

It is recommended that this tentative map not be approved at this time. This recommendation is based upon information or lack of information that is available concerning the subject property. The removal of this recommendation is contingent upon the submission and satisfactory review of the following:

1. Change “A” street from “Private” to “Private and Future”.
2. An approve drainage concept, Low Impact Development Plans and Standard Urban Stormwater Mitigation Plan. Please see attached Storm Drain and Hydrology review sheet (Comments 1 to 4, and 6) for requirements. The above mentioned plans shall be submitted directly to Public Works.
3. An approved engineering geologic report. Please see attached Geologic review sheet. The engineering geologic report shall be submitted directly to Public Works and a review fee is also required.
4. An approved soils report. Please see attached Soils Engineering review sheet. The soils report shall be submitted directly to Public Works and a review fee is also required.
5. Please see attached Grading review sheet (Comments 2 to 4) for requirements.
6. Please see attached Road review sheet (Comments 1 and 6) for requirements.
7. Provide a copy of the water will serve letter from the water purveyor. Please see attached Water review sheet (Comment 1) for requirements.
8. A revised tentative map and a revised exhibit map are required to show the following additional items:
 - a. Provide street address (if known) and Assessor Parcel Number(s).
 - b. Provide easement status. Label all easements as “to remain”, “to be relocated”, or “to be abandoned”. If easement is to be abandoned, indicated the proposed timing of the abandonment.
 - c. Please see attached Storm Drain and Hydrology review sheet (Comment 5) for additional requirements.
 - d. Please see attached Grading review sheet (Comments 1 and 4) for requirements.

- d. Please see attached Grading review sheet (Comments 1 and 4) for requirements.
- e. Please see attached Road review sheet (Comments 1 to 5) for requirements.
- f. Please see attached Sewer review sheet for additional requirements.
- g. Please see attached Water (Comment 2) review sheet for additional requirements.

HW

JC

Prepared by John Chin
Tr71892L-new.doc

Phone (626) 458-4918

Date 06-18-2012



COUNTY OF LOS ANGELES DEPARTMENT OF PUBLIC WORKS
LAND DEVELOPMENT DIVISION
ENGINEERING AND SURVEY BRANCH
STORM DRAIN AND HYDROLOGY SECTION

TRACT MAP NO. 71892

TENTATIVE MAP DATED: 05/17/2012

Approval and clearance of the tentative map is subjected to compliance with the following **drainage** comments:

- 1. Portions of the property with proposed improvement that are lying in and adjacent to the natural drainage courses and are subject to flood hazard.
- 2. A Low Impact Development Plans (LID) (as part of the drainage concept) is required prior to the tentative approval of the map.
 - Per County Code Section 12.84.460 comply with LID requirements and provide calculations in accordance with the Low Impact Development Standards Manual which can be found at [http://dpw.lacounty.gov/wmd/LA County LID Manual.pdf](http://dpw.lacounty.gov/wmd/LA_County_LID_Manual.pdf)
- 3. Prior to tentative map approval for drainage, submit a drainage concept showing the extent of drainage impacts and provide mitigation acceptable to the County. The analysis should address increases in runoff, any change in drainage patterns, debris producing areas, and the capacity of existing storm drain facilities. Provide line identification of all proposed drainage facilities. Preliminary soils and geology reports related to debris, retention, and detention basins may be required based on geographic and adverse geotechnical conditions. Provide engineering calculations to support sizing of debris, retention, and detention basins. Provide approximate flood hazard and bank erosion setbacks and lot identifications (as needed). Show slopes for existing and proposed streets. Provide a drainage/grading covenant for any offsite work.
- 4. Prior to tentative map approval for drainage, submit a revised tentative map showing clear and legible contour lines, existing offsite elevations and proposed onsite elevations at the property line, offsite drainage patterns, and existing & proposed drainage patterns. Show and label all existing Los Angeles County drainage systems and easements.
- 5. Prior to tentative map approval for drainage, submit an exhibit map showing building footprints, pad elevations, and proposed drainage and grading patterns.
- 6. Comply with Interim Peak Flow Policy, adopted by the Regional Water Quality Control Board February 1, 2005. The Peak Flow Standard shall require that all post-development runoff from a 2-year, 24-hour storm shall not exceed the pre-development peak flow rate, burned, from a 2-year, 24-hour storm. The Peak Flow Standard shall also require that post-development runoff from the 50-year capital storm shall not exceed the pre-development peak flow rate, burned and bulked, from the 50-year capital storm (**required for projects where drainage discharges to natural drainage course AND only for new subdivisions or Revised maps with application date after 02/01/05**).

