree species can be found in the Specific Plan area. This lack of uniformity contributes to the inconsistent and fragmented character of the Specific Plan area streets. A palette of carefully selected street trees will unify the variety of existing and future land uses in the Specific Plan area. In addition, street trees will provide shade, add seasonal color, define the street edge, and add to the urban forest. A uniform pattern of street trees will also increase connectivity and sense of cohesion on a pedestrian and auto-oriented scale, as well as offer a pleasant sidewalk experience.

Table 5-1 lists and Figure 5-3 illustrates the street tree designations for the streets within the Specific Plan area. Figure 5-4 illustrates the Street Tree Palette. The street trees for Wilmington and E. 120th/E. 119th Street (between Compton and Willowbrook Avenues), as planned in the WAAI Project, are also incorporated in this list. It also lists the existing street tree type found on each street. The designated street trees are also on the LACDPW Urban Forestry Unit's Maintenance District street tree lists.

Tree spacing recommendations for each street and tree type are also noted in Table 5-1. These are based on the canopy size of a mature tree of each species, the land uses along the streets, presence or absence of overhead power lines, and the available area for planting along the sidewalks and/or parkways. The final spacing will be determined by LACDPW and will need to consider field conditions including driveway aprons, street lights, and utilities on a block-by-block basis.

The following street tree and planting guidelines shall be adhered to for new street trees in addition to the standards established by LACDPW:

- All plant material shall meet the minimum standard of the American Nurserymen and Landscape Association and California State Department of Agriculture Regulations, and be local container or field grown material.
- All trees shall be planted in accordance with established County planting standards.
- The minimum planting size for a tree in parkways and medians is a 24-inch boxed container.
- Median and parkway trees shall be trimmed to retain a trunk space clear of branches of at least 6 feet from grade at maturity.
- All street trees that are not placed in parkways shall be placed in a tree well per LACDPW specifications. Table 5-1 lists the minimum tree well dimensions.

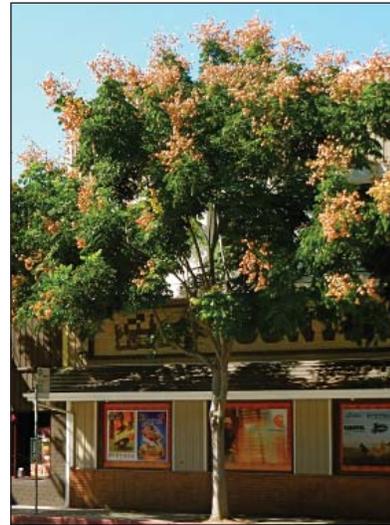
Figure 5-4: Street Tree Palette



**Pink Trumpet Tree**  
*Handroanthus impetiginosus*  
 Deciduous  
 Height: 30-50 feet  
 Canopy: 30 feet  
 Bloom: Spring  
 Water: low to moderate  
 Powerline Friendly: Yes



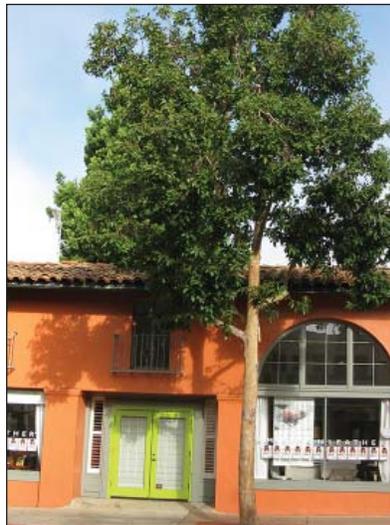
**Bronze Loquat**  
*Eriobotrya deflexa*  
 Evergreen  
 Height: 25-30 feet  
 Canopy: 20 feet  
 Bloom: Winter  
 Water: low to moderate  
 Powerline Friendly: Yes



**Chinese Flame Tree**  
*Koelreuteria bipinnata*  
 Deciduous  
 Height: 25-45 feet  
 Canopy: 40 feet  
 Bloom: Summer  
 Water: moderate  
 Powerline Friendly: Yes



**'Natchez' Crape Myrtle**  
*Lagerstroemia indica x fauriei*  
 'Natchez'  
 Deciduous  
 Height: 15-25 feet  
 Canopy: 25 feet  
 Bloom: Summer  
 Water: low to moderate  
 Powerline Friendly: Yes



**Brisbane Box**  
*Lophostemon confertus*  
 Evergreen  
 Height: 30-60 feet  
 Canopy: 40 feet  
 Bloom: Summer  
 Water: low to moderate  
 Powerline Friendly: No



**Holly Oak**  
*Quercus ilex*  
 Evergreen  
 Height: 30-65 feet  
 Canopy: 50 feet  
 Bloom: Spring, Summer  
 Water: low to moderate  
 Powerline Friendly: No

Figure 5-4 (continued)



**Chinese Pistache**  
*Pistacia chinensis*  
 Deciduous  
 Height: 60 feet  
 Canopy: 50 feet  
 Bloom: Winter  
 Water: low to moderate  
 Powerline Friendly: No



**London Plane Tree**  
*Platanus x acerifolia*  
 'Bloodgood'  
 Deciduous  
 Height: 40-80 feet  
 Canopy: 70 feet  
 Bloom: Spring  
 Water: moderate  
 Powerline Friendly: No



**Callery Pear 'Aristocrat'**  
*Pyrus calleryana*  
 Deciduous  
 Height: 35-45 feet  
 Canopy: 35 feet  
 Bloom: Spring  
 Water: moderate  
 Powerline Friendly: Yes



**Canary Island Pine**  
*Pinus canariensis*  
 Evergreen  
 Height: 65-80 feet  
 Canopy: 40 feet  
 Bloom: None  
 Water: low to moderate  
 Powerline Friendly: No



**Fraser Photinia**  
*Photinia x fraseri*  
 Evergreen  
 Height: 10-15 feet  
 Canopy: 20 feet  
 Bloom: Spring  
 Water: moderate  
 Powerline Friendly: Yes

**Table 5-1:  
Street Tree Designations in the Specific Plan Area**

Street Names	Existing	Specific Plan (2015)	Recommended Spacing * (in feet)	Minimum Tree Well Dimensions ** (in feet)
<b>North-South</b>				
Compton Avenue (East side only)	Victorian Box <i>Pittosporum undulatum</i> Lemon Bottlebrush <i>Callistemon citrinus</i>	Brisbane Box <i>Lophostemon confertus</i>	30'-40'	5 feet square
Holmes Avenue	Mixed	Chinese Flame Tree <i>Koelreuteria bipinnata</i>	30'-40'	5 feet square / Turf Parkway
Wilmington Avenue	Mixed	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i> London Plane Tree (Maintain Existing) <i>Platanus x acerifolia</i>	-- -- --	
Willowbrook Avenue	Canary Island Pine <i>Pinus canariensis</i>	Fraser Photinia <i>Photinia x fraseri</i>	Alternating between existing	Parkway
Mona Boulevard (West side only)	None	Holly Oak <i>Quercus ilex</i>	40'-50'	Turf Parkway
Bandera Street	None	Chinese Flame Tree <i>Koelreuteria bipinnata</i>	30'-40'	5 feet square / Turf Parkway
<b>East-West</b>				
Imperial Highway (South side only)	Mixed	Chinese Pistache <i>Pistacia chinensis</i>	30'-40'	5 feet square
117th Street	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 feet square / Turf Parkway
118th Street (West of Wilmington Avenue) (North side only)	Mixed	Callery Pear <i>Pyrus calleryana</i>	30'-40'	5 feet square / Turf Parkway
118th Street (East of Wilmington Avenue)	London Plane Tree <i>Platanus x acerifolia</i>	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 feet square / Turf Parkway
119th Street (East of Willowbrook Avenue)	Mixed	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i>	20'-30'	4 feet square / Turf Parkway
120th Street (East of Wilmington Avenue)	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 feet square / Turf Parkway
120th Street (West of Wilmington Avenue)	Canary Island Pine <i>Pinus canariensis</i>	Natchez Crape Myrtle <i>Lagerstroemia indica x fauriei 'Natchez'</i> Brisbane Box <i>Lophostemon confertus</i>	-- --	
121st Street (North side only)	None	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 feet square / Turf Parkway
122nd Street (North side only)	Mixed	Pink Trumpet Tree <i>Handroanthus impetiginosus</i> Alternative: Bronze Loquat <i>Eriobotrya deflexa</i>	25'-35'	5 feet square / Turf Parkway

\* Recommended spacing is based on canopy size of mature trees, available planting areas along sidewalks/parkways and adjacent land uses

\*\* Minimum Tree Well Dimension: Optimal sizes provided. Actual size and planting shall be dependent on sidewalk width, adjacent land uses and tree type

## 6.1 Introduction

The purpose of this chapter is to provide an overview of existing and planned infrastructure facilities, and to outline recommended infrastructure upgrades for the Willowbrook Transit Oriented District (TOD) Specific Plan area. The recommended upgrades are based on analyses of the capabilities and capacities of existing facilities and projected infrastructure needs based on build-out of the Specific Plan. Figure 6-1 shows how the Specific Plan area has been divided into “groups” for the purposes of the analyses.

