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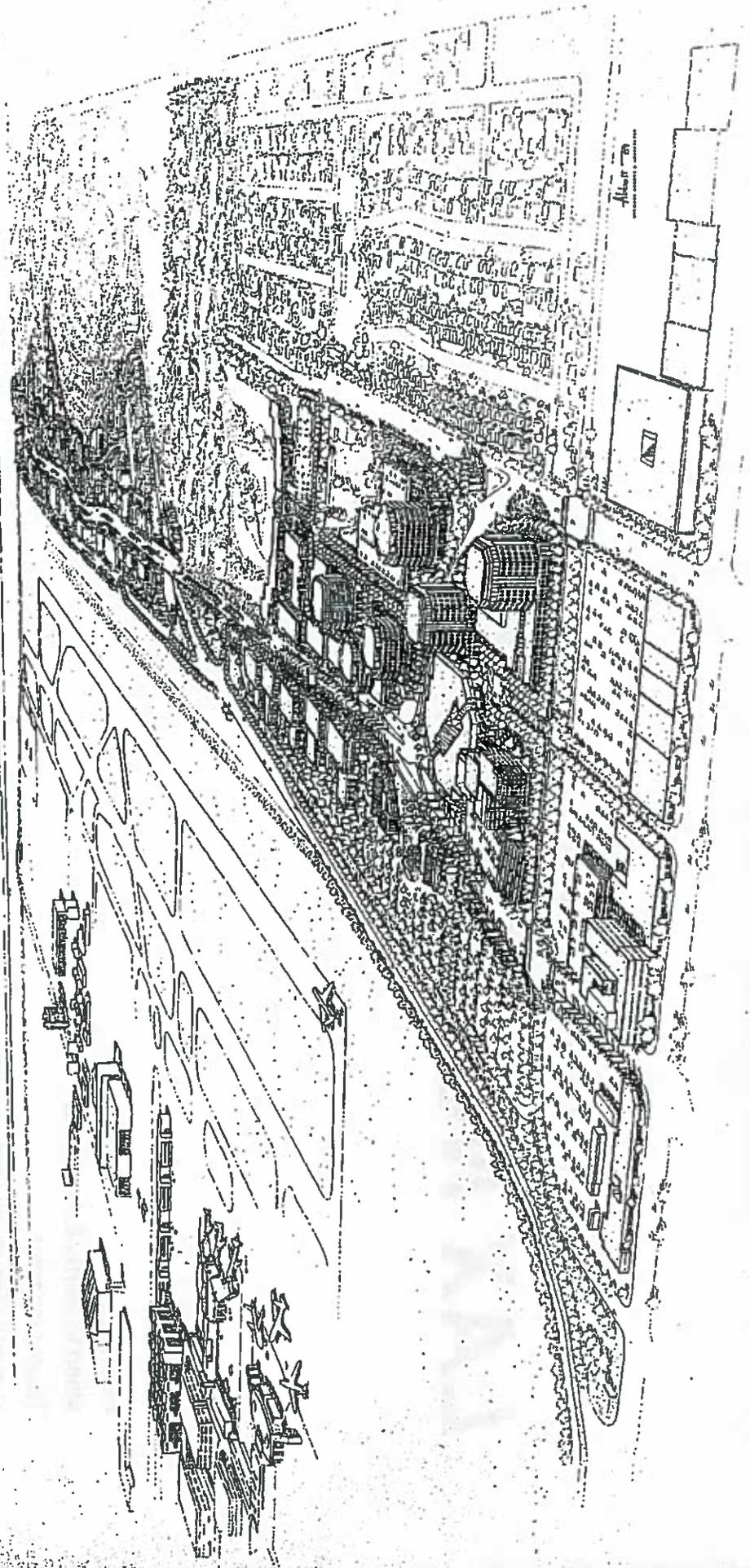
AIA and Noise Contour Map

Attachment B:

1989 LAX Northside Design Plan and Development Guidelines

Design Plan and Development Guidelines

LAX NORTHSIDE



Design Plan and Development Guidelines

LAX NORTHSIDE

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Prepared For:

The City of Los Angeles

Department of Airports

April 20, 1989

LAX NORTHSIDE

Design Plan and Development Guidelines

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Design Plan and Development Guidelines

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1.0 INTRODUCTION

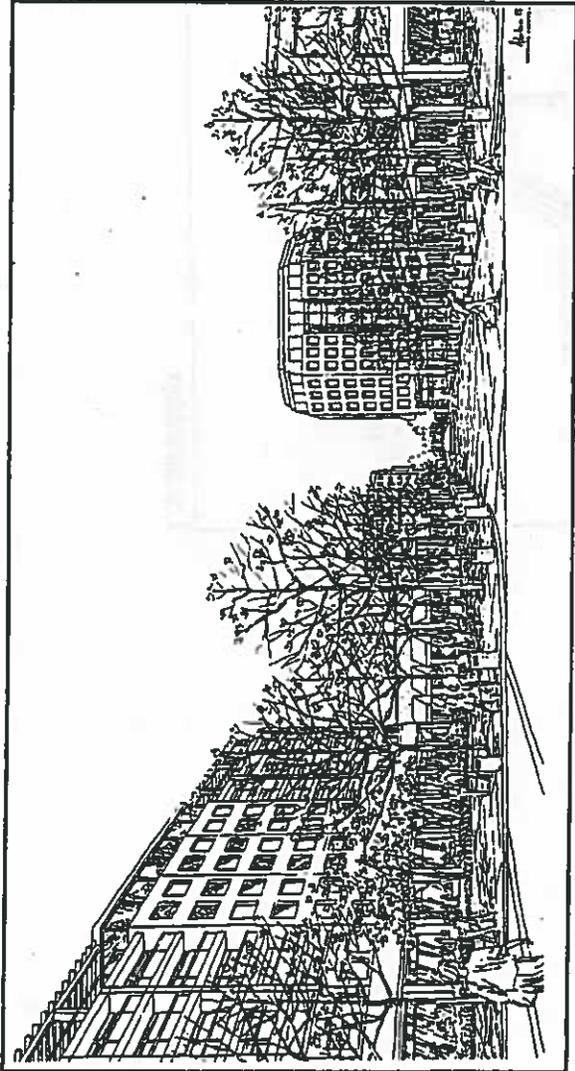




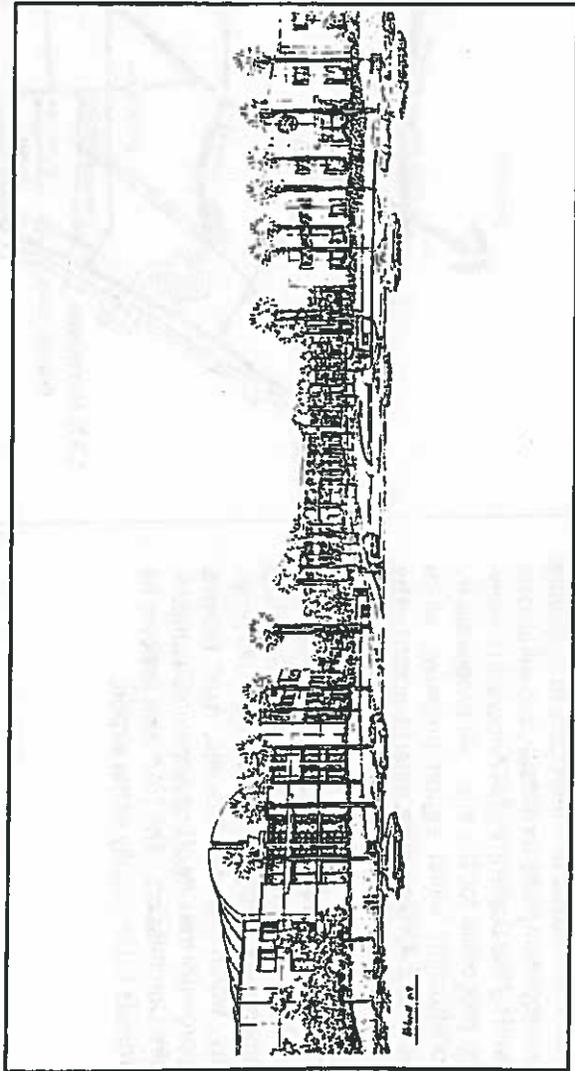
LAX Northside is a new business community and commercial center which will be built on City of Los Angeles Department of Airport (DOA) property north of the north runway at Los Angeles International Airport. The 350 acre development will consist of a balanced mix of office and research park, recreation, hotel, restaurant, and airport-related land uses along Westchester Parkway, a new, landscaped arterial highway being built across the property. At the center of LAX Northside lies a redesigned, refurbished 18 hole public golf course.

The site extends nearly two and a half miles from the Westchester business district to Pershing Drive. Formerly a residential area, the property was acquired by the DOA as a buffer between the airport and residential neighborhoods which continue north of the project.

The DOA has determined that the property, which is not needed for airport operations, should be developed in a way which will be compatible with the adjoining neighborhoods, help restore vitality to the Westchester downtown business community and return the land to a productive use. The DOA conducted a series of public workshops and prepared market studies to determine the land uses which would be appropriate for this unique location. A tentative tract map, Zoning and Environmental Impact Report were approved for the site in 1984. At that time numerous Qualifying ("Q") conditions were applied as part of the approval process. One of the requirements was that a design plan and development guidelines would be prepared. This document will serve as those guidelines.