* Effective July 30, 1999 all development projects shall comply with Standard Urban Stormwater Mitigation Plans (SUSMP) requirements. For information see SUSMP web page address: <http://888cleanla.com>. The first 0.75 inches of stormwater runoff volume from the site must be treated prior to discharge into stormwater conveyance system.

Reviewed by Eden Berhan Date 06/12/12 Phone (626) 458-4921
EDEN BERHAN

County of Los Angeles Department of Public Works
GEOTECHNICAL AND MATERIALS ENGINEERING DIVISION
GEOLOGIC REVIEW SHEET
900 So. Fremont Ave., Alhambra, CA 91803
TEL. (626) 458-4925

DISTRIBUTION
___ Geologist
___ Soils Engineer
1 GMED File
1 Subdivision

TENTATIVE TRACT MAP 71892
SUBDIVIDER The Roman Catholic Archbishop of L. A.
ENGINEER CRC Enterprises
GEOLOGIST ---
SOILS ENGINEER ---

TENTATIVE MAP DATED 5/17/12 (Tentative)
LOCATION Acton
REPORT DATE ---
REPORT DATE ---

The Regional Planning Commission, developer, and engineer are advised that:

PRIOR TO RECOMMENDING APPROVAL OF THE TENTATIVE TRACT OR PARCEL MAP:

1. An engineering geology report is required to evaluate the feasibility of the proposed subdivision.
2. Based on the State of California Seismic Hazard Maps, the subject site is located in an area with a potential for liquefaction and may be subject to secondary effects of seismic shaking. In accordance with California Public Resources Code § 2697 and California Code of Regulations § 3724, prior to the approval of a project in a seismic hazard zone, a geotechnical report defining and delineating any seismic hazard must be submitted for review and approval. The report must address the potential for liquefaction and ground failure, and must comply with the provisions of the "Manual for Preparation of Geotechnical Reports" prepared by the County of Los Angeles, Department of Public Works, (available on the internet at <http://dpw.lacounty.gov/gmed/Manual.pdf>), and DMG Special Publication 117. Provide this office with two (2) original copies of the report for review and distribution to the State of California.

All parameters and data utilized in the liquefaction analysis must comply with the requirements of the Geotechnical and Materials Engineering Division's Administrative Manual memo G045.0.

3. Effective August 1, 2006, all geotechnical reports submitted for review must include an electronic copy of the report on a CD in Adobe® Portable Document Format (PDF). The electronic version shall include an electronically generated representation of the licensee's seal, signature, and date of sealing or signing. This project cannot be approved until this requirement has been met. The submittal in response to this review must include a CD containing an electronic version of the original report and the supplemental report in response to this review.
4. The Soils Engineering review dated 6/6/12 is attached.

NOTE Provide a copy of this review with your resubmittal

Prepared by

Geir Mathisen

Date 6/4/12

COUNTY OF LOS ANGELES
DEPARTMENT OF PUBLIC WORKS
GEOTECHNICAL AND MATERIALS ENGINEERING DIVISION

SOILS ENGINEERING REVIEW SHEET

Address: 900 S. Fremont Ave., Alhambra, CA 91803
Telephone: (626) 458-4925
Fax: (626) 458-4913

District Office N/A
Job Number LX001129 / 1hrs
Sheet 1 of 1

Tentative Tract Map 71892
Location Acton
Developer/Owner The Roman Catholic Archbishop of Los Angeles
Engineer/Architect CRC Enterprises
Soils Engineer _____
Geologist _____

DISTRIBUTION:
____ Drainage
____ Grading
____ Geo/Soils Central File
____ District Engineer
____ Geologist
____ Soils Engineer
____ Engineer/Architect

Review of:

Tentative Parcel Map Dated by Regional Planning 5/17/12

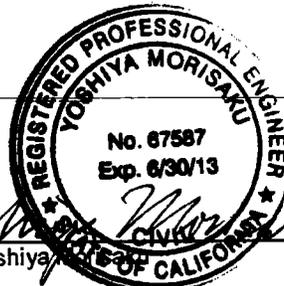
ACTION:

Tentative Map feasibility is not recommended for approval.