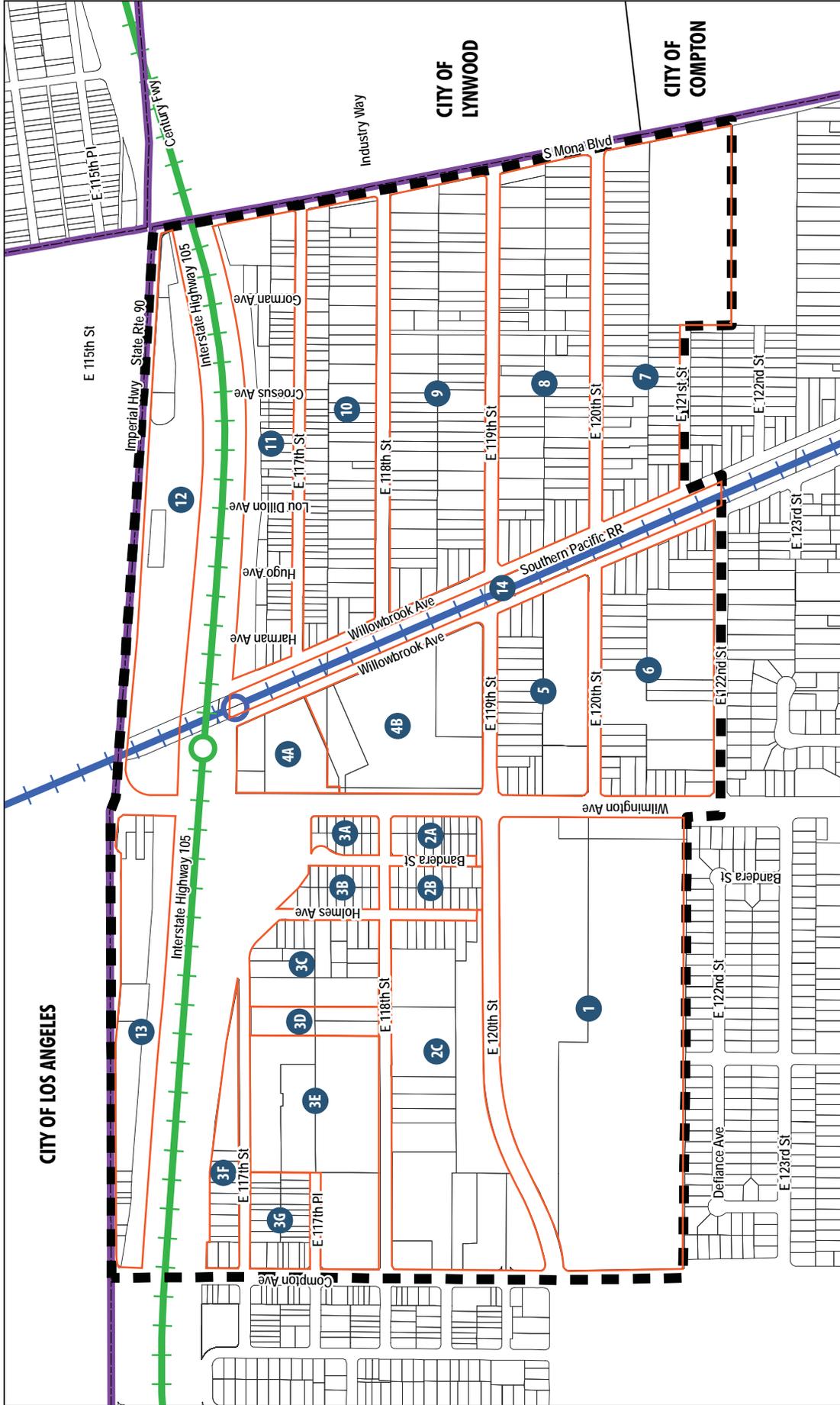
## 6.2 Water System

Two water purveyors serve the Specific Plan area: Park Water Company and Golden State Water Company. Park Water Company provides water service to the area west of Wilmington Avenue, and Golden State Water Company provides water service to the area east of Wilmington Avenue. There are potable water lines under almost all streets and alleys within the Specific Plan area. The sizes of the potable water lines vary from 4 to 12 inches. The existing pipes that distribute water services in the Specific Plan area include ST (steel pipe), Asbestos Cement pipes (AC), ductile iron pipes (DI), and Cast Iron pipes (CI). Figure 6-2 shows the layout of the existing water lines. This figure also illustrates the upgrades necessary to meet the projected demand.

As shown in Figure 6-2, except for Group 14 (which remains unchanged), almost all development capacity groups under total build-out of the Specific Plan have some increase in land use density, which in turn results in an increase in water demand. Most of the existing water lines in the Specific Plan area are 8 inches in diameter and above. The 8 inch pipes generally have the capacity to handle the increase in water demand/load under total build-out of the Specific Plan area. However, in order to meet the demand of water consumption, including water services for fire prevention purposes, such as demand of new fire hydrants and fire sprinkler systems in the new developments in the post total “build-out” condition, the following upgrades in water line pipe sizes are recommended:

**Group 2A, 2B:**

Replace existing water line of 4” CI on 118th Street north of this area with 8” DI (or PVC C-900).



**Figure 6-1**  
**Key Map by Block for Calculating**  
**Infrastructure Demand**

**WILLOWBROOK TOD SPECIFIC PLAN**



-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line

**Group 3B:**

Replace existing water line of 6" CI in the alley between Holmes Avenue and Bandera Street with 8" DI (or PVC C-900).

**Group 3C, 3D, 3E, 3F, 3G:**

Upgrade existing water line of 4" CI on 117th Street from Compton Avenue to Holmes Avenue and the 4" CI in 117th Place with 8" DI (or PVC C-900).

**Group 8, 9:**

Upgrade existing water line of 6" CI and 4" CI in 119th Street to 8" DI (or PVC C-900) from Willowbrook Avenue to Mona Boulevard.

**Group 9, 10:**

Replace existing water line of 6" CI and 4" CI in 118th Street with 8" CI from Willowbrook Avenue to Mona Boulevard.

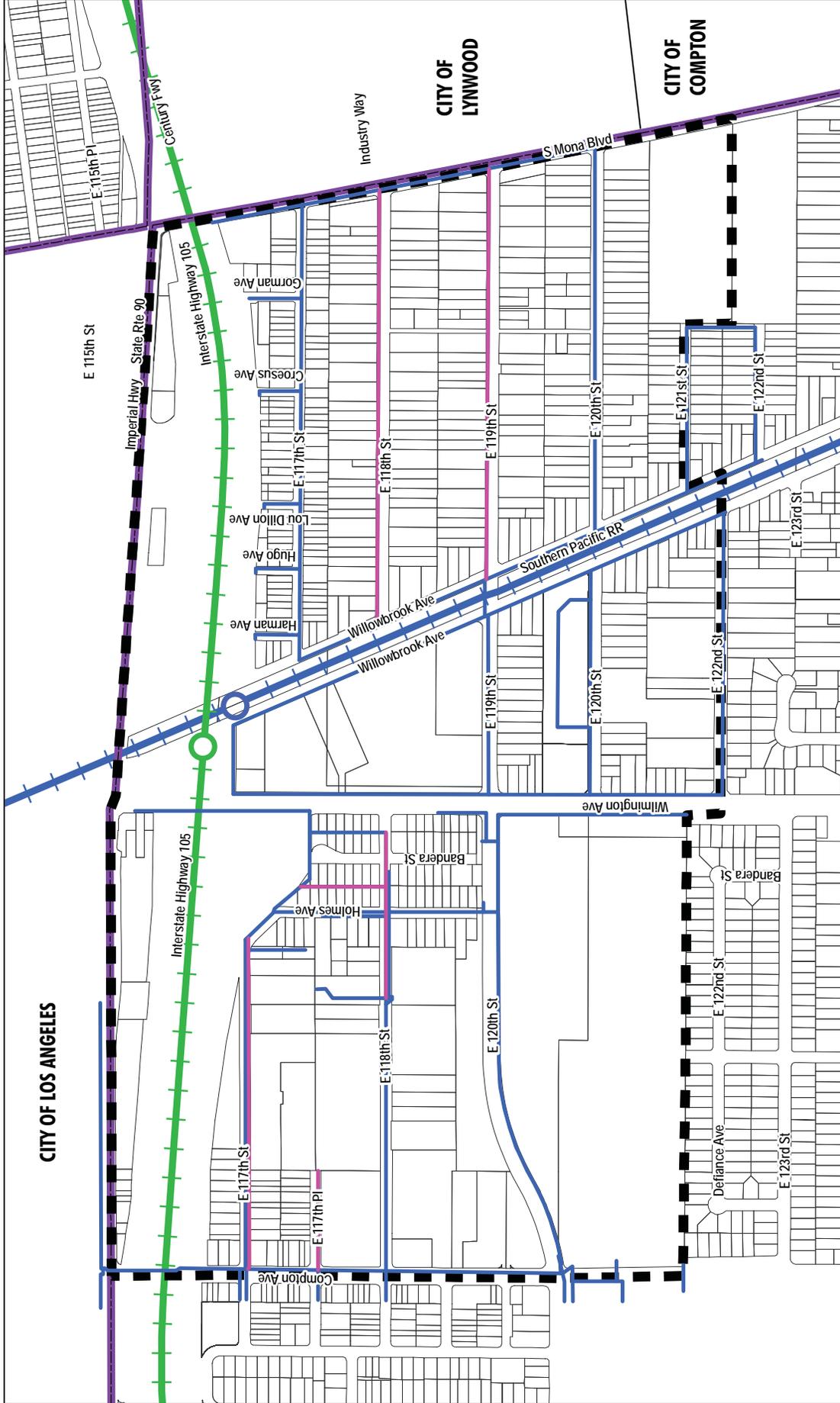
Both Park Water Company and Golden State Water Company will evaluate all development that will occur in the future and conduct their own detailed analysis, and ultimately decide whether the water distribution facilities are required to be upgraded.