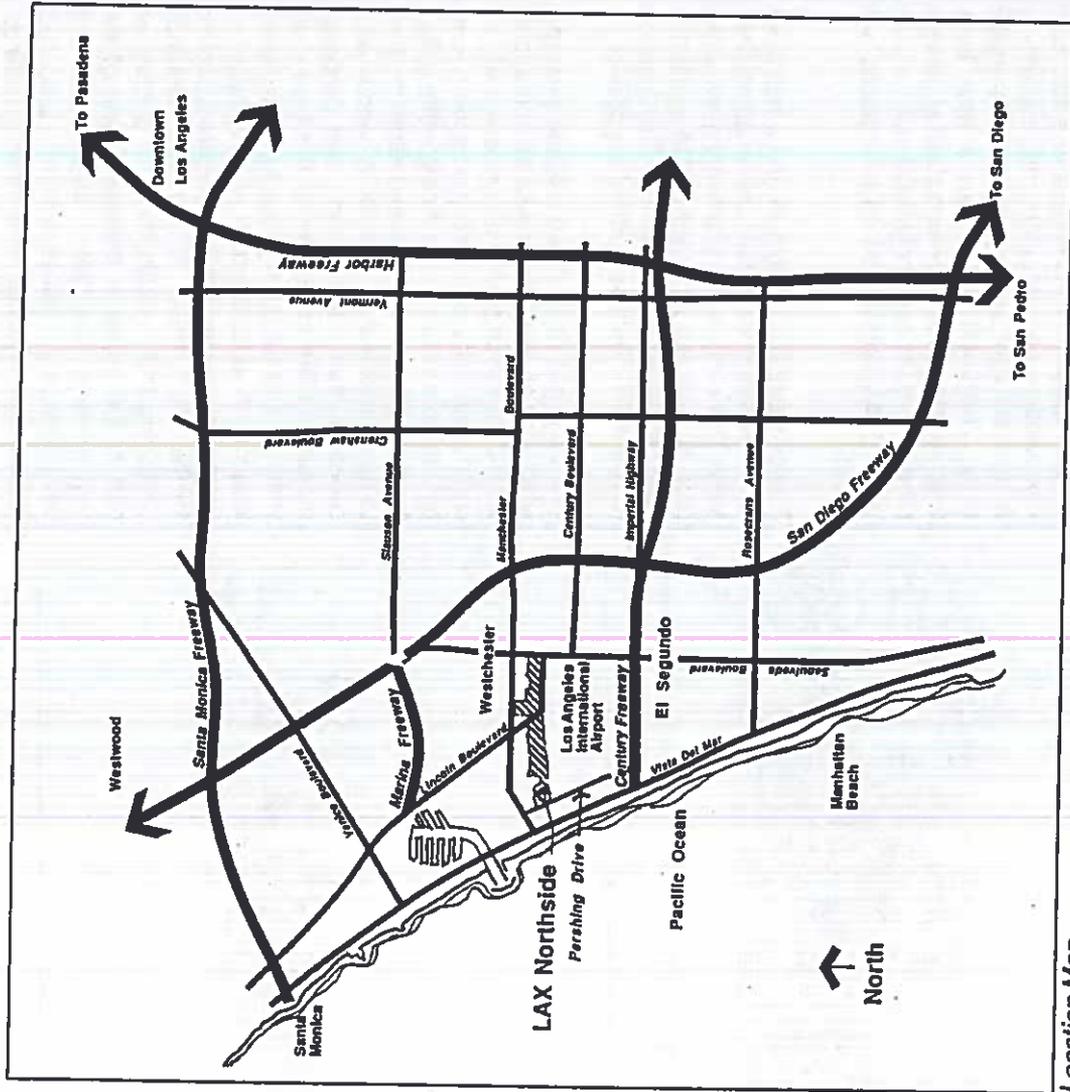
View of LAX Northside from Westchester



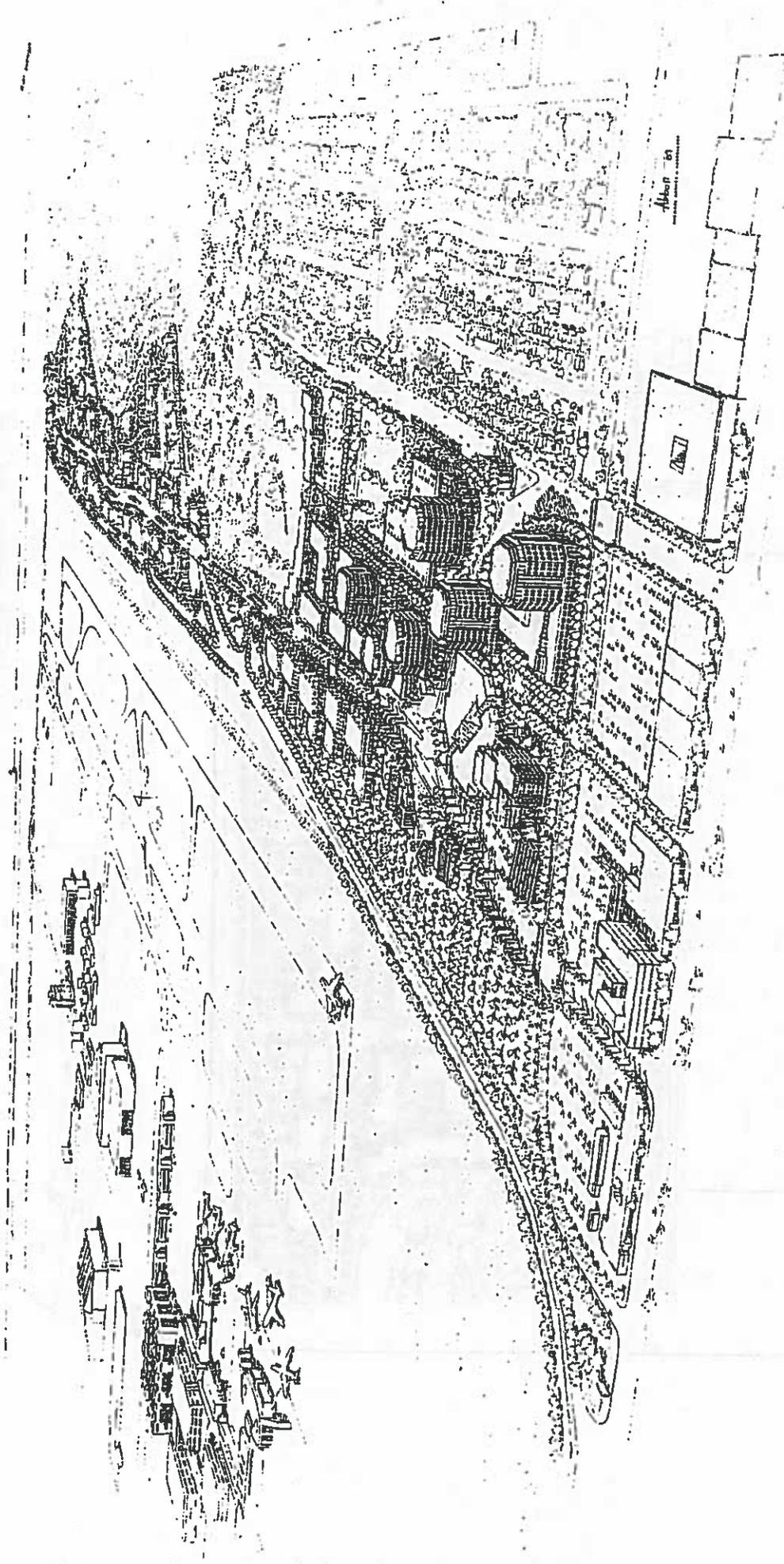
View of LAX Northside at Pershing Drive

The design standards contained in these guidelines will be used by developers in preparing development plans for the site, and will also be used by the Department of Airports in reviewing proposals for the site. All proposals for construction within airport property, which includes LAX Northside, must be approved by the Department of Airports.

In addition to receiving DOA approval, and satisfying normal zoning and building department criteria, all projects on City property, which includes this site, must receive approval of the City of Los Angeles Cultural Affairs Commission. The FAA also reviews all projects in the vicinity of the airport.