REMARKS:

1. Provide a soils report, with sufficient subsurface exploration data, pertinent test results and analyses, which addresses and evaluates the site and the proposed development. The report must comply with the provisions of "Manual for Preparation of Geotechnical Reports" prepared by County of Los Angeles, Department of Public Works. The Manual is available on the Internet at the following address: <http://ladpw.org/construction/manual.pdf>
2. The site is located within a mapped liquefiable area, per the State of California Seismic Hazard Zone Map, Acton Quadrangle. Therefore, provide data and analyses to determine liquefaction potential of the on-site soils. Also, evaluate the potential for seismically induced settlement (dry and saturated soils), lateral spreading, surface manifestation, etc. The analyses must be performed for soils within the upper 50 feet, as a minimum, for shallow foundation, or greater depth where deep foundation and/or subterranean structure is proposed. The historic-high water table shall be used in the analyses, unless other information is provided which indicates a higher or lower level is appropriate. Recommend mitigation as necessary. The liquefaction data and analyses must conform to the State of California Division of Mines and Geology "Special Publication 117A", dated 2008 and "Recommended Procedure For Implementation of DMG Special Publication 117", dated March 1999.
3. Provide chemical test results (sulfate, chloride, resistivity, etc.) for the on-site soils to address the presence of chemicals deleterious to concrete and ferrous materials. The chemical tests must be in accordance with California Test Methods, Department of Transportation, or equivalent (aqueous solution tests, such as EPA Tests or similar methods are not acceptable for determination of resistivity). Resistivity tests must be performed on soils in a saturated condition.
4. Show the following on the geotechnical maps:
 - a. Approximate limits and depth of removal and recompaction of unsuitable soils.
 - b. Grading required for construction of buttress/stabilization fills, as necessary.
 - c. All standard notes regarding fill compaction and density testing requirements.
 - d. All recommended mitigation measures (including liquefaction).
5. Requirements of the Geology Section are attached.
6. Include a copy of this review sheet with your response.

Reviewed by _____



Yoshiya Morisaku

Date 6/6/12

NOTICE: Public safety, relative to geotechnical subsurface exploration, shall be provided in accordance with current codes for excavations, Inclusive of the Los Angeles County Code, Chapter 11.48, and the State of California, Title 8, Construction Safety Orders.
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It is recommended that this tentative map not be approved at this time. This recommendation is based upon information or lack of information that is available concerning the subject property. The removal of this recommendation is contingent upon the submission and satisfactory review of the following:

1. A revised tentative map and/or exhibit map is required to show the following additional items:
 - a. Pad elevations for all proposed rough grading and finished floor elevations for all proposed precise grading.
 - b. Retaining wall information (if required). All retaining walls should be labeled and dimensioned with the height provided at the tallest point in the wall and any points where the height changes.
 - c. Earthwork volume, including cut, fill, import, and export, as applicable.
 - d. Slope set back as required per grading ordinance (J108).
 - e. Place proposed property lines at the top of the slope.
 - f. All existing on-site public and private easements with names of the holders, document numbers and recorded dates. Label all easements as "to remain", "to be relocated", or "to be abandoned. If applicable, add a general note that no private easements exist on the site.
 - g. Grading limits for "A" Street (show grading limits and cross-section detail).
 - h. Reconcile grading quantities shown on the tentative map and subdivision application (grading quantities shall include cut, fill, and over-excavation). If the grading information on the subdivision application is incorrect, resubmit a revised application.
 - i. Indicate maintenance responsibilities for all drainage devices.
 - j. Indicate maintenance responsibilities for all slopes to be landscaped per grading ordinance (J110).
2. Approval of the latest drainage concept/hydrology/Standard Urban Stormwater Mitigation Plan (SUSMP)/Low Impact Development (LID) plan by the Storm Drain and Hydrology Section of Land Development Division.