### 6.3 Sewer System

The sewer lines serving the Specific Plan area are owned and maintained by the Los Angeles County Department of Public Works (LACDPW) and Los Angeles County Sanitation District (LACSD). The sewer lines within the area are mainly Vitrified Clay Pipes (VCP). The sewer mains are all under the major streets with smaller lateral lines connecting directly to the residential building or businesses (Figure 6-3).

As mentioned earlier in the water system section, except for Group 14 (which remains unchanged), almost all development capacity groups under total build-out of the Specific Plan have some increase in land use density, which in turn results in an increase in sewage effluent throughout the Specific Plan area. A rough estimate of cumulative sewage effluent by each Group is calculated (Table 6-1) using the Zoning Coefficient for runoff for the sewer area study developed by LACDPW Land Development Division. All existing sewer mains in the Specific Plan area are 8" or above in diameter. Using the minimum allowable slope of 0.32%, the design capacity for the existing sewer mains are at least 0.36 cfs (some pipes are larger than 8"). Therefore, all the existing sewer mains should be adequate to meet the future cumulative sewage effluent in every group except for Group 1.

Group 1 has an estimated cumulative sewage effluent of 1.225 cfs. For the purposes of the infrastructure analyses, it is assumed at build-out, the sewage from new development will evenly flow in three separate directions, namely, west, north and east (there is no existing sewer main in the south side of MLK Medical Center). With this assumption, it is estimated that the average cumulative sewage effluent going into each direction is  $1.225 \text{ cfs} / 3 = 0.408 \text{ cfs}$ , which is slightly higher (13.33%) than the design capacity of any existing sewer main around the hospital, 0.36 cfs. Since the design capacity refers to half-full capacity of the existing main, an increment of 13.33% does not justify an upgrade of the sewer main. Regardless, when new development within Group 1 is proposed, a detailed sewer area study of new development within Group 1 will be required by the LACDPW. The sewer area study would determine whether any upgrade will be required for sewer system network.



**Figure 6-2**  
**Existing and Planned**  
**Water System**

**WILLOWBROOK TOD SPECIFIC PLAN**



- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Existing Water System
- Upgrade 4 or 6 (E) Water to 8" DI Water

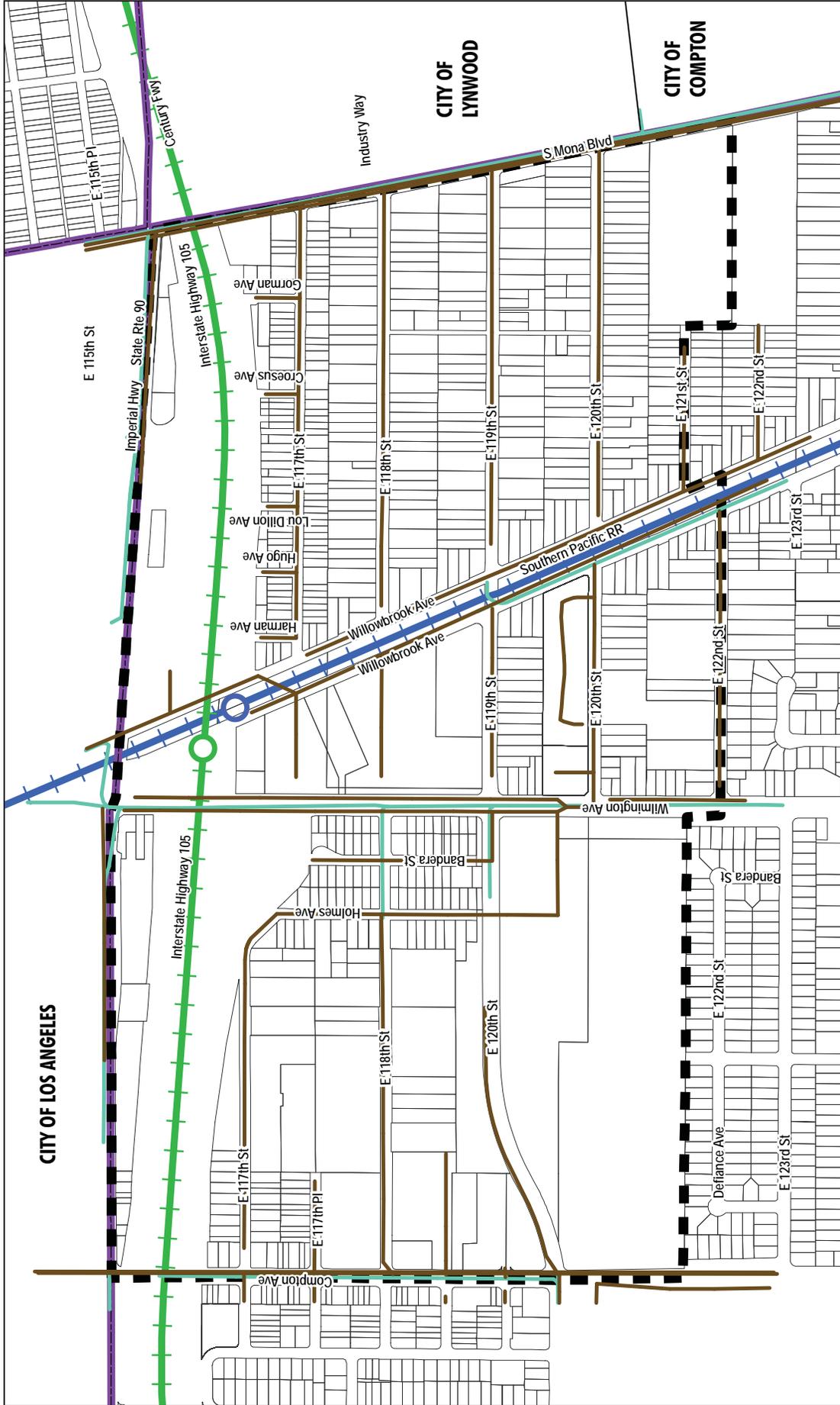
Sewer laterals are sewer pipes that connect with buildings with sewer mains that run in the street. In general, new or upgraded sewer laterals are required for new buildings, with the cost of the installation borne by the developers. The LACSD will also review all new sewer infrastructure upgrades as the LACDPW will be sending new or updated tentative tract or parcel maps for the district to review.

**Table 6-1:  
Estimated Cumulative Sewage Effluent by Each Development Capacity Group**

Group	GPD for Existing Development	In cfs	GPD for Maximum Development	In cfs
1	267267.0	0.414	791676.0	1.225
2A	9900.0	0.015	34520.8	0.053
2B	955.0	0.001	38160.8	0.059
2C	31799.0	0.049	110131.0	0.170
3A	0.0	0.000	27143.9	0.042
3B	4750.0	0.007	33108.1	0.051
3C	7500.0	0.012	101383.1	0.157
3D	3750.0	0.006	8790.3	0.014
3E	6870.0	0.011	218130.7	0.337
3F	1000.0	0.002	57561.8	0.089
3G	6335.9	0.010	53421.6	0.083
4A	4944.7	0.008	33709.4	0.052
4B	13983.9	0.022	104919.4	0.162
5	20900.0	0.032	42400.0	0.066
6	68000.0	0.105	103000.0	0.159
7	17500.0	0.027	26750.0	0.041
8	24750.0	0.038	41500.0	0.064
9	29000.0	0.045	45000.0	0.070
10	32400.0	0.050	45400.0	0.070
11	16750.0	0.026	29000.0	0.045
12	0.0	0.000	25308.8	0.039
13	4306.6	0.007	20865.5	0.032
14	0.0	0.000	0.0	0.000

Note

1. According to the LA County Estimated Average Daily Sewage Flows for Various Occupancies, for church building, the average daily flow is 3gpd/seat. Assuming there are at least 50 seats in the existing church in Group 5 area
2. GPD - Gallons per day
3. CFS - Cubic feet per second



**Figure 6-3  
Existing Sewer and  
Storm Drain System**

- County of Los Angeles
- Willowbrook TOD Specific Plan Area
- Metro Green Line
- Metro Blue Line
- Existing Sewer System
- Existing Storm Drain System

**WILLOWBROOK TOD SPECIFIC PLAN**



## 6.4 Storm Drain System

The storm drain system serving the Specific Plan area is maintained by the Los Angeles County Flood Control District (LACFCD). The storm drain system within the Specific Plan area consists of mostly underground Reinforced Concrete Pipes (RCP) ranging from 24 to 54 inches (Figure 6-3).