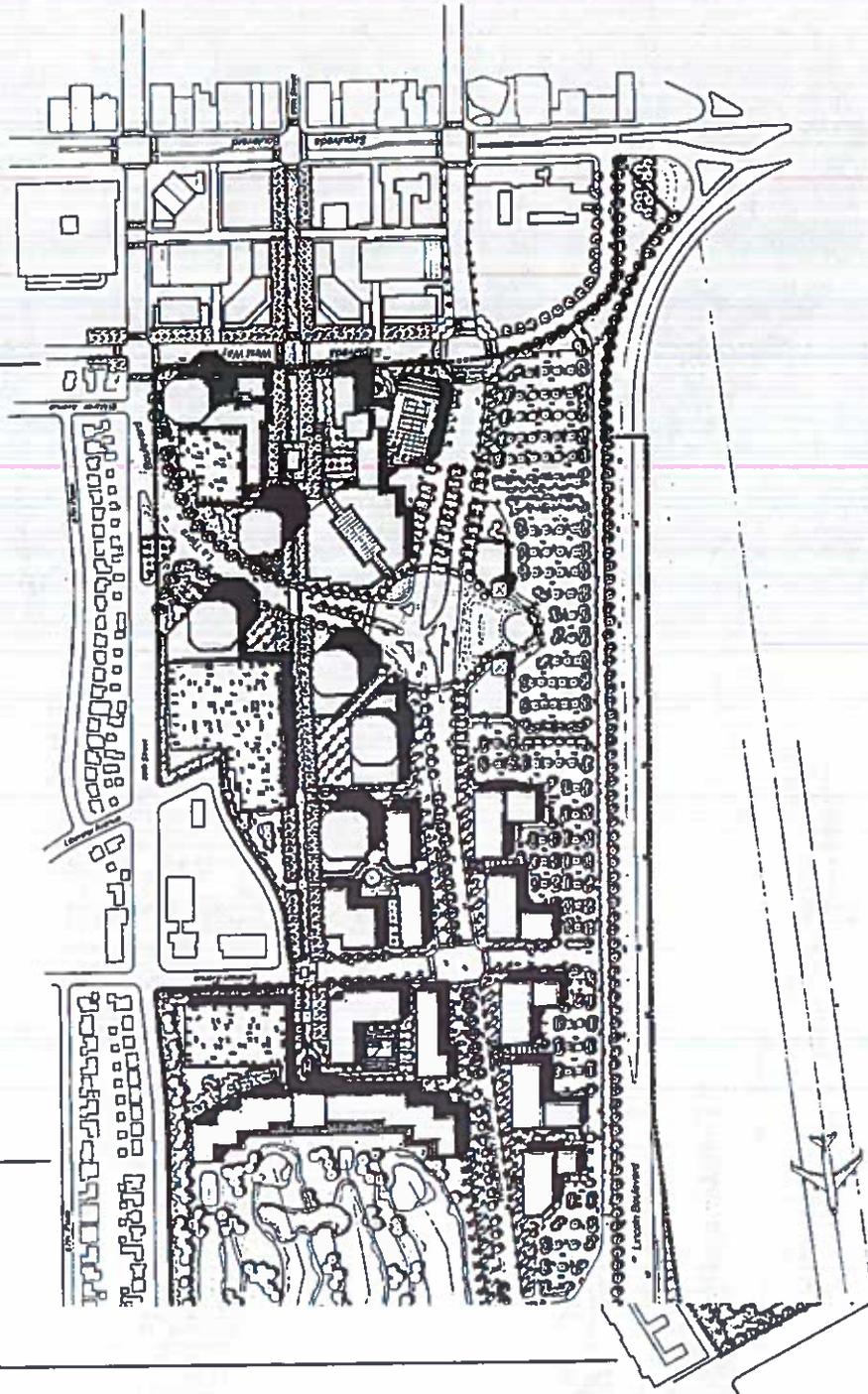
Location Map



Alber 10

CHAMP HALL (WEST) BUILDING

Commercial Center
Office, Hotel, Retail, Restaurant



ILLUSTRATIVE PLAN
LAX NORTHSIDE



ARCHITECTS AND PLANNERS
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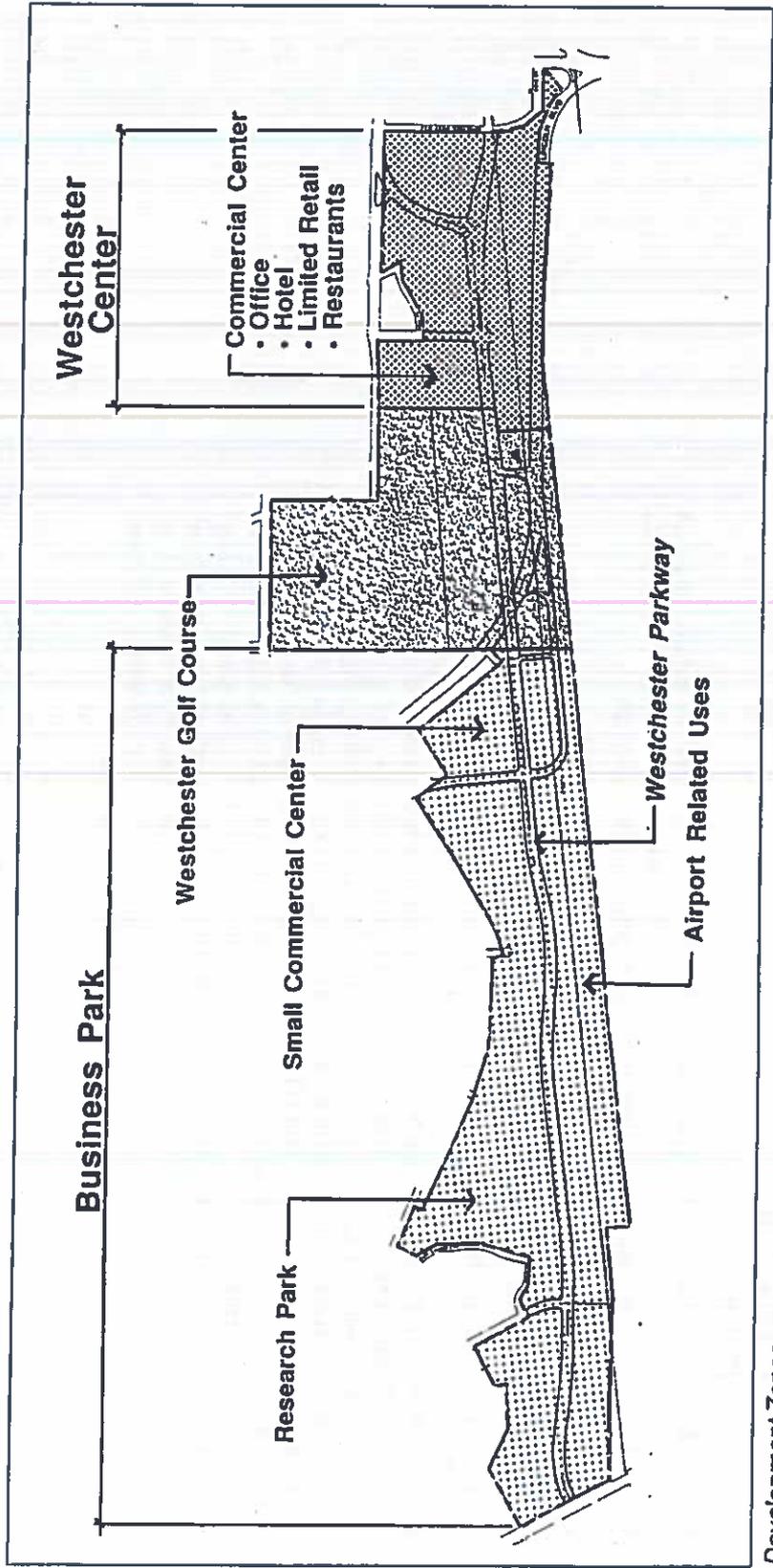
1.1 Purpose of the Guidelines

These guidelines have several basic purposes:

- To provide developers with a unifying theme and design handbook.
- To provide the Department of Airports with a basis for reviewing and coordinating project development plans.
- To help implement the zoning and tract conditions.
- To establish a high level of design quality, and a method for reviewing projects for conformance with that quality.
- To protect private investment, both existing and future, by assuring the compatibility between adjacent developments and avoiding future conflicts.
- To minimize delays in the design process.

Within these goals the guidelines are designed to provide maximum flexibility to respond to changing market requirements.

These guidelines do not replace the requirements of the "Q" conditions imposed as part of the zoning approval. These guidelines contain a summary of some of the conditions related to design and, in instances where there are overlapping concerns, reference is made within these guidelines to the zoning or tract conditions. However, for a full representation of the zoning and tract requirements, the zoning and tract conditions, as well as the City of Los Angeles Zoning Code, should be referred to directly. A copy of the Zoning and Tract Map conditions are contained in an appendix to this report. Wherever the Zoning Code is more restrictive than these guidelines, the Zoning code takes precedence.



Development Zones

1.2 Development Summary

LAX Northside consists of two major development areas:

LAX Northside—Westchester Center in the eastern part of the site, and LAX Northside—Business Park in the western part of the site.

The Westchester Golf Course occupies the central portion of the site, between these two areas.

Westchester Center

Westchester Center will be a mixed use urban center, consisting of office buildings and hotels, with a limited amount of supporting services, retail and restaurants. To reinforce their urban character, the buildings will have close relationships to each other and to the street. Pedestrian connections and the uses along them are important. Open spaces will be relatively small, enclosed or partially enclosed, and shielded from airport noise to the extent practical. Westchester Center will have moderate densities, and may be built to the maximum heights allowed by the FAA.

Business Park

The Business Park will be a business and research park of relatively low density. Westchester Parkway will provide the frontage and identity for most of the buildings within the business park. Buildings along Westchester Parkway will be set back from the right of way to create a band of open space which will act as a linear park. Within the Business Park buildings will be distributed evenly, and most of the uses will be low, two-story research buildings containing a mix of office and light industrial spaces, some of which will be oriented toward airport-related uses.

LAX Northside—Business Park will also contain a small commercial center designed to serve the needs of the business park. This center may include a hotel.

Summary Program

The zoning for the site describes the permitted land uses. The maximum buildable density of LAX Northside is defined not in terms of floor area ratios, but in terms of the traffic which the project will be allowed to generate.

Based on this limit, Westchester Center is projected to include, on development sites totalling 69 acres, development of:

- Low- and mid-rise office space 1,360,000 s.f.
- Restaurant and retail services 100,000 s.f.
- Hotel space 650,000 s.f.
- Total 2,110,000 s.f. = 48.4 Ac

The Business Park is projected to include, on sites totalling 170 acres:

- Low-rise offices 220,000 s.f.
- Research park 1,170,000 s.f.
- Airport support facilities ~~750,000~~ s.f. 2,482,920
- Restaurant and retail services 30,000 s.f.
- Hotel space 220,000 s.f.
- Total ~~2,390,000~~ s.f. 7

At buildout, development at LAX Northside will total approximately 4,500,000 s.f. The final program determination will be based on traffic generation, which will be monitored after the project achieves a floor area of 3,500,000 s.f.

1.3 Organization of the Guidelines

The Design Plan and Development Guidelines are comprised of several sections, each dealing with a separate topic. Following this introduction there is a description of the planning framework (Section 2) which defines the connections between this project and the surrounding community, and describes the general design features which will provide continuity within the project. The next section (Section 3) details the land uses and building density which will be permitted within each of the parcels of development.