3. A breakdown of earthwork volumes should be shown on the map if phased grading is proposed. Earthwork quantities performed with each unit phase should be consistent with, and considered a fraction of, the overall proposed earthwork quantity for the entire tract. Construction staging or phased grading should also be addressed through the CEQA document associated with the project.
4. Provide a grading exhibit to demonstrate feasibility of future development by showing enough information/detail to prove that the future grading footprints will be within the tract boundaries and that reasonable future driveway grades can be constructed to the satisfaction of Public Works (provide existing and proposed elevations on Crown Valley Road and "A" Street).

 Name Tony Hui Date 06/12/2012 Phone (626) 458-4921
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It is recommended that this tentative map not be approved at this time. This recommendation is based upon information or lack of information that is available concerning the subject property. The removal of this recommendation is contingent upon the submission and satisfactory review of the following:

- 1 Change "A" street from "Private" to "Private and Future".
- 2 Please accurately show on the tentative map the horse trail easement and ensure that trail crossing on Crown Valley Road, in the vicinity of the project, will only be allowed at the intersection of Crown Valley and "A" Street.
- 3 Call out the existing and proposed property line annotation "P/L" on Crown Valley Road and along its boundaries with the adjacent properties.
- 4 Modify note # 11 to remove the "Exhibit map" reference since it is not included in the package for review.
- 5 If applicable, show and call out all public and private easements. Label all easements as "to remain", "to be relocated", or "to be abandoned". If easement is to be abandoned, indicate the proposed timing of the abandonment.
- 6 Submit a conceptual striping plan for Crown Valley Road to the satisfaction of Public Works.



Prepared by Sam Richards
tr71892r

Phone (626) 458-4921

Date 06-18-2012

TRACT MAP NO. 71892

TENTATIVE MAP DATED 05-17-2012

It is recommended that this tentative map not be approved at this time. This recommendation is based upon information or lack of information that is available concerning the subject property. The removal of this recommendation is contingent upon the submission and satisfactory review of the following:

- A revised tentative and exhibit map is required to show the following additional items:
 - a. If applicable, show and call out all existing on-site public and private easements with names of the holders, document numbers and recorded dates. Label all easements as “to remain”, “to be relocated”, or “to be abandoned”. If easement is to be abandoned, indicate the proposed timing of the abandonment. If there are no existing on-site public and private easements, add the annotation “No existing on-site public and private easements” on the tentative map.
 - b. Show location of existing/proposed sewer main lines or private sewer system to serve the proposed development.
 - c. Show easements to be dedicated for sanitary sewer purposes (if any).



Prepared by Tony Khalkhali

tr71892s-new.doc

Phone (626) 458-4921

Date 06-12-2012

TRACT NO. 71892

TENTATIVE MAP DATED 05-17-2012

It is recommended that this tentative map not be approved at this time. This recommendation is based upon information or lack of information that is available concerning the subject property. The removal of this recommendation is contingent upon the submission and satisfactory review of the following:

- (1) Provide "Will Serve Letter" from the water purveyor indicating that the water system will be operated by the purveyor, and that under normal conditions, the system will meet the requirements for the land division, and that water service will be provided to each building.
- (2) A revised tentative and exhibit map is required to show the following information:
 - a. If applicable, show and call out all existing on-site public and private easements with names of the holders, document numbers and recorded dates. Label all easements as "to remain", "to be relocated", or "to be abandoned". If easement is to be abandoned, indicate the proposed timing of the abandonment. If there are no existing on-site public and private easements, add the annotation "No existing on-site public and private easements" on the tentative map.
 - b. Show location of existing/proposed water main lines to serve the proposed development and call out the proposed points of connection.
 - c. Show how the on-site proposed development is to be served by proposed public water and call out the proposed points of connection.