Build-out of the Specific Plan area will generate little or no increase in runoff to the existing drainage system, since the area is completely developed. Approximately 80-90% of the existing Specific Plan area is impervious. It is predicted that the new development will not directly trigger any need for upgrades to the County's existing storm drain major backbone facilities, mainly due to the Low Impact Development (LID) Ordinance requirements for percolation and on-site detention for new development, which will stabilize and/or even reduce runoff in the area.

There are no recommendations to upgrade the existing storm drain system within the Specific Plan area.

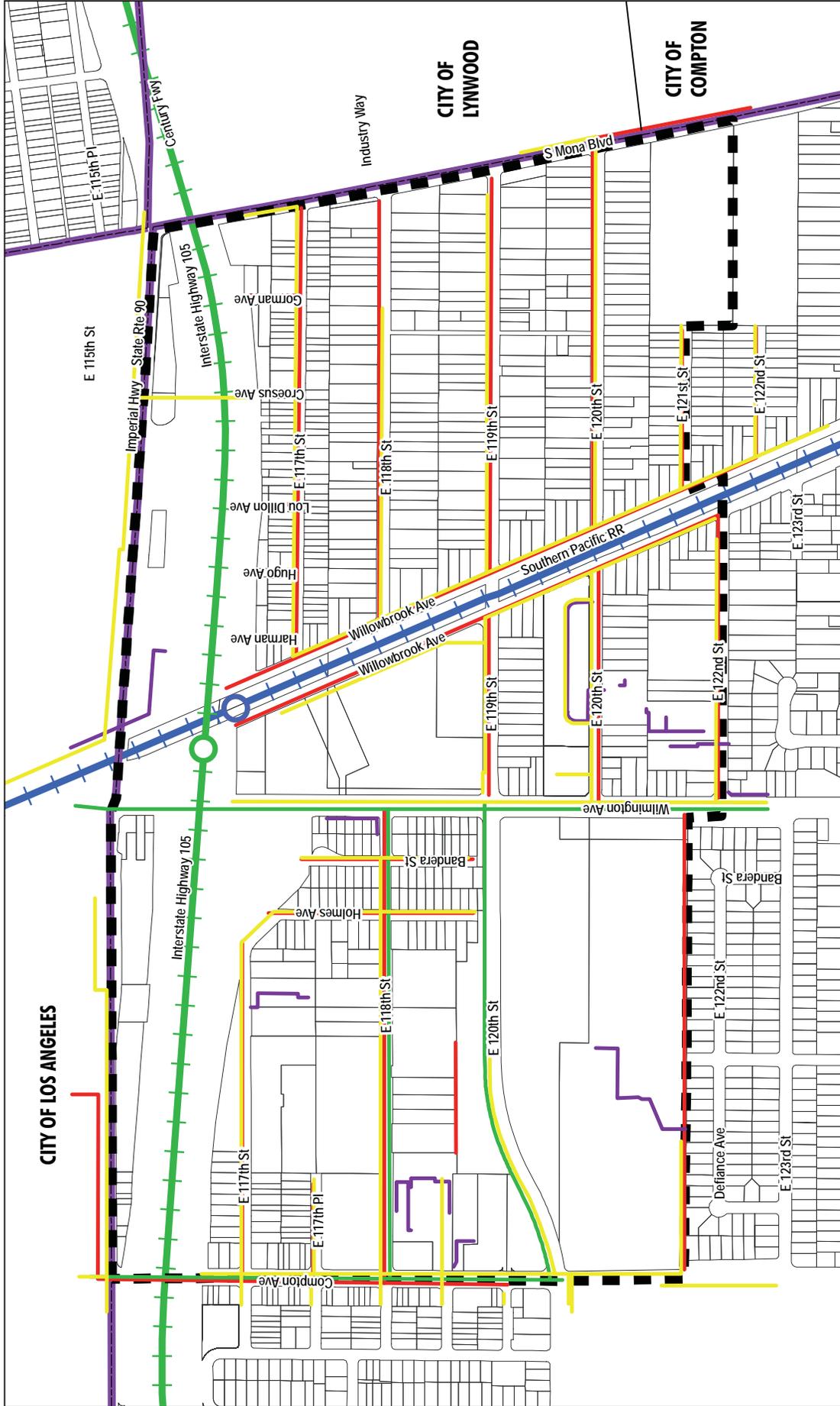
### LID Practices and Project Requirements

In October 2008, the County adopted the LID Ordinance as part of Los Angeles County Code Title 12, Chapter 84 to require the use of LID principles in all development projects except road and flood infrastructure projects. Unlike traditional stormwater management, which collects and conveys stormwater runoff through storm drains, pipes, or other conveyances to a centralized stormwater facility, LID uses site design and stormwater management to maintain the site's pre-development runoff rates and volumes. The goal of LID is to mimic a site's pre-development hydrology by using design techniques that infiltrate, filter, store, evaporate, and detain runoff close to the source of rainfall.

LID practices or stormwater quality control measures can be categorized into the following types:

- *Retention-based stormwater quality control measures*  
Examples: bio-retention, infiltration basin, dry well, permeable pavement, etc.
- *Bio-filtration*  
Example: bio-filtration area
- *Vegetation-based stormwater quality control measures*  
Example: stormwater planter (or planter box), vegetated swale, green roof, etc.
- *Treatment-based stormwater quality control measures*  
Examples: sand filter, constructed wetland, proprietary treatment control measures.

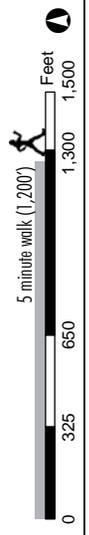
All new development that requires new grading in the Specific Plan area will require preparation of a hydrology study to demonstrate that building sites are free from flooding hazard. New development will be required to mimic the site's pre-development runoff by choosing the appropriate LID practice most suitable for the site. A proposed project must demonstrate that any proposed improvement, including filling, does not raise the flood level upstream or downstream of the project. As required by the County, developers shall prepare National Pollution Discharge Elimination System (NPDES) reports, such as the Water Quality Management Plan (WQMP) and Stormwater Pollution Prevention Plan (SWPPP), to ensure the quality of water is preserved and adverse environmental impacts are minimized.



**Figure 6-4**  
**Existing Dry Utilities**

-  County of Los Angeles
-  Willowbrook TOD Specific Plan Area
-  Metro Green Line
-  Metro Blue Line
-  Electrical, Telecommunications, Cable TV Aerial Facilities
-  Gas
-  Underground Telecommunications Conduit
-  Cable Television

**WILLOWBROOK TOD SPECIFIC PLAN**



## 6.5 Electrical System

Southern California Edison (SCE), which is a private utility company, provides electrical power service to the Specific Plan area. SCE sets its own service standards, with oversight from the California Public Utilities Commission (CPUC), and facility improvement strategies. Currently, there is a network of aerial electrical facilities creating a power grid that supplies sufficient electrical service to the Specific Plan area. There is no major deficiency or functional problem in the power supply facilities within the Specific Plan area. The specific locations of the existing underground and overhead electrical lines are illustrated in Figure 6-4.

The decision to upgrade the power supply facilities and the number of upgrades to meet the demand of future development will be determined by SCE in coordination with the County after developers have submitted their building plans. Demand for services and the ability to serve new developments are generally determined on a case-by-case basis.

Most of the electrical distribution facilities in the Specific Plan area are aerial facilities. The Specific Plan recommends that existing aerial electrical facilities be placed underground whenever funding is available. Underground electricity provides higher reliability, is safer in general and is also less unsightly. One of the potential funding mechanisms is CPUC Rule 20. CPUC Rule 20 is a set of policies and procedures established by the CPUC to regulate the conversion of overhead electric equipment to underground facilities, a process often referred to “undergrounding”. Rule 20 determines the level of ratepayer funding for different undergrounding arrangements.