This is followed by sections which describe the design guidelines for:

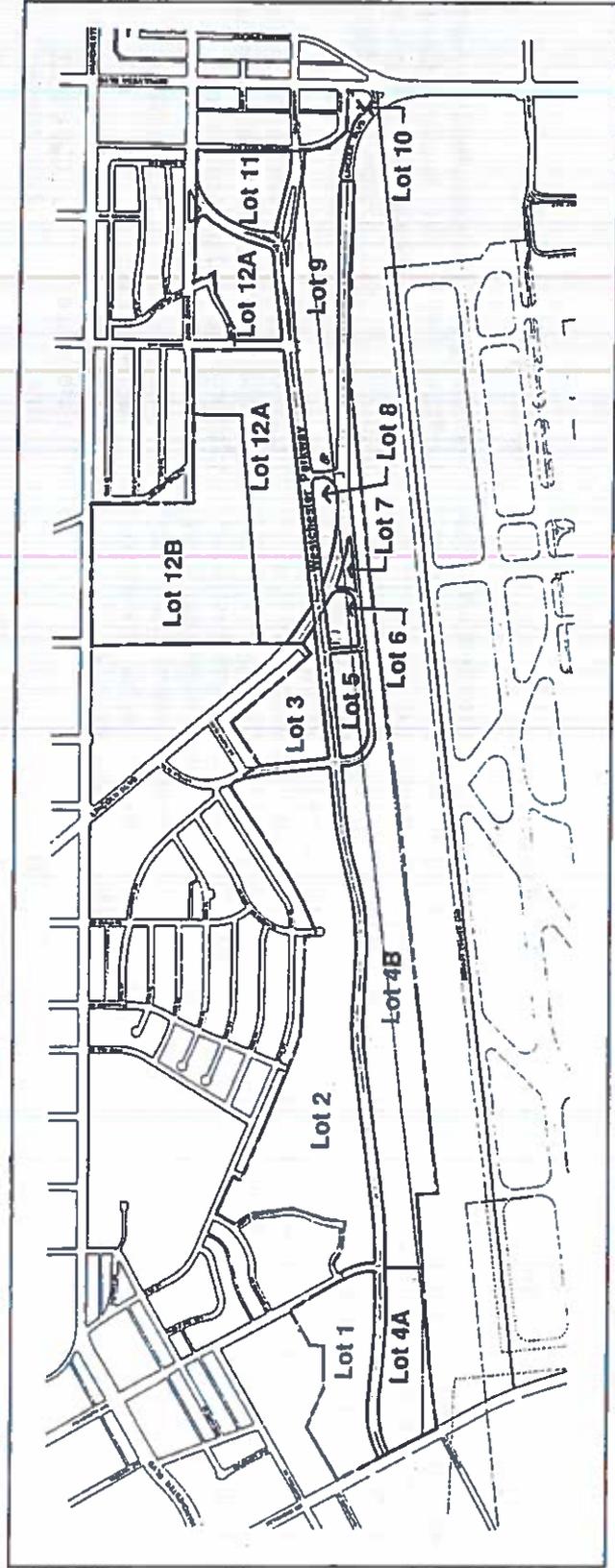
- site access
- architecture
- landscape
- lighting
- signing

Within each guideline section, there is a discussion of the background and issues relating to the topic, followed by a set of directive guidelines.

There are three types of guidelines, based on the area of application:

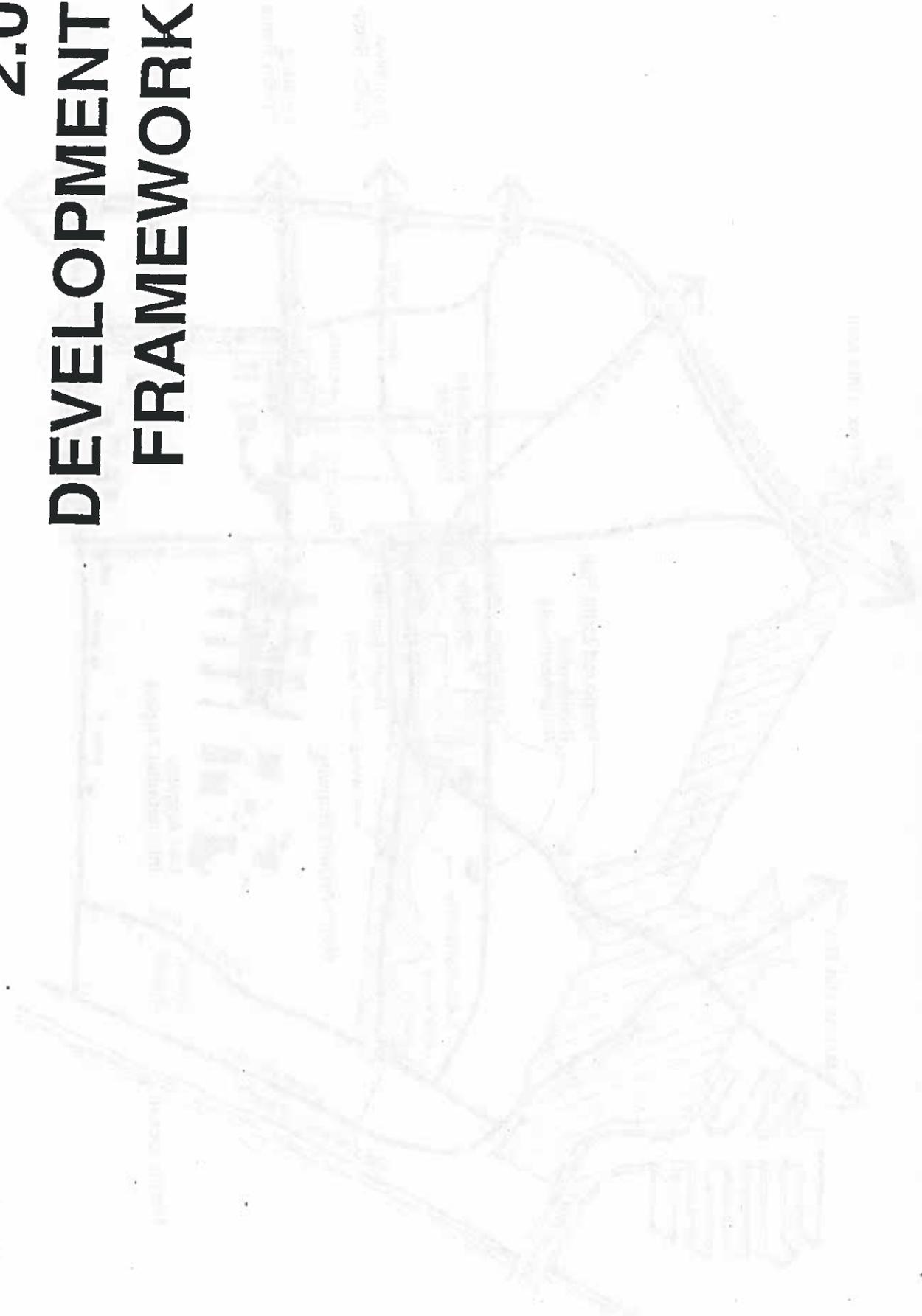
- **General Guidelines** apply to the entire project area.
- **Westchester Center Guidelines** apply to the areas east of Lincoln Boulevard.
- **Business Park Guidelines** apply to the areas west of Lincoln Boulevard.

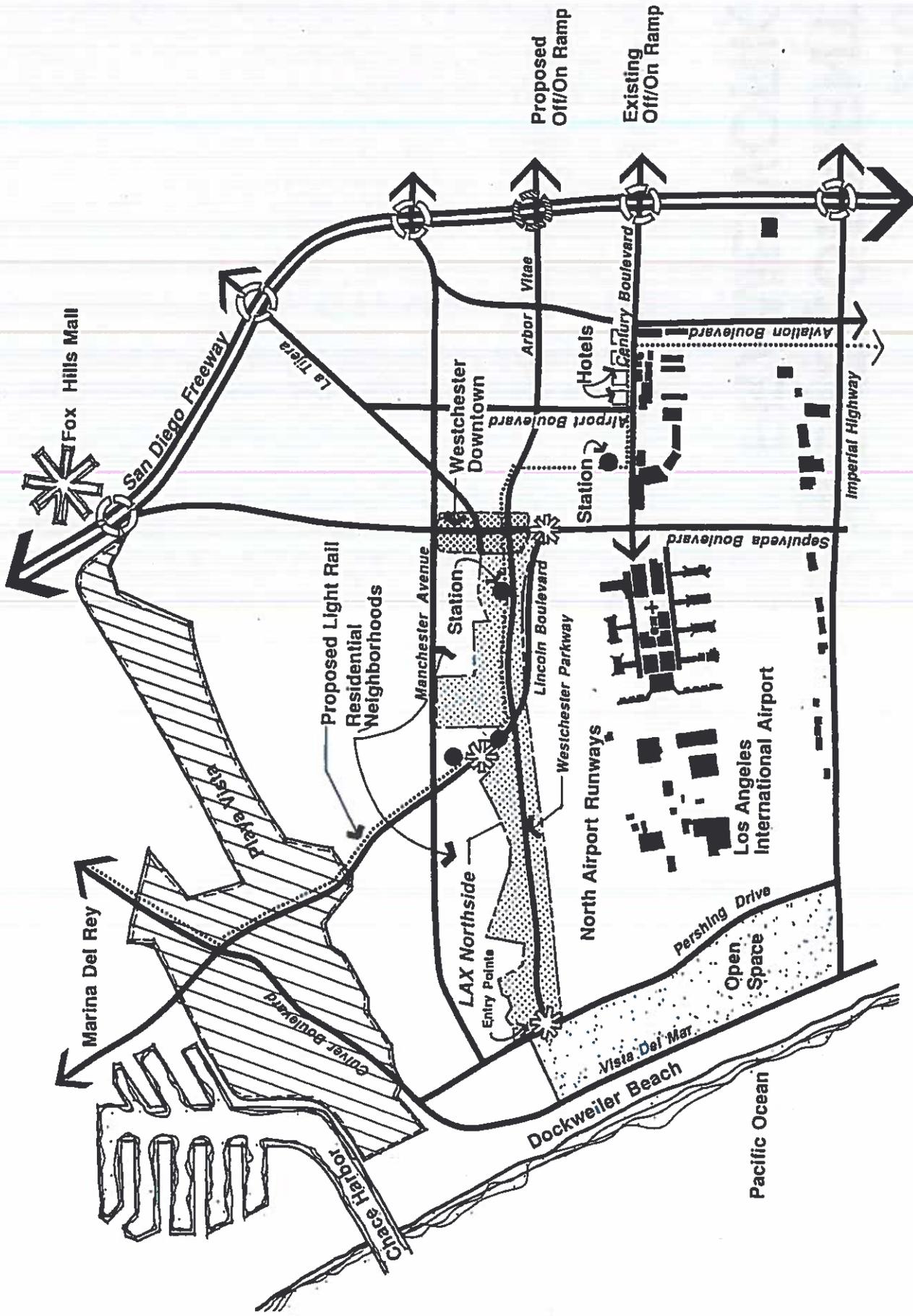
In addition there are, in some cases, guidelines which apply to specific locations or uses within the project. The Tract Map for the project has divided the site into twelve legal parcels, numbered 1 through 12. These numbers will be used in referring to specific lots.