Prepared by Tony Khalkhali
tr71892w-new.doc

Phone (626) 458-4921

Date 06-12-2012

COUNTY OF LOS ANGELES
DEPARTMENT OF PUBLIC WORKS
LAND DEVELOPMENT DIVISION – SUBDIVISION
TRACT NO. 71892

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TENTATIVE MAP DATED 05-17-2012

- If this recommendation of disapproval is changed to a recommendation of approval based on additional information, the following reports would be recommended for inclusion in the conditions of tentative approval:

Prepared by  John Chin
Tr71892L-new.doc

Phone (626) 458-4918

Date 06-13-2012

The following reports consisting of ___ pages are the recommendations of Public Works.

The subdivision shall conform to the design standards and policies of Public Works, in particular, but not limited to the following items:

1. Details and notes shown on the tentative map are not necessarily approved. Any details or notes which may be inconsistent with requirements of ordinances, general conditions of approval, or Department policies must be specifically approved in other conditions, or ordinance requirements are modified to those shown on the tentative map upon approval by the Advisory agency.
2. Easements are tentatively required, subject to review by the Director of Public Works to determine the final locations and requirements.
3. Easements shall not be granted or recorded within areas proposed to be granted, dedicated, or offered for dedication for public streets, highways, access rights, building restriction rights, or other easements until after the final map is filed with the Registrar-Recorder/County Clerk's Office. If easements are granted after the date of tentative approval, a subordination must be executed by the easement holder prior to the filing of the final map.
4. In lieu of establishing the final specific locations of structures on each lot/parcel at this time, the owner, at the time of issuance of a grading or building permit, agrees to develop the property in conformance with the County Code and other appropriate ordinances such as the Building Code, Plumbing Code, Grading Ordinance, Highway Permit Ordinance, Mechanical Code, Zoning Ordinance, Undergrounding of Utilities Ordinance, Water Ordinance, Sanitary Sewer and Industrial Waste Ordinance, Electrical Code, and Fire Code. Improvements and other requirements may be imposed pursuant to such codes and ordinances.
5. All easements existing at the time of final map approval must be accounted for on the approved tentative map. This includes the location, owner, purpose, and recording reference for all existing easements. If an easement is blanket or indeterminate in nature, a statement to that effect must be shown on the tentative map in lieu of its location. If all easements have not been accounted for, submit a corrected tentative map to the Department of Regional Planning for approval.
6. Quitclaim or relocate easements running through proposed structures.

7. Adjust, relocate, and/or eliminate lot lines, lots, streets, easements, grading, geotechnical protective devices, and/or physical improvements to comply with ordinances, policies, and standards in effect at the date the County determined the application to be complete all to the satisfaction of Public Works.
8. A final tract map must be processed through the Director of Public Works prior to being filed with the Registrar-Recorder/County Clerk's Office.
9. Prior to submitting the tract map to the Director of Public Works for examination pursuant to Section 66442 of the Government Code, obtain clearances from all affected Departments and Divisions, including a clearance from the Subdivision Mapping Section of the Land Development Division of Public Works for the following mapping items; mathematical accuracy; survey analysis; and correctness of certificates, signatures, etc.
10. A final guarantee will be required at the time of filing of the final map with the Registrar-Recorder/County Clerk's Office.
11. Within 30 days of the approval date of this land use entitlement or at the time of first plan check submittal, the applicant shall deposit the sum of \$2,000 (Minor Land Divisions) or \$5,000 (Major Land Divisions) with Public Works to defray the cost of verifying conditions of approval for the purpose of issuing final map clearances. This deposit will cover the actual cost of reviewing conditions of approval for Conditional Use Permits, Tentative Tract and Parcel Maps, Vesting Tentative Tract and Parcel Maps, Oak Tree Permits, Specific Plans, General Plan Amendments, Zone Changes, CEQA Mitigation Monitoring Programs and Regulatory Permits from State and Federal Agencies (Fish and Game, USF&W, Army Corps, RWQCB, etc.) as they relate to the various plan check activities and improvement plan designs. In addition, this deposit will be used to conduct site field reviews and attend meetings requested by the applicant and/or his agents for the purpose of resolving technical issues on condition compliance as they relate to improvement plan design, engineering studies, highway alignment studies and tract/parcel map boundary, title and easement issues. When 80% of the deposit is expended, the applicant will be required to provide additional funds to restore the initial deposit. Remaining balances in the deposit account will be refunded upon final map recordation