### **CPUC Rule 20**

*Under Rule 20, undergrounding projects are financed by utility rate money, combined rate funds and local tax proceeds, or private funds, depending on whether Rule 20A, Rule 20B or Rule 20C provisions apply.*

**Rule 20A:** *Rule 20A projects are paid for by all SCE customers and ratepayers, not just those who live in locations where facilities will be undergrounded. To qualify for full funding through utility rate proceeds, projects must produce a benefit to the general public, not just customers in the affected area, by satisfying one or more of these criteria:*

- *The location has an unusually heavy concentration of overhead facilities.*
- *The location is heavily traveled.*
- *The location qualifies as an arterial or major collector road in a local government’s general plan.*
- *The overhead equipment must be located within or pass through a civic, recreational or scenic area.*

*Using CPUC formulas, SCE allocates rate funds to communities for undergrounding based on previous allocations, the ratio of customers served by overhead facilities to all the customers in the community, and the fraction that customers in the community represent of all SCE customers. Local governments use these formulas to project allocations, which allow them to prioritize projects and develop project schedules. Because funds are limited, local governments sometimes must wait and accumulate their allocations before starting an undergrounding project.*

**Rule 20B:** *If an area is not eligible for Rule 20A or if local government cannot or chooses not to rely on the Rule 20A allocation process, Rule 20B allows rate funds to subsidize an undergrounding project. The subsidy includes an amount equal to about 5-20% of the total cost. The remaining cost is funded by local governments or through neighborhood special assessment districts.*

**Rule 20C:** *Rule 20C enables property owners to pay for undergrounding electric lines and equipment if neither Rule 20A nor 20B applies.*

It is also worth noting that, prior to any undergrounding process, SCE will take the lead in contacting the other low voltage utilities that might be sharing the power poles with SCE aerial facilities, such as the telecommunication providers (AT&T) and cable TV provider (Time Warner Cable) to coordinate and execute a joint trench. During this process, SCE will also reach out to the LACDPW, Public Utility Division, for their assistance in contacting and coordinating with the aforementioned utility providers.

It is recommended that prior to approving new development, the County should contact SCE regional manager in the Willowbrook area and set up an inspection at the new development site in order to determine whether the existing aerial SCE facilities in the area qualifies for Rule 20A funding.

## 6.6 Natural Gas System

The Southern California Gas Company/Sempra Utilities (The Gas Company), which is a private utility company, is the gas service provider for the Specific Plan area. Currently, gas pipelines are in all major streets in the area, specific locations of gas pipelines are illustrated in Figure 6-4.

The analysis on the capacity and capability to meet future demand will be conducted by The Gas Company in coordination with the County upon submittal of building plans by developers.

## 6.7 Telecommunications System

The Specific Plan area is within the service area of AT&T, which is a private utility company. AT&T is the provider of both local and long distance telecommunications in the Specific Plan area (Figure 6-4). The current local telecommunications network lacks the high speed internet service that will be in demand by future businesses and residents in the area.

AT&T will assess the demand for services and the ability to serve new developments on a case-by-case basis. The capacity and capability analysis for meeting future demands within the Specific Plan area will be conducted by AT&T after building plans are submitted by developers.

If there is “undergrounding” of existing aerial telecommunication facilities, SCE will reach out to AT&T and coordinate a joint trench. To save on costs, AT&T is likely to participate in the joint trench operation. SCE will dictate the layout of the trench, and the telecommunication lines would follow. In most cases, if there is an upgrade of facilities required, AT&T will be responsible for the construction cost up front, and will recoup the cost later on with the additional revenue from added customers.

According to the representative from AT&T, there is no major facility upgrade planned in the Specific Plan area in the near future.

## 6.8 Cable Television System

The cable service provider for this area is Time Warner Cable (TWC), which is a private utility company. Most of the existing cable TV systems within the Specific Plan area are aerial facilities that share power poles with the electrical system, with a few existing underground conduits (Figure 6-4).

New land uses resulting from the Specific Plan will consist of both commercial and residential development; therefore, high speed internet and cable television services will be in demand. Similar to a telecommunications system, TWC will assess the demand for services on a case-by-case basis and ultimately make the decisions concerning upgrades for the existing cable TV systems to meet the demand of the future developments.

If there is undergrounding of existing aerial telecommunication facilities, SCE will reach out to TWC and coordinate a joint trench. Similar to a telecommunications provider, TWC is likely to participate in the joint trench operation to save on construction costs.

In most cases, if there is an upgrade of facilities required, TWC will be responsible for the construction costs up front, and recouping the cost later on with the additional revenue from the added customers.

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# Economic Development Strategy

## 7.1 Introduction

The Specific Plan area is expected to undergo sizable growth in employment to support uses related to the hospital, medical offices, education and research and development, and retail and commercial offices. The purpose of this Chapter is to provide the Economic Development Strategy for the Willowbrook TOD Specific Plan. The strategy provides economic analysis, a financing strategy, and policy recommendations to realize the full potential of Metro’s investments in the redesigned Willowbrook/Rosa Parks Station and the County’s investments in the Martin Luther King, Jr. Medical Center Campus (MLK). The economic development strategy includes:

- Strategies that leverage public investments within the Specific Plan framework and build upon local assets to revitalize the Willowbrook community;
- Identification of potential community benefits from the implementation of the Specific Plan;
- Projections of employment and wage growth from implementation of the Specific Plan;
- Capital Improvement Program (CIP) that identifies and prioritizes a range of infrastructure and amenity improvements that benefit the community; and
- Financing approaches for the implementation of infrastructure and services.

The analysis is informed by the SCAG employment and demographic forecasts for the study area and the MLK Medical Campus Master Plan, as well as supporting documents and studies for the MLK Community Hospital and Charles R. Drew University of Medicine and Science.

## 7.2 Opportunities for Economic Development

The rehabilitation and expansion of the MLK Hospital Campus, the redesign of the Willowbrook/Rosa Parks Station and the potential demand for ancillary medical services that may locate nearby, are the main economic opportunity drivers for the area. It is important to leverage these economic investments to provide employment and housing development opportunities to the Specific Plan area and the greater Willowbrook community.

### 7.3 Vision for a Community of Health and Wellness

The revitalized MLK Medical Campus is a catalyst for broader economic development and community service benefits to the Willowbrook and neighboring communities. By facilitating bicycle and pedestrian improvements and increasing transit access and housing choices, this Specific Plan helps realize the community's vision for health and wellness. This vision was inspired by an Urban Land Institute Technical Assistance Panel study in 2009. The ULI panel identified the entire master-planned area as having the potential to become a health-focused, transit-oriented district.

The Specific Plan area has three interconnected subareas: 1) Medical Center Subarea encompassing the MLK Medical Center campus; 2) Education and Research Subarea encompassing Charles R. Drew University, King-Drew Magnet High School, Compton Lincoln-Drew Elementary School, a proposed community college workforce training center, and an expanded County library; and 3) a Gateway Subarea encompassing Kenneth Hahn Plaza, single and multi-family homes and sites with retail and mixed-use development potential around the Willowbrook/Rosa Parks Station and on Wilmington Avenue.

### 7.4 Economic Development Strategy

The Economic Development Strategy recommends implementation strategies with measurable outcomes. Key components of the strategy are discussed in this section and summarized in Table 7-1. Other key components are public financing and economic incentive methods for implementing the conceptual Capital Improvement Program (CIP) in the Specific Plan area, as presented in Chapter 8.

The Economic Development Strategy presents a range of public and private financing techniques and related programs that could be used as part of the implementation strategy for the CIP, including land use incentive strategies, financing program initiatives, and public-private partnerships that build upon the substantial public and private investment into the Martin Luther King, Jr. Community Hospital, Charles Drew University and the redesign of the Willowbrook/Rosa Parks Station.

Additionally, the CIP compiles the required public infrastructure and related costs to implement the Specific Plan. The CIP also identifies the projects that are essential to attract private investment within the Specific Plan area, and presents short-, mid- and long-range priorities over the anticipated build-out of the Specific Plan area.

Furthermore, the Economic Development Strategy presents five categories of actions to facilitate local economic development objectives. These categories highlight actions available to local County government to promote the vision of the Specific Plan. These actions include: a) institutional arrangements; b) targeted development opportunities; c) land use planning; d) infrastructure and services planning; and e) implementation and financing strategies.

#### A. Institutional Arrangements

**Community Development Corporation.** Establishing a local decision-making agency — Community Development Corporation (CDC) — that directs the visioning, formulation and implementation of economic development strategies/plans offers an effective means to achieve Specific Plan goals. Such an agency could be established under the auspices of the County with stakeholder roles at the county, district and community levels through adequate representation in executive roles. The CDC could