LAX Northside Lot Numbers

2.0 DEVELOPMENT FRAMEWORK





Urban Context

2.1 Urban Context

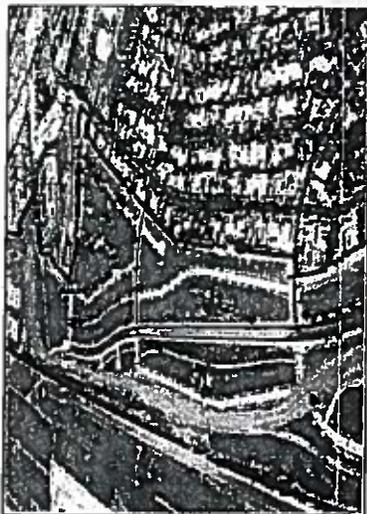
LAX Northside is a linear development, approximately 2 1/3 miles in length, paralleling the north runway of Los Angeles International Airport. Each of the edges of the project will require a different response to assure compatibility of the new development with the existing urban fabric.



Westchester Business District

Westchester Business District

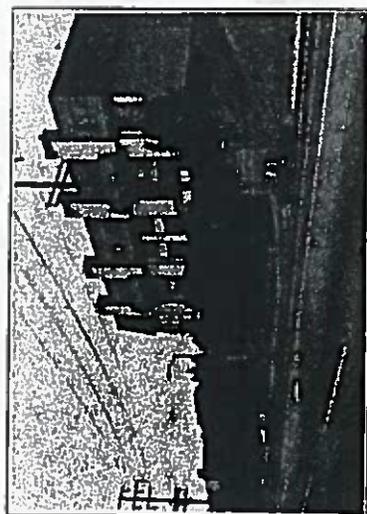
The eastern edge of the project is contiguous with the existing Westchester Business District. This district has served as a small regional center, anchored by two department stores. It has deep shops fronting Sepulveda Boulevard, with large parking areas behind. Increasingly the retail buildings in Westchester have been converted to office uses, with a smaller core of retail consolidated in the blocks northeast of LAX Northside. One of the goals of LAX Northside is to become a catalyst for increased vitality in the Westchester Business District.



Adjoining Residential Neighborhood

Residential Neighborhoods

Along its entire southern edge LAX Northside adjoins the north runway of Los Angeles International Airport. For much of its common boundary, there is an open drainage channel which separates the runway from the site. There have been discussions about enclosing this channel; however at this point there are no plans to do so. The DOA may permit direct access from some parcels to the LAX internal



Pershing Drive

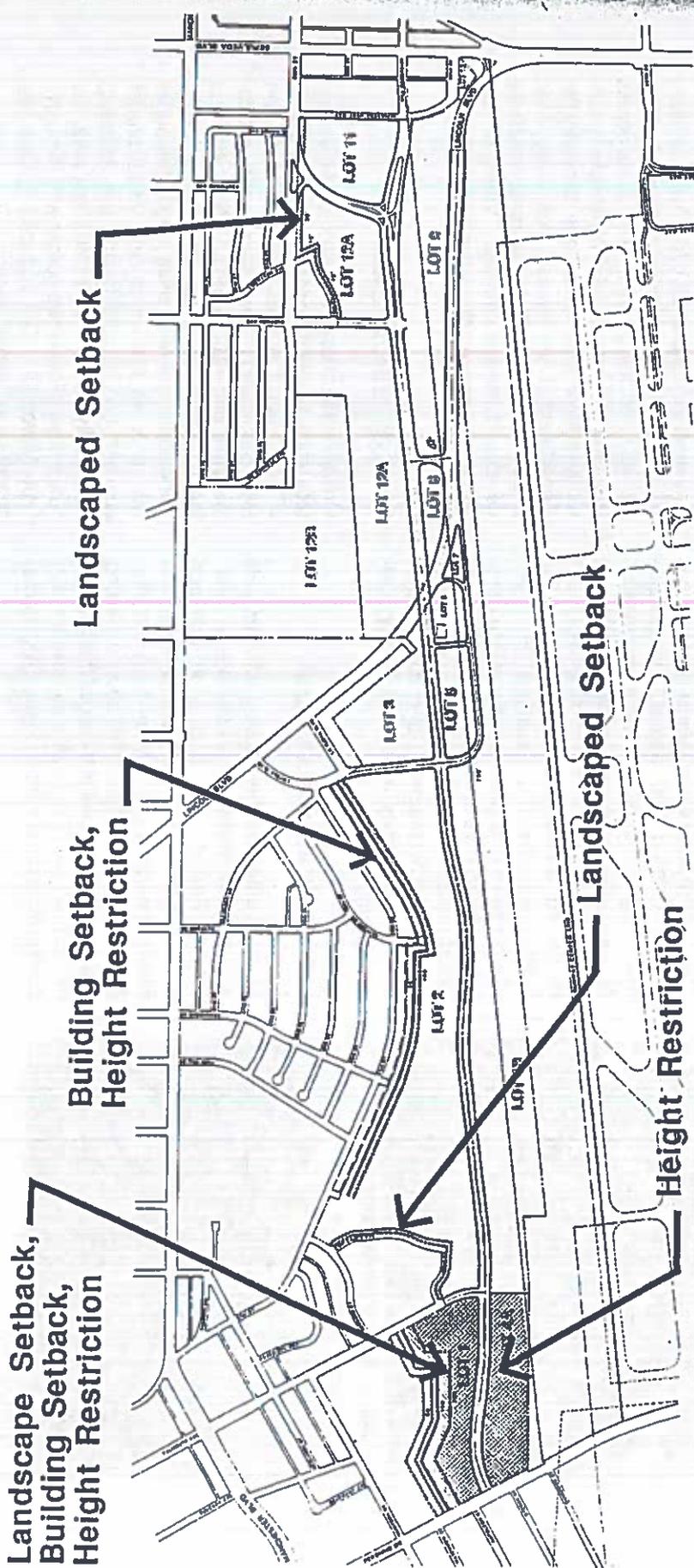
road system, if the channel is bridged, and security requirements can be met.

Building setback and height requirements based on FAA standards will strongly influence development along this edge. (See Section 2.3.)

Pershing Drive

The western edge of LAX Northside is along Pershing Drive, a major highway which offers visibility into the project. The area across Pershing Drive from LAX Northside is currently undeveloped open space owned by the Department of Airports as a clear zone at the end of the runways. The land is proposed to be developed as golf courses. Views of and across this space are a valuable asset of the site.

On the northern edge of the project lie several residential neighborhoods, comprised mostly of single family houses, but also containing two schools and some higher density condominiums. Buffers between LAX Northside and these neighborhoods are required by the "Q" Conditions. For most of its length, the buffer will serve as a physical and visual barrier between LAX Northside and the adjoining community, but in the westernmost area, the buffer must allow views across and through LAX Northside. The required buffers are described in the next section.



LEGEND

-  BUILDING HEIGHT RESTRICTION NOT TO EXCEED 3 STORES OR 45' FROM FINISHED GRADE
-  BUILDING HEIGHT RESTRICTION NOT TO EXCEED 4 STORES OR 55' FROM FINISHED GRADE
-  BUILDING SETBACK-NO STRUCTURES ALLOWED WITHIN SETBACK
-  LANDSCAPED BUFFER SETBACK
-  6' CHAIN LINK FENCE
-  6' MASONRY WALL

"Q" Condition Buffer Requirements

2.2 "Q" Conditions

The zoning adopted for the site in 1984 culminated a long process of community involvement. Public workshops had been conducted and well attended by neighboring residents and business people, in order to assure that the planning for the project reflected their concerns. Principal concerns of workshop participants included effective and attractive buffer systems between the development area and adjoining residential property, and a roadway system that would handle increased traffic load without intrusion into bordering residential neighborhoods. To meet these concerns, the plan calls for a variety of buffer systems, including fencing, shrubbery, trees, ground cover, walls, earth berms and landscaped building setbacks. These buffers were incorporated into the "Q" conditions adopted as part of the zoning.

Other issues addressed in the "Q" conditions are land use, allowable density, height, parking, hours of delivery, noise, access restrictions, security, and Transportation Systems Management. For a complete representation of the conditions, the full document should be referred to.

Many of the guidelines in this report exceed the "Q" conditions, and are additional requirements.

2.3 FAA Restrictions

The Federal Aviation Administration regulates the heights of structures in the vicinity of the airport. Parallel to the centerline of the north runway there is a clear zone of 500 feet, and then a transition slope of 7 to 1 (horizontal to vertical) which defines the maximum height of objects (buildings, landscaping, vehicles, etc.) relative to the elevation of the runway. This slope prevails until a height of 150 feet above the runway is reached. Buildings beyond this transition slope are limited to 150 feet in height, relative to the base elevation of the runway.

Also parallel to the runway is a building setback line 750 feet from the runway centerline. Between 500 feet and 750 feet, parking and landscaping may be permitted, if they do not penetrate the transition slope, but no structures are permitted.