HW

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Phone (626) 458-4918

Date 06-13-2012

Preliminary Road Condition

The subdivision shall conform to the design standards and policies of Public Works, in particular, but not limited to the following items:

1. A minimum centerline curve length of 100 feet shall be maintained on all local streets. A minimum centerline curve radius of 100 feet shall be maintained on all cul-de-sac streets. Reversing curves of local streets need not exceed a radius of 1,500 feet, and any curve need not exceed a radius of 3,000 feet.
2. The minimum centerline radius is 350 feet on all local streets with 64 feet of right of way and on all the streets where grades exceed 10 percent.
3. Compound curves are preferred over broken-back curves. Broken-back curves must be separated by a minimum of 200 feet of tangent (1,000 feet for multi-lane highways or industrial collectors). If compound curves are used, the radius of the smaller curve shall not be less than two-thirds of the larger curve. The curve length of compound curves shall be adjusted to exceed a minimum curve length of 100 feet, when appropriate.
4. Curves through intersections should be avoided when possible. If unavoidable, the alignment shall be adjusted so that the proposed BC and EC of the curve through the intersection are set back a minimum of 100 feet away from the BCR's of the intersection.
5. The central angles of the right of way radius returns shall not differ by more than 10 degrees on local streets.
6. Driveways will not be permitted within 25 feet upstream of any catch basins when street grades exceed 6 percent.
7. Provide minimum landing area of 100 feet for local collectors, 50 feet for local access roads, and 25 feet for cul-de-sacs at a maximum 3 percent grade on all "tee" intersections to the satisfaction of Public Works.
8. Provide property line return radii of 27 feet at the intersection of local streets with planned highways (those on the County Highway Plan) and where all planned highways intersect or where one of the roads serves a commercial or industrial development plus additional right of way for corner cut off to meet current guidelines of the Americans with Disabilities Act (ADA) to the satisfaction of Public Works.

9. Dedicate right of way 40 feet from centerline on Crown Valley Road. Additional slope and drainage easements may be required.
10. Make an offer of private and future right of way 29 feet from centerline plus additional right of way for a standard cul-de-sac bulb on "A" Street (private and future street).
11. Whenever there is an offer of a future street or a private and future street, provide a drainage statement/letter as well as slope and drainage easements as applicable to the satisfaction of Public Works.
12. Dedicate vehicular access rights on Crown Valley Road along the entire project frontage.
13. Construct improvements, using rural section including concrete inverted shoulder, base and pavement for widening of Crown Valley Road, along with pavement transition, commensurate with the approved signing and striping plan to the satisfaction of Public Works.
14. Construct improvements using rural section including concrete inverted shoulder, base and pavement on "A" Street to the satisfaction of Public Works.
15. The street typical sections on the tentative map are not necessarily approved as shown. It shall conform with the final design requirement to the satisfaction of Public Works.
16. Provide detailed 40-foot scale signing and striping plan for Crown Valley Road consistent with the approved conceptual striping plan to the satisfaction of Public Works.
22. Construct drainage improvements (and parkway drains, if needed) and offer easements needed for street drainage or slopes to the satisfaction of Public Works. Where streets or highways are located within flood hazard areas or subject to inundation, provide adequate freeboard and slope protection to the satisfaction of Public Works. Construct adequate embankment protection along any sections of highways or streets located within flood plain boundaries or subject to inundation. Adequate freeboard shall also be provided.
23. Depict all line of sight easements on the landscaping and grading plans.
24. Install postal delivery receptacles as approved by the United States Postal Service

(this may require receptacles to be placed in groups).

25. Underground all existing service lines and distribution lines that are less than 50 KV and new utility lines to the satisfaction of Public Works and Southern California Edison. Please contact Construction Division at (626) 458-3129 for new location of any above ground utility structure in the parkway.
26. Provide and install street name signs prior to the occupancy of buildings.
27. Prior to final map approval, the subdivider shall enter into an agreement with the County franchised cable TV operator (if an area is served) to permit the installation of cable in a common utility trench to the satisfaction of Public Works; or provide documentation that steps to provide cable TV to the proposed subdivision have been initiated to the satisfaction of Public Works.



Prepared by Sam Richards
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Date 06-18-2012