**Table 7-1:  
Implementation Strategy**

Implementation Step	Key Action	Responsible Parties	Phasing
<b>Develop Institutional Arrangements</b>	<ul style="list-style-type: none"> <li>Establish a Community Development Corporation (CDC)</li> </ul>	County of Los Angeles Community Based Stakeholders	Short-Term
<b>Foster Public-Private Partnerships</b>	<ul style="list-style-type: none"> <li>Coordinate with MLK Jr. Community Hospital and Charles Drew University to integrate local workforce training and capabilities with ultimate job growth, direct spin-off activities in healthcare services and allied office uses.</li> </ul>	County of Los Angeles MLK Jr. Community Hospital Charles Drew University CDC	Ongoing
<b>Improve Kenneth Hahn Plaza</b>	<ul style="list-style-type: none"> <li>Work with Kimco, the operator of Kenneth Hahn Plaza, to enhance the retail and dining experience and coordinate pedestrian, safety and landscape improvements at the Plaza</li> <li>Assist in tenant recruitment</li> <li>Work with Kimco to explore mixed use development</li> <li>Continue to collaborate with the Workforce Collaborative's initiative to coordinate workforce training programs for residents with public and private employers.</li> </ul>	County of Los Angeles Kimco CDC	Ongoing  Ongoing Long-term
<b>Continue partnership with Los Angeles County Workforce Collaborative</b>	<ul style="list-style-type: none"> <li>Continue to collaborate with the Workforce Collaborative's initiative to coordinate workforce training programs for residents with public and private employers.</li> </ul>	County of Los Angeles LAC Workforce Collaborative CDC	Ongoing
<b>Create opportunities for infill mixed use development</b>	<ul style="list-style-type: none"> <li>Coordinate with existing property owners on economic development opportunities of under-utilized and vacant sites</li> <li>Coordinate with Compton Unified School District(CUSD) regarding the redevelopment of the school sites</li> <li>Coordinate with the LACDC regarding the use of the Successor Agency-owned parcels</li> </ul>	County of Los Angeles CDC	Mid- to Long- Term
<b>Pursue Local, State and Federal Infrastructure Funding Sources</b>	<ul style="list-style-type: none"> <li>Identify, monitor and apply for other government funding sources, for infrastructure and transportation improvements (also see Tables 7-2, 8-2 and 8-3)</li> </ul>	County of Los Angeles CDC Metro	Ongoing
<b>Establish an Enhanced Infrastructure Financing District (EIFD)</b>	<ul style="list-style-type: none"> <li>Conduct feasibility analysis and necessary elections for establishing an EIFD for financing public improvements and amenities.</li> </ul>	County of Los Angeles CDC	Short- to Mid- Term
<b>Establish a Development Impact Fee (DIF) Program</b>	<ul style="list-style-type: none"> <li>Conduct feasibility analysis for establishing a DIF Program for equitably spreading infrastructure costs equitably over new development.</li> </ul>	County of Los Angeles CDC LACDPW	Short- to Mid- Term
<b>Establish a Parking Management District in the Northwest Subarea</b>	<ul style="list-style-type: none"> <li>Consider a parking management plan in the Northwest Subarea to improve the availability of parking supply and pricing</li> </ul>	Partnership between County of Los Angeles and Major Property Owners/Developers	Mid- to Long- Term
<b>Establish a Business Improvement District (BID)</b>	<ul style="list-style-type: none"> <li>Marketing/promotional events/security/maintenance above existing public services; small scale capital improvements.</li> <li>Assessments must be benefit based.</li> </ul>	Property Owners/Business Owners/DPW	Mid- to Long- Term

1. Short-Term: 0-2 Years; Mid-Term: 2-5 years; and Long-Term: > 5 years;  
Ongoing: Occurring annually over development period.

Source: Stanley R. Hoffman Associates, Inc.

take on the critical function of directing and coordinating various actors and available resources in pursuing targeted economic development opportunities for real estate development and identifying infrastructure and services in coordination with other partners. The CDC could also play a central role in workforce development through skills gap assessments and through leveraging existing programs at the national, state and county levels and instituting new ones, as necessary.

There is currently underway an initial planning and assessment effort for the re-use and/or renovation of the former hospital building at the Martin Luther King, Jr. Community Hospital. The complex is in the process of relocating certain County programs and facilities into the existing building. We understand that the County is leading this effort and that a CDC is in the process of being formed to administer this facility. The Specific Plan recommends the expansion of this future CDC to provide a broader coverage of the Specific Plan area.

**Public Private Partnerships.** Public-private partnerships provide effective means in the current constrained economy to leverage limited resources and achieve mutually desired goals. Potential public-public partnerships between local and regional public agencies include those with specialized institutions in education, health care and research and development such as the Martin Luther King Jr. Community Hospital and Charles Drew University. Partnerships with the private and non-profit sectors enable the integration of local workforce capabilities with the ultimate job growth. Expanding these partnerships will help achieve the Specific Plan goals.

**Los Angeles County Workforce Collaborative.** An initiative is underway (ICIC - Initiative for a Competitive Inner City) to help coordinate national, state and county wide workforce training programs, and for targeted collaboration with key stakeholders, including the private sector. Continuing to build on the work of the Workforce Collaborative helps the Specific Plan goals of improved employment opportunities for Willowbrook residents.

## B. Targeted Development Opportunities

Several specific and targeted development opportunities have been identified for Willowbrook under the TOD Specific Plan, including:

**Leveraging Health Care and Education Opportunities.** Ongoing collaboration with the MLK Community Hospital should be leveraged for spin-off activities in health-care services and allied office uses. Similarly, ongoing partnership with Charles Drew University has the potential to spur research and development activities in healthcare and science programs for employers who want or need to be part of the medical community around MLK.

**Kenneth Hahn Plaza.** The Plaza offers an opportunity for enhancing the retail and dining experience in the community through store location and enhanced design and capital improvements; the shopping center also has the potential for mixed-use, transit oriented development.

## C. Land Use Planning

**Land Assembly.** Land assembly incentives become important, particularly in a post-redevelopment environment, where encouraging the consolidation of smaller parcels into a larger, developable area results in more efficient development. While land assembly incentives are limited in the Willowbrook

area, one incentive is the expedited processing of development that furthers the goals and objectives of the Specific Plan vision.

**Use of Successor Agency Parcels.** The economic development strategy recommends the catalytic use of parcels inherited by the successor agency to the County Redevelopment Agency (RDA) to further incentivize the Specific Plan goals and objectives.

**Reconfiguring Publicly Owned Parcels.** Land resources already owned by agencies should be assessed for reconfiguration and consolidation for new development opportunities. Local public schools in the northwest portion of the Specific Plan area have the potential to be reconfigured to make additional land resources available for private sector development under either a lease or sale arrangement.

## D. Implementation and Financing Strategy

Various financing and implementation methods are viewed as important in the long-term context of the Specific Plan vision for a health and wellness community that builds on MLK and CDU investments and attracts both residential and non-residential development, preferably with a medical employment related focus. These actions, their primary uses, responsible parties for implementation and phasing are summarized in Table 7-2.

Some of the techniques need to start early to build resources for the future, such as an Enhanced Infrastructure Financing District, and others may be implemented after some development has occurred, such as a Parking Management District or a Business Improvement District, but are still considered important as part of the overall strategy. Some of the techniques are exclusively for capital financing or operations and maintenance, and some of these techniques can provide both funding approaches. The strategy also includes the early establishment of public-private partnerships that can foster confidence in the attractiveness and vision of the Specific Plan area.

**Core Strategies.** The core, early strategies include maximizing infrastructure financing through an Enhanced Infrastructure Financing District (EIFD) and establishing a development impact fee program for essential infrastructure, such as parking or street improvements, that spreads the costs equitably over new development. Also considered important in the short-term is a non-profit Community Development Corporation (CDC) that directs the visioning, formulation and implementation of Specific Plan goals and objectives – that builds on the CDC that is currently being established to administer beneficial community programs within MLK surplus property. Additionally, part of the mid- to long-term strategy would be to create a parking management plan that could address both the long-term supply and operations side of the parking system, including unbundling the cost of parking from the underlying rent or lease payments.

Along with maximizing these funding sources, the Capital Improvement Program (CIP) for the Specific Plan, as discussed conceptually in Chapter 7, identifies top community priorities in order to facilitate timely provision of public infrastructure and services.

**Grant Funding Opportunities.** Ongoing grant funding opportunities at the local level include Cap-and-Trade under the Low Carbon Transit Operations Program and the Metro and Transportation Development Act (TDA) funding. The Metro and TDA grant opportunities are generally funded through local sales tax revenue measures on an annual or bi-annual cycle and have their own competitive