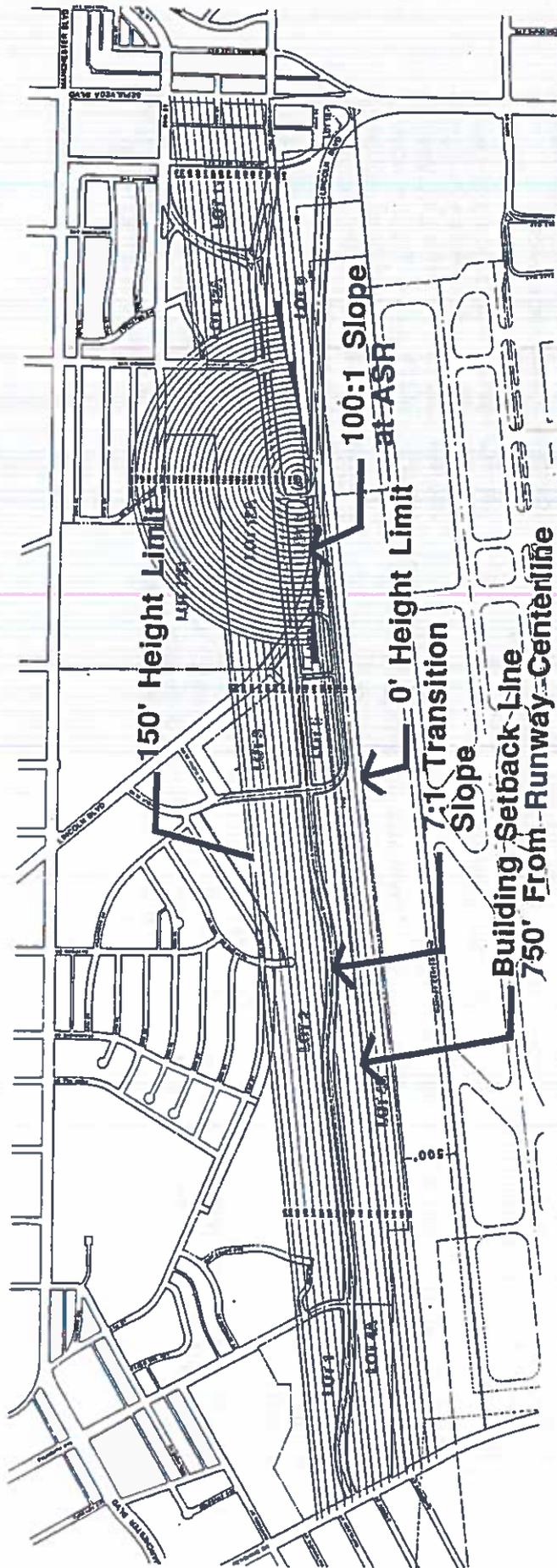
Heights are also controlled by the proximity to Airport Surveillance Radar (ASR) equipment located in Lot 9. From the elevation of this radar, there is an inverted cone which rises at a slope of 100 to 1 (horizontal to vertical) for a radius of 1500 feet from the center of the radar. Relocation of this radar is under consideration, and the FAA has determined, in a January 1988 report, that two alternate sites several hundred feet north and west of the current site would be acceptable. Of the two sites, the

location preferred by the FAA would be within the Westchester Golf Course, on the north side of Westchester Parkway. The preferred site is more distant from the runway and would allow the ASR to be located on a higher tower than currently. Because of both the increased height and the location, the associated height limitations would be less restrictive on potential development. If desirous of relocating the ASR, the developer of *Westchester Center* will be responsible for pursuing the issue with the FAA.

The FAA should be consulted before finalizing plans for Westchester Center to determine the feasibility of relocating the ASR.

The FAA has also expressed a desire to relocate ASDI radar which monitors ground traffic, and has selected a site within *LAX Northside* for its potential location. The site would be north of Westchester Parkway, west of Lincoln Boulevard. This radar could be located on the roof of the tallest building in the vicinity, and may have little impact on the heights of surrounding structures.

The FAA should be consulted before finalizing plans for the research park to determine the feasibility of locating ASDI equipment on the roof of a building within the project area.



LEGEND

-  7:1 Height Restriction From Center Line North Runway
-  FAA Radar Requirements
-  Setback Line

FAA Height Restrictions

2.4 Regional Access

Westchester Parkway

Westchester Parkway will be built as an arterial highway and will provide access to every development site within *LAX Northside*. Westchester Parkway will run the length of the site as the primary circulation, orientation, and open space feature for all the development within the project.

East of the project, Westchester Parkway extends through downtown Westchester, crosses LAX Parking Lot C, and continues as Arbor Vitae Street to the San Diego Freeway (Interstate 405). There are proposals to provide direct access to and from Arbor Vitae and the San Diego Freeway. Construction of this interchange depends on the provision of funding by CalTrans, the City of Los Angeles, and the City of Inglewood.

Within *LAX Northside*, Westchester Parkway will change from an urban streetscape to a suburban parkway as it passes from the Westchester Business District on the east toward the research park on the west. However, the landscaping, both within and along Westchester Parkway, will establish a common theme. These guidelines describe the character of the street, and help assure that each project, whether it be a single building or a group of buildings, will contribute to the design of *LAX Northside*.

Light Rail

The Coastal Corridor Rail Transit Project is proposed to serve *LAX Northside*. The rail line is planned to extend from El Segundo on the south to Marina Del Rey on the north, connecting with the Norwalk-El Segundo /Rail Transit Project being constructed within the right-of-way of the Century Freeway (I-105). Future rail projects would extend this line into a network serving much of Los Angeles County, as mandated by Proposition A adopted by the voters in 1980. The Los Angeles County Transportation Commission is the lead agency responsible for planning the rail line.

Within *LAX Northside* the rail line is proposed to be built as an elevated structure along Westchester Parkway between Sepulveda Westway and Lincoln Boulevard. North of *LAX Northside*, the route would continue along the eastern edge of Lincoln Boulevard, going underground before Manchester Avenue, in order to maintain a gentle slope before surfacing at Playa Vista.

Two options for the line within *LAX Northside* have been identified by the LACTC: either within the median or along the south side of Westchester Parkway. The width of the right of way of Westchester Parkway has been designed to accommodate an elevated structure; however some redesign and modification of the street may be required as definitive

engineering plans for the light rail system are developed. If the south alignment is selected, additional right-of-way will be required.

Both options would provide a station along Westchester Parkway, between Sepulveda Westway and La Tijera. This station would serve the development within the Westchester Center, as well as existing and potential development within all of downtown Westchester. If the median alignment is adopted, the Westchester Station would be in the median of Westchester Parkway, between Sepulveda Westway and La Tijera Boulevard. If the south alignment is adopted, the Westchester Station would be opposite Sepulveda Westway. The LACTC desires a park-and-ride facility for 500 cars adjacent to this station. This park and ride facility should be located in Lot 9, which has very limited development potential due to the closeness to the runway and FAA restrictions. North of *LAX Northside* an aerial station on Lincoln Boulevard is proposed 1500 feet south of Manchester. This station would provide service to the eastern end of the Business Park.

The Coastal Corridor, although a high priority route, is in competition with other light rail segments for funding, and fiscal constraints may require that the construction proceed in phases. If so, the terminus of the first phase

Bus Service

would be either the LAX/Lot C Station or the Westchester Station. (Source: Draft Environmental Impact Report, January 1989, Coastal Corridor Rail Transit Project-Northern Segment).

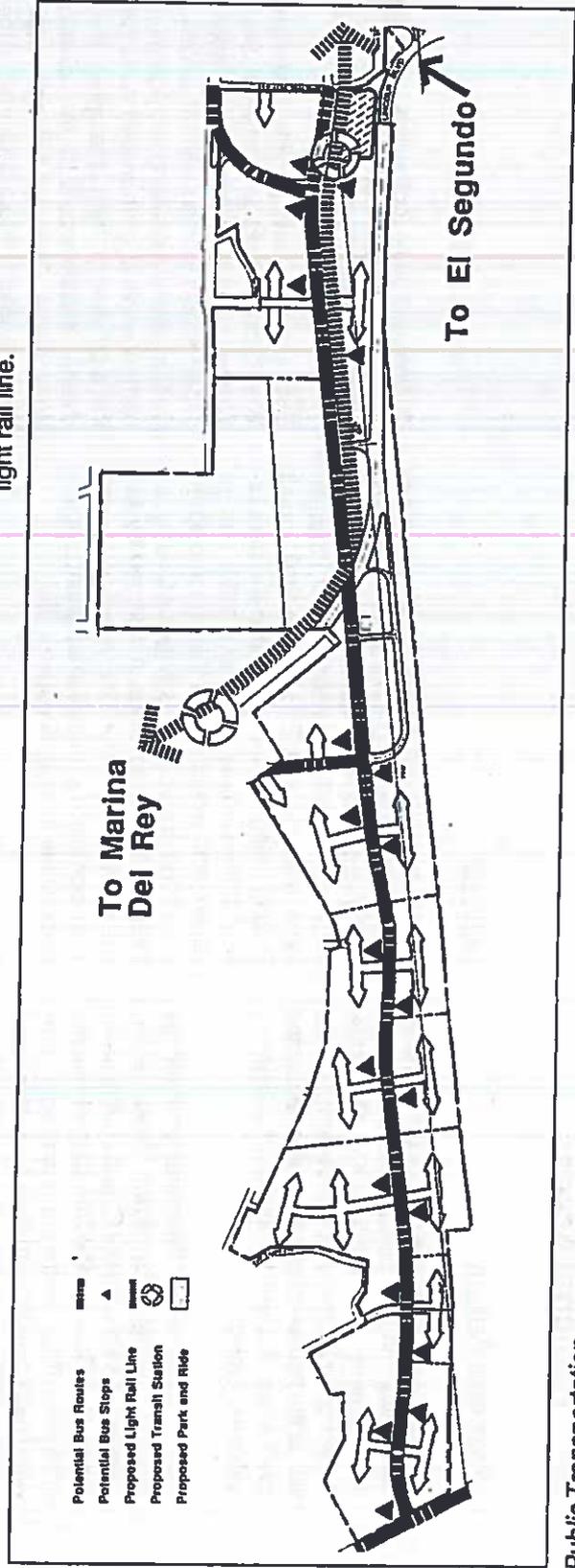
A recent proposal, by the Councilwoman's office, suggests evaluation of an alternative route which would not run along Westchester Parkway and Lincoln Boulevard, but would instead run north along Sepulveda Boulevard. No plans for this alternative have been developed, although it is under consideration for further study.