**Table 7-2:  
Summary of Potential Financing Techniques**

<b>Financing Methods</b>	<b>Primary Uses</b>	<b>Responsible Parties</b>	<b>Phasing</b>
<b>Development Impact Fee (DIF)</b>	<ul style="list-style-type: none"> <li>Capital improvements necessitated by new development. AB 1600 requires that a reasonable relationship or “nexus” must exist between the amount of the impact fee and the project on which it is imposed.</li> </ul>	County Department of Public Works (LACDPW)	Short- to Mid- Term
<b>Special Benefit Assessment</b>	<ul style="list-style-type: none"> <li>Capital improvements necessitated by new development.</li> <li>Assessments must be benefit based.</li> </ul>	Los Angeles County Treasurer and Tax Collector, Office of Public Finance	Mid- to Long- Term
<b>Landscape Maintenance District (LMD)</b>	<ul style="list-style-type: none"> <li>Landscaping improvements.</li> <li>Landscaping operations and maintenance.</li> <li>LMD assessments must be benefit based.</li> </ul>	Los Angeles County Treasurer and Tax Collector, Office of Public Finance	Mid- to Long- Term
<b>Mello-Roos Special Tax</b>	<ul style="list-style-type: none"> <li>Capital improvements.</li> <li>Operations and maintenance.</li> <li>Can finance projects and services with both local and general benefits.</li> </ul>	Los Angeles County Treasurer and Tax Collector, Office of Public Finance	Mid- to Long- Term
<b>Business Improvement District (BID)</b>	<ul style="list-style-type: none"> <li>Marketing/promotional events/security/maintenance above existing public services; small scale capital improvements.</li> <li>Assessments must be benefit based.</li> </ul>	Property Owners/Business Owners/LACDPW	Mid- to Long- Term
<b>Community Development Block Grant (CDBG)</b>	<ul style="list-style-type: none"> <li>Federal program that provides direct annual grants to cities, counties, and states across the country.</li> <li>Grants are intended to revitalize disadvantaged areas, expand affordable housing and economic opportunities.</li> </ul>	County Department of Regional Planning (DRP)	Mid- to Long- Term
<b>Enhanced Infrastructure Financing District (EIFD)</b>	<ul style="list-style-type: none"> <li>Use of County's future basic 1% property tax increment for financing public improvements and amenities.</li> <li>Requires a 55% popular vote to incur bonded indebtedness; no increase in local property taxes.</li> </ul>	LACDPW	Short- to Mid- Term
<b>Community Revitalization and Investment Authorities (AB 2)</b>	<ul style="list-style-type: none"> <li>Similar to an EIFD, AB2 allows use of the County's basic 1% property tax increment for capital financing.</li> <li>Subject to a majority protest vote and subject to certain conditions related to serving disadvantaged communities.</li> </ul>	LACDPW	Short- to Mid- Term
<b>Community Development Corporation (CDC)</b>	<ul style="list-style-type: none"> <li>Community development corporations (CDCs) are 501(c)(3) non-profit organizations for local areas.</li> <li>Community-based and provides economic development, education, community and affordable housing services.</li> </ul>	County of Los Angeles/Community Based Stakeholders	Short- to Mid- Term

Table 7-2 (Continued)

Financing Methods	Primary Uses	Responsible Parties	Phasing
<b>California Infrastructure and Economic Development Bank</b>	<ul style="list-style-type: none"> <li>Infrastructure State Revolving Fund (ISRF) Program provides economic development financing to agencies and non-profit corporations.</li> <li>Bank has broad authority to issue tax-exempt and taxable revenue bonds.</li> </ul>	Governor's Office of Business and Economic Development	Mid- to Long- Term
<b>Parking Management District</b>	<ul style="list-style-type: none"> <li>Local districts that regulate parking supply and rates to meet the parking needs of the area</li> <li>They can promote transit use, ridesharing, and alternative modes of transportation.</li> </ul>	Partnership between County of Los Angeles and Major Property Owners/Developers	Mid- to Long- Term
<b>Cap-and-Trade (Pursuant to AB 32)</b>	<ul style="list-style-type: none"> <li>AB 32 authorizes the use of market mechanisms through an annual auction of surplus emissions reduction credits to generate funds, a portion of which can be targeted for clean transportation and sustainable community plans.</li> <li>The Low Carbon Transit Operations Program provides funding assistance for transit agencies to reduce GHG gas emissions and serve disadvantaged communities.</li> </ul>	Caltrans in coordination with the California Air Resources Board (CARB)	Ongoing applications for grant funding
<b>Metro and Transportation Development Act (TDA)</b>	<ul style="list-style-type: none"> <li>Two percent of total TDA funds are allocated for bicycle and pedestrian projects</li> </ul>	Metro	Bi-Annual Metro "Call for Projects" application process; applications due in 2016 for funding opportunities in 2017
<b>California Capital Investment Incentive Program (CIIP)</b>	<ul style="list-style-type: none"> <li>Existing State program that provides property tax abatement for up to 15 consecutive years for large capital investment by qualified manufacturing facilities (must exceed \$150 million)</li> </ul>	Resolution by County Board of Supervisors; certification by the Governor's Office of Business and Economic Development	
<b>Other Outside Grants and Loans</b>	<ul style="list-style-type: none"> <li>Potential TOD grants from State's Strategic Growth Council</li> <li>SCAG Sustainable Communities Planning grants</li> <li>Business facade improvement program grants</li> <li>Affordable housing grants</li> </ul>	County of Los Angeles DRP	Ongoing applications for grant funding

1. Short-Term: 0-2 Years; Mid-Term: 2-5 years; and Long-Term: > 5 years;  
Ongoing: Occurring annually over development period.

application process. Other grant funding opportunities are also available at the Federal, State and Regional levels.

Additionally, programs such as the State of California's Infrastructure and Economic Development Bank (I-Bank) provides low-cost loans for qualified local programs; and Community Development Block Grant (CDBG) funds - while CDBG funds have been significantly reduced in recent years - they still provide an ongoing source of funding to meet local priorities as part of the overall strategy.

**Traditional Financing Techniques.** Some of the more traditional financing techniques include: special benefit assessments, Mello-Roos special taxes and landscape lighting districts and are seen as mid- to long-term techniques as significant development begins to occur. The special tax and landscape lighting districts can fund both capital and operations and maintenance costs. These later techniques will be subject to a majority property owner vote in order to establish these districts while a special tax district requires a 2/3 vote.

Additionally, a Business Improvement District (BID) is a public/private partnership created to perform a variety of services to revitalize and improve commercial neighborhoods above existing local governmental services. Currently, there may not be enough business interest to form a BID, but over the mid- to long-term, interest may increase, particularly if businesses see this as a joint effort to efficiently use their collective funding to increase the quality and overall competitiveness of the area.

**Emerging Techniques.** The newly adopted State legislation (September 22, 2015) for a Community Revitalization and Investment Authorities (AB 2) as a partial replacement for redevelopment should be considered, but has some similarities to an EIFD, so both would not be implemented. The Authorities Act does have the power of eminent domain while the EIFD does not; also the Authorities Act has a number of restrictions that relate to serving disadvantaged communities and providing affordable housing. Finally, the County does have a California Capital Incentive Program (CIIP) that provides property tax abatement for up to 15 years for large capital investment by qualified manufacturing facilities (must exceed \$150 million); however, under an EIFD, this program would probably not be applicable as sufficient property tax increment revenues would likely not be available for both purposes.

## 7.5 Community Benefits

Community benefits from the implementation of the Economic Development Strategy are summarized below:

**Identification and Coordination of Economic Opportunities.** One of the pivotal benefits of public-private partnerships is the ongoing leveraging of opportunities that benefit firms/enterprises and the local workforce. A mechanism to implement workforce programs in response to private sector skill demands establishes an approach for realizing the potential for jobs growth and payroll increases within the community.

**Capturing Benefits of Large Public Investments.** Significant investments within Willowbrook in institutional and public transit development are expected to catalyze future growth in residential and non-residential uses over time. However, capturing and actually realizing these projected benefits depends upon programs in place, including those outlined above, such as a public private dialogue and the establishment of the EIFD for capital improvements for community recapture of fiscal benefits

from increased property valuation. Appropriate land use zoning, such as mixed use, could further help incentivize the potential around large public investments.

**Expanded Business Activities and Local Job Opportunities.** A combination of the various initiatives outlined above could result in expanded business activities in spin-off industries associated with health care services and research and development, and an expanded spillover demand for local services, which would also benefit the local community. Land use planning serves a critical role in providing locational opportunities for such activities. The community would benefit from the expansion of better paying jobs in coordination with appropriate skills development initiatives, as discussed previously.

**Better Transit Access and Jobs-Housing Balance.** Public transit investments additionally provide the benefit of improving two-way access to jobs and housing opportunities. The ability of workers to access job opportunities in other parts of the county has the benefit of increasing community prosperity. On the flip side, local firms' ability to access qualified workforce from a larger commute shed is an important factor in locational choice in addition to availability of land. Expanded housing opportunities over time could have the stabilizing benefit of improving the match of jobs and housing within the sub-regional economy. Capital improvements connecting and improving local community access to transportation infrastructure is an important community benefit in this regard to realize the above economic outcomes.

**Affordable Housing.** With the loss of redevelopment agency affordable housing set-aside funds, the decline in available affordable housing funds has been substantial. The County of Los Angeles, through its Housing Element, will continue to address existing and new sources of funding for affordable housing programs, such as new market housing tax credit programs and programs that are currently being considered by the California State legislature, but have not yet been adopted.

**Enhancing Overall Quality of Life.** An overarching community benefit of expanded jobs, access to transit and improved local services is enhanced local quality of life, supported by improved local public infrastructure and public amenities.

**Increasing Public Safety.** The proposed street and access improvements under this Specific Plan would also have the benefit of improving public safety. A neighborhood with expanded business activities, work opportunities, transit access, and housing opportunities would likely reinforce community identity and citizen participation in local events, resulting in greater social and economic cohesion.

# Capital Improvement Program

## 8.1 Introduction

The conceptual capital improvement program (CIP) is summarized in Table 8-1 and outlines the general categories of improvements, estimated costs, responsible agencies and phasing. The total estimated costs are about \$73.3 million. The bulk of these costs are for the Willowbrook/Rosa Parks Station relocation and improvements in the Kenneth Hahn Plaza by Metro. The phasing for this work is identified as short-to-mid range, depending upon when the funding resources become available.