The LACTC should be consulted for the current status of all plans regarding the light rail project before development plans at LAX Northside are finalized.

Coastal Transportation Corridor Specific Plan

LAX Northside is part of the Coastal Transportation Corridor Specific Plan area adopted by the City of Los Angeles in 1985. The purpose of the plan is to address the present and future needs for transportation within the corridor, which is experiencing serious traffic and transportation problems. Within the project area, Transportation Impact Assessment Fees will be required, based on the number of vehicle trips which a project is projected to generate. Trips are projected based on floor area, and a trip factor contained in the ordinance. The funds generated by this fee will be used to finance identified mitigation measures, which include transportation management, street improvements, such as the Arbor Vitae interchange, and public transportation, such as the light rail line.

There is currently no bus service to the property, except along Sepulveda Boulevard east of the project, and along Pershing Drive at the western edge of the site. It is probable, however, that after Westchester Parkway has been completed, and development occurs, that RTD will provide bus service along that street. The developer should consult with the RTD, Los Angeles County Transportation Commission, and any other agencies which may operate transit lines within the project, and make provisions for bus stops and other required facilities. If bus service is provided, bus stops along Westchester Parkway should be located near major site entries and should connect with site pedestrian circulation systems.



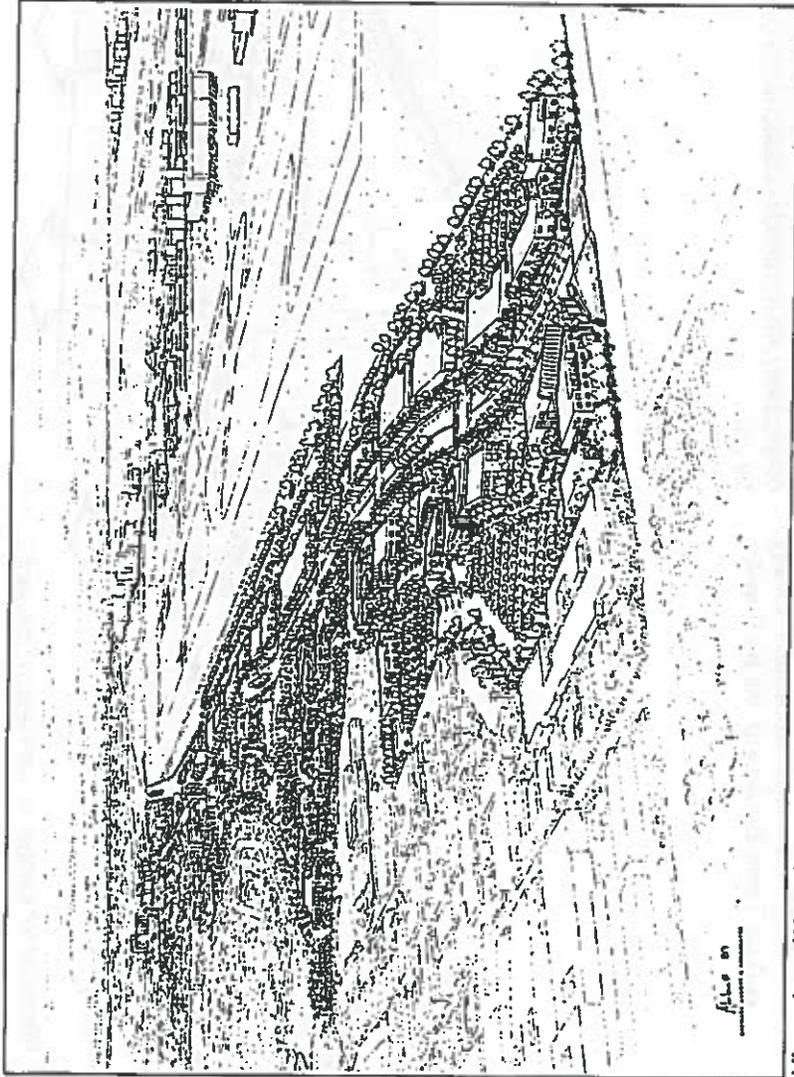
2.5 Urban Design Concept

An urban design concept will mark *LAX Northside* as a special district within the Los Angeles region. The concept takes advantage of the relationships between Los Angeles International Airport, the Westchester Business District and *LAX Northside*, while creating an edge to the existing Westchester residential neighborhoods.

The primary design elements which will unify the project include:

- clearly marked project gateways;
- a consistent street treatment along Westchester Parkway;
- a sequence of spatial experiences along the length of the development;
- large-scale landscape treatment at major intersections;
- a regular pattern of site entries, which incorporate repeating landscape and graphic elements.

Within the overall concept for *LAX Northside*, the two development zones will each have a different character, reflecting the differences of location and land uses.



View along Westchester Parkway within the Business Park

LAX Northside—Westchester Center Urban Design Concept

Westchester Center offers a special opportunity in urban design for several reasons:

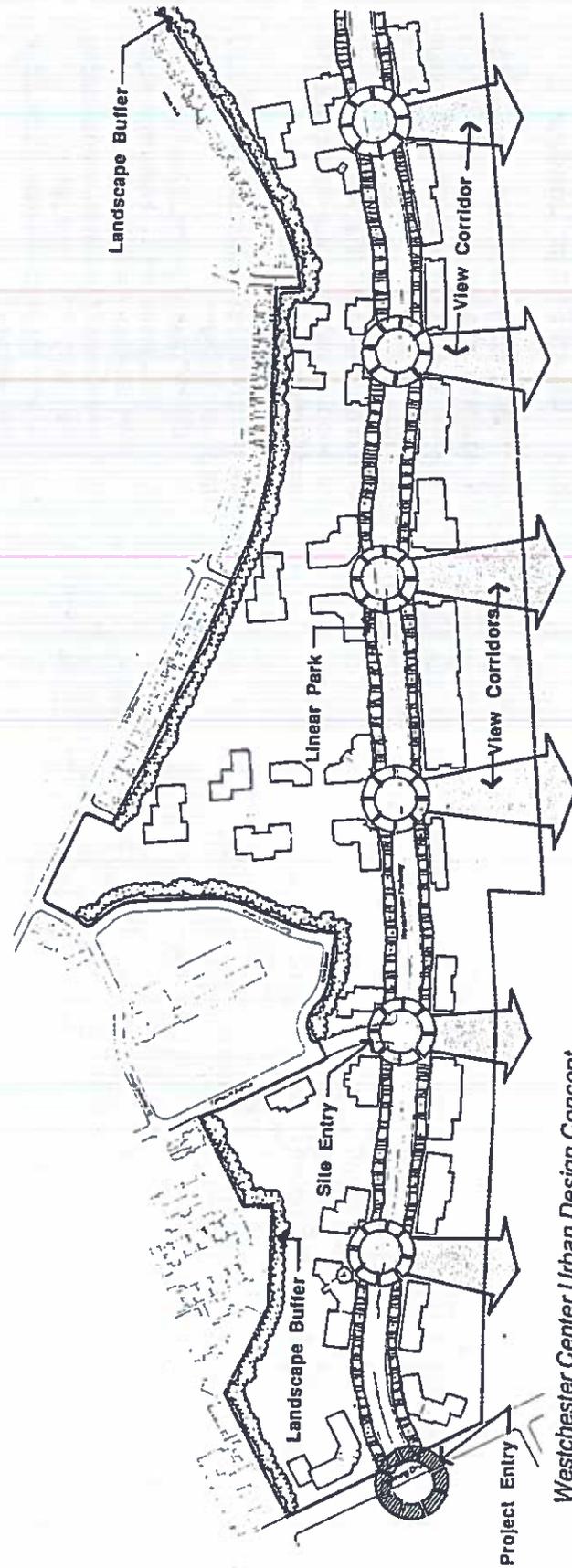
- It will, along with the Westchester Downtown, be the major urban activity center in the area.
- It will be the location of the transit station which is designed to serve not only this project, but the Westchester Business District as well.
- The intersection of Westchester Parkway and La Tijera Boulevard will be a major

traffic carrier with the potential of becoming a focal point of LAX Northside.

- The site has high visibility from and views of the airport runways and terminal.
- The western end of the center fronts onto the redeveloped Westchester Golf Course.
- The proximity to the north runway requires mitigation of noise in public areas.

Some of the objectives of the development concept for LAX Northside—Westchester Center are:

- To develop LAX Northside—Westchester Center as a single masterplanned project, which integrates multiple uses into a distinctive, enjoyable place.
- To extend the urban pattern of Westchester.
- To present a friendly face toward the existing Westchester Business District.
- To use the golf course in establishing views and vistas.



Westchester Center Urban Design Concept

The primary features of the urban design concept for *Westchester Center* are a tightly-scaled pedestrian environment, and an axis which provides visual and pedestrian connections between the various buildings in the center and ties this development with the existing Westchester downtown.