The major local infrastructure and accessibility costs are estimated for Streetscape Improvements at about \$16.8 million and followed by about \$3.5 million for Transportation-related Improvements. These improvements will be overseen primarily by the County of Los Angeles Department of Public Works (LACDPW) and funded by a variety of other sources that are presented in this chapter. The phasing for this work is estimated for short-to-mid to long-range. Again, this would depend on the availability of funding, particularly if the newly adopted Enhanced Infrastructure Financing District (EIFD) legislation is used; this is because the EIFD's bonding capacity will increase over time as property tax increment increases with new development.

Finally, Public Parking Facility Improvements are identified as possible incentives for cost reductions for private sector development within the northwest quadrant. However, no specific plans or garage cost estimates are available at this time. An option to full construction of a public garage might be cost sharing garage subsidies with the private sector. The phasing of these costs is seen as Mid-to-Long-Range because it is assumed that it will take five (5) years or longer to create public infrastructure and amenities to begin to draw private sector investment.

## 8.2 Potential CIP Improvements

A detailed listing of potential transportation and related improvements is presented in Table 8-2 along with an identification of responsible lead and partner agencies, potential funding sources and phasing. Table 8-3 presents the potential utility improvements by private or enterprise utility companies also with their responsible lead companies and partner agencies, potential funding sources and phasing.

**Table 8-1:  
Summary of Capital Improvement Plan Priorities**

Improvement	Estimated Cost	Responsible Agency	Phasing <sup>1</sup>
Land Assembly <sup>2</sup>	N/A	County and School Districts	Short-to-Mid- Term
Transit-Related/Transit-Station Improvements	\$ 53,000,000	Metro <sup>3</sup>	Short-to-Mid- Term
Transportation Improvements	\$ 3,456,500	LACDPW	Short-to-Mid- Term
Streetscape Improvements	\$ 16,835,315	LACDPW	Short-to-Mid- Term
Public Parking Garage Improvements/Subsidies	N/A	Potential Parking District <sup>4</sup>	Mid-to-Long- Term
<b>TOTAL</b>	<b>\$ 73,291,815</b>		

1. Short-Term: 0-2 Years

Mid-Term: 2-5 Years

Long-Term: >5 Years

2. Potential land resources are available from the successor agency to redevelopment and from public school districts possibly reconfiguring their local campuses.

3. About \$10.25 million from the U.S. Department of Transportation (DOT) TIGER competitive grant funds is currently available; TIGER is an acronym for Transportation Investment Generating Economic Recovery.

4. A parking management district will incentivize private development through reduction in parking costs and management of parking supply.

Source: Stanley R Hoffman Associates, Inc.

**Table 8-2: Streetscape and Transportation: Improvements, Responsibility, Potential Funding Sources**

Improvements	Responsibility - Lead and Partner Agency <sup>1</sup>	Potential Funding Sources <sup>1</sup>	Phasing <sup>2</sup>
<b>Transit-related Improvements</b>			
Upgraded Metro Rosa Parks Transit Station	Metro	Metro	Short-Term
<b>Vehicular/Pedestrian Improvements</b>			
Road diet on Mona Boulevard	LACDPW	<ul style="list-style-type: none"> <li>• Metro Call for Projects</li> <li>• Cap and Trade</li> <li>• Los Angeles County Transportation Funds</li> <li>• Federal and State grants and loans</li> <li>• Transportation Development Act Funds</li> </ul>	Mid-Term
Road diet on Willowbrook Avenue West (between the Metro Station and 119th Street)	LACDPW		Mid-Term
Traffic signal - intersection of Mona Blvd. and 119 <sup>th</sup> Street - <i>Traffic signal analysis and subsequent implementation</i>	LACDPW		Short-Term
Traffic signal - intersection of Wilmington and 122 <sup>nd</sup> Street - <i>Traffic signal analysis and subsequent implementation</i>	LACDPW		Mid-Term
Pedestrian signal - intersection of Mona Blvd. and 120 <sup>th</sup> Street - <i>Pedestrian signal analysis and subsequent implementation</i>	LACDPW		Short-Term
Pedestrian sidewalk paving on the west side of Willowbrook Avenue West between Metro Station & 119 <sup>th</sup> Street	LACDPW / Metro		Mid-Term
Pedestrian sidewalk improvements on Wilmington Avenue between Imperial Highway and 117th Street (both sides)	LACDPW		Mid-Term
Pedestrian sidewalk improvements along 119th Street between Wilmington Avenue and Willowbrook West (north side only)	Private development		Mid-Term
Pedestrian Oriented Improvements Package (Standard Improvement package) – 12 locations <i>Standard Improvement package includes:</i> - add zebra-style crosswalks to all crossings - add pedestrian countdown signals and audio signals to all crossings - add passive pedestrian detection to all crossings - add advanced stop bars to all approaches - add bulb-outs or reduce curb returns on corners where feasible	LACDPW		Mid-Term

**Table 8-2 (Continued)**

<b>Bicycle Improvements</b>			
Class I bicycle path and pedestrian trail on Mona Boulevard	LACDPW	<ul style="list-style-type: none"> <li>• Metro</li> <li>• DPW</li> </ul>	Mid-Term
Class I bicycle path/Class IV cycle track on Willowbrook Avenue West	Metro/DPW	<ul style="list-style-type: none"> <li>• Cap and Trade</li> <li>• Federal and State Grants</li> </ul>	Short-Term
Class II bicycle lane on Imperial Highway between Compton Avenue and Mona Blvd.	LACDPW		Mid-Term
Class II bicycle lane on Wilmington Avenue between 120th and 124th Streets	LACDPW		Mid-Term
Class III bicycle route signage/stripping on Compton Avenue, 119 <sup>th</sup> Street (between Wilmington & Mona), and Willowbrook Avenue West (south of 119 <sup>th</sup> Street)	LACDPW		Mid-Term
Implement Bike Stations	MLK, CDU, Private Development		Short- to Mid- Term
<b>Streetscape Enhancements</b>			
Wilmington/119th/120th Streetscape Project (DPW-funded)	LACDPW	<ul style="list-style-type: none"> <li>• Metro</li> <li>• DPW</li> </ul>	Short- to Mid-Term
Street trees on Specific Plan streets not included above	LACDPW	<ul style="list-style-type: none"> <li>• BID</li> </ul>	Long-Term
Pedestrian-scaled lighting on Specific Plan streets not included above	LACDPW	<ul style="list-style-type: none"> <li>• Landscape and Lighting District</li> </ul>	Long-Term
Street furniture	LACDPW		Mid-Term
<b>Other Mobility Related Actions</b>			
Community contest to rename 120th Street between Compton and Wilmington Avenues	LACDPW	<ul style="list-style-type: none"> <li>• Metro</li> <li>• Los Angeles County</li> <li>• CDC</li> </ul>	Short Term
Setup Transportation Demand Management Program	LACDPW		Short Term
<b>Parking</b>			
Parking Management District	LACDPW	<ul style="list-style-type: none"> <li>• Los Angeles County, EIFD</li> <li>• Future parking revenues</li> </ul>	Mid-Term

1. Metro is Los Angeles County Metropolitan Transit Authority  
 LACDPW is the Los Angeles County Department of Public Works  
 BID refers to Business Improvement District  
 CDC refers to Community Development Corporation  
 EIFD refers to Enhanced Infrastructure Financing District
2. Phasing: Short Term: 0-2 years; Mid-Term: 2-5 years; Long Term: >5 years  
 Source: Stanley R. Hoffman Associates, Inc., The Arroyo Group, The Mobility Group

**Table 8-3: Utilities: Improvements, Responsibility, Potential Funding Sources**

Improvements	Responsibility - Lead and Partner Agency <sup>1</sup>	Potential Funding Sources	Phasing <sup>1</sup>
Evaluation of the need for water distribution lines upgrades in the Specific Plan area	<i>Primary responsibility:</i> Park Water Company and Golden State Water Company <i>Secondary responsibility:</i> LACDPW	Water Companies	Short- to Mid-Term
Detailed sewer study for Group 1 - LACDPW	LACDPW and MLK Community Hospital	LACDPW through impact fees and connection charges	At time of development in Group 1
Other sewer related review by required by LACSD	Developers	Development Impact Fees	Ongoing
Evaluation of demand for electrical services	<i>Primary responsibility:</i> SCE <i>Secondary responsibility:</i> LACDPW	SCE through their utilities charges, as well as state and federal grants and loans	Ongoing
Evaluation of demand for telecommunication services	<i>Primary responsibility:</i> AT&T <i>Secondary responsibility:</i> LACDPW	AT&T funded through monthly phone charges	Ongoing
Evaluation of demand for cable services	<i>Primary responsibility:</i> TWC <i>Secondary responsibility:</i> LACDPW	TWC through their utilities charges, as well as state and federal grants and loans	Ongoing
Joint trenching with all utility providers	<i>Primary responsibility:</i> SCE <i>Secondary responsibility:</i> LACDPW oversight and other private dry utilities companies	SCE in coordination with other utility providers	Ongoing

1. *Short term:* 0-2 years  
*Mid-term:* 2-5 years  
*Long term:* >5 years

Source: Stanley R. Hoffman Associates, Inc., JMC<sup>2</sup> Civil Engineering