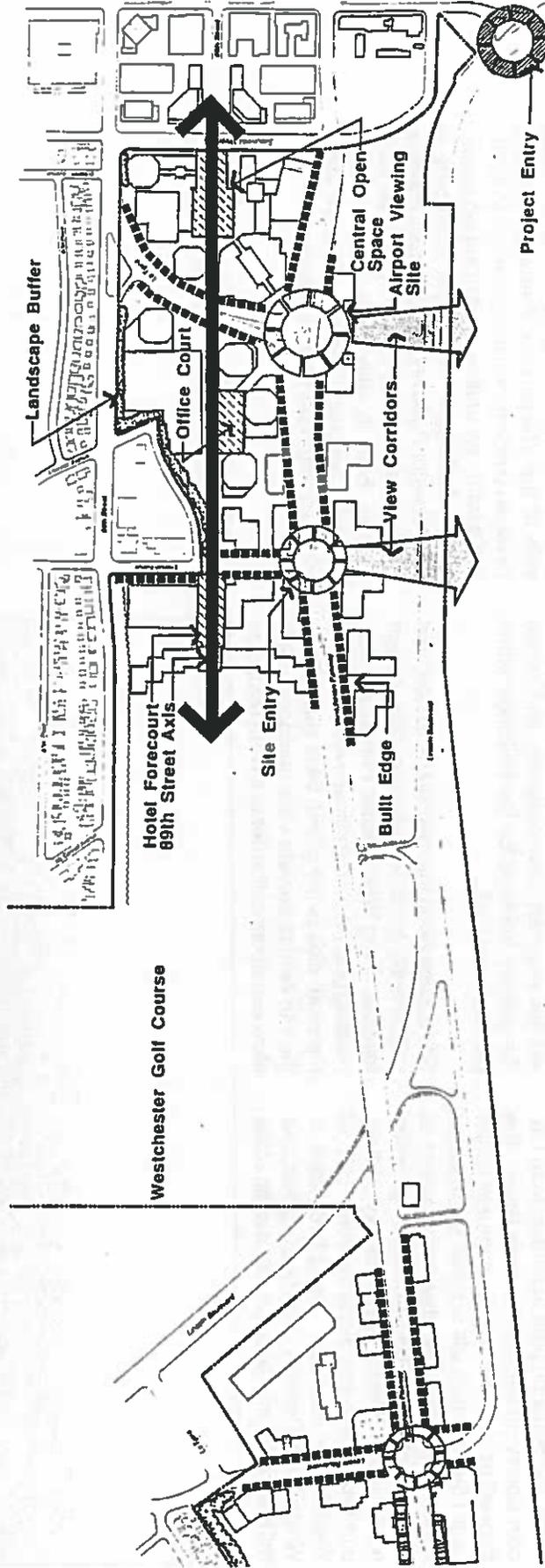
The axis will be formed as an extension of 89th Street. This axis will terminate in the golf course on the west, and be open-ended on the east, providing an opportunity for future con-

nections with the Westchester Business District. This axis will act as an organizing element and may function in places as a driveway, in other places as a strictly pedestrian walkway. It will visually link both sides of La Tijera, although pedestrian crossing will need to take place above grade or at the intersections.

Buildings will define a series of small, landscaped open spaces. These spaces should be along the edge of pedestrian activity and

should provide seating areas and access to the sun with shading provided by trees or umbrellas. These spaces should also be sheltered, when possible, from direct runway noise.

There will be an airport viewing site incorporated into the development south of Westchester Parkway, at the intersection of La Tijera and Westchester Parkway. This site will be designed in conjunction with restaurants or offices on either side which can overlook the site and monitor activity there.



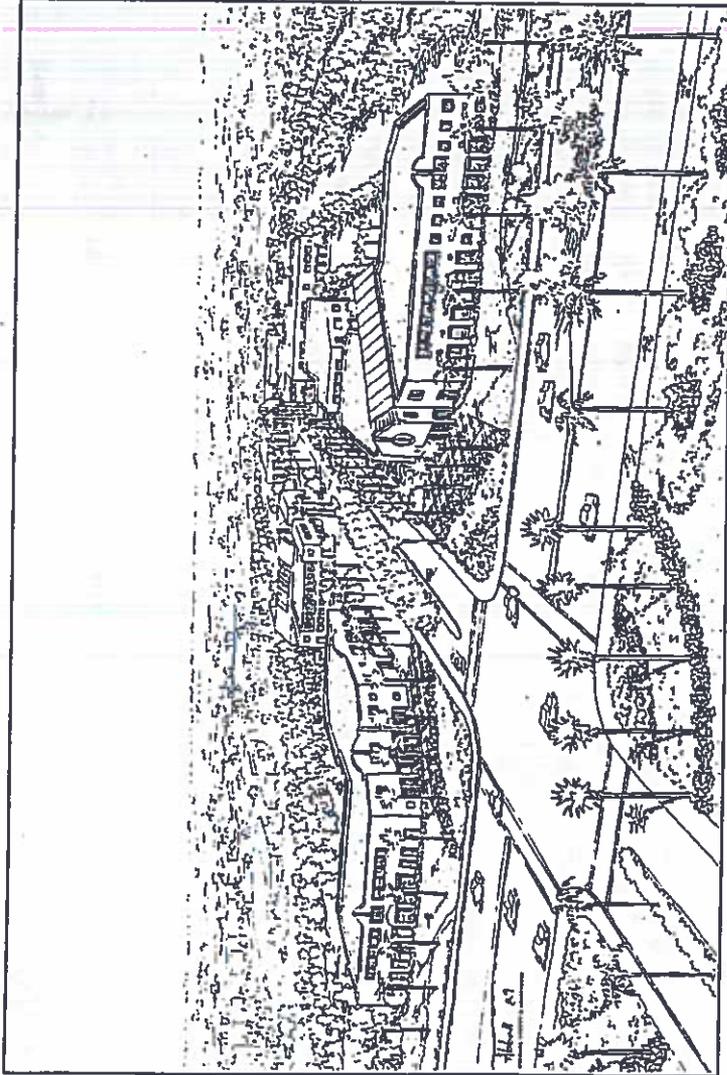
LAX Northside—Business Park Urban Design Concept

The *Business Park* will be a low-rise, suburban office and research park extending from Lincoln Boulevard west to Pershing Drive. The park will be anchored by a commercial center near Lincoln which will achieve a somewhat urban quality and will elsewhere consist of low-scale office and research buildings on the north side of Westchester Parkway and airport-related services along the south side of Westchester Parkway. Along both sides of Westchester Parkway deep building setbacks will be used to increase the visual width of the

roadway and will establish a linear "park" which will link separate developments, and create the primary frontage for the buildings within the *Business Park*.

Site access along the road will be limited, and project entry points will become major design features along Westchester Parkway, incorporating graphic and landscape elements. On the south side of the street, wide setbacks at the site entries provide view corridors which allow visual connection between LAX Northside

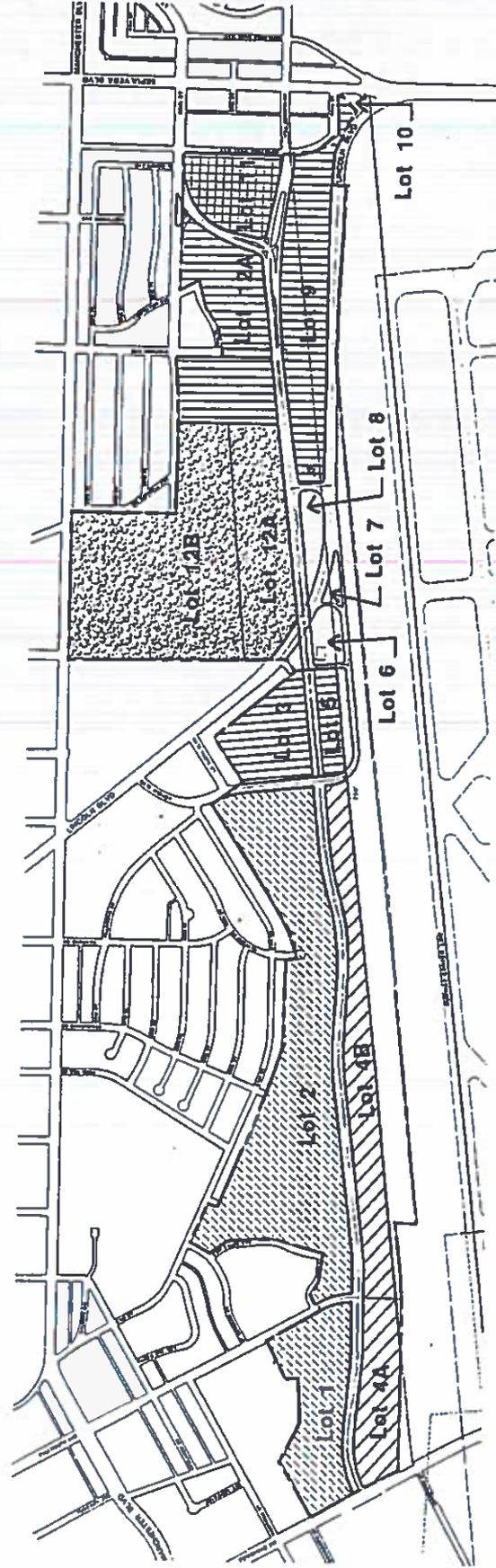
and the airport. Elsewhere along the south side of the Westchester Parkway, setbacks between buildings will be minimized to establish a relatively uniform wall of airport-related service buildings. Along the north side of Westchester Parkway, buildings will maintain a relationship to the street, but will be more diverse, both in siting and design. Wider setbacks between buildings will allow views into the deeper portions of the site.



Entry Point into Business Park

3.0 DEVELOPMENT PROGRAM





Recommended Land Uses



Mixed Use: Hotel / Office / Retail

Commercial: Hotel / Office / Retail

Research Park

Airport Related Uses

Golf Course

Proposed Land Uses

3.1 Land Use

Background

The zoning for the site allows a range of commercial and light industrial uses on most of the parcels.

Market studies identified strong potential for hotel, office and light industrial uses, especially those directly related to the airport. The retail market is weak, except that which would be generated by on-site development. The airport has committed itself not to directly compete with existing Westchester retail.

Objectives

- To create a mix of land uses which responds to the needs of the market, while being compatible with adjacent neighborhoods.
- To develop the site to the maximum potential allowed by the zoning.
- To concentrate hotel and office uses in an area where they have access to public transportation, can utilize common services, and help provide a market base for the existing Westchester retail district.

General Guidelines

1. Land uses should be distributed so that hotel and office uses are concentrated in the east part of the site (*Westchester Center*), with industrial and research park uses to the west (*Business Park*).
2. Retail uses will consist of service and food establishments which are designed to serve the needs of the business and hotel interests. Retail uses should complement rather than compete with retail uses within the existing Westchester Business District. Large department stores or other retail uses drawing on a regional market will not be permitted.
3. Retail and restaurant uses should be clustered in two small centers: In Lot 3 (at the corner of Loyola Boulevard and Westchester Parkway), and in Lots 9, 11 and 12 (at the intersection of La Tijera and Westchester Parkway). These two locations will each provide an identifying focal point for the two zones of the project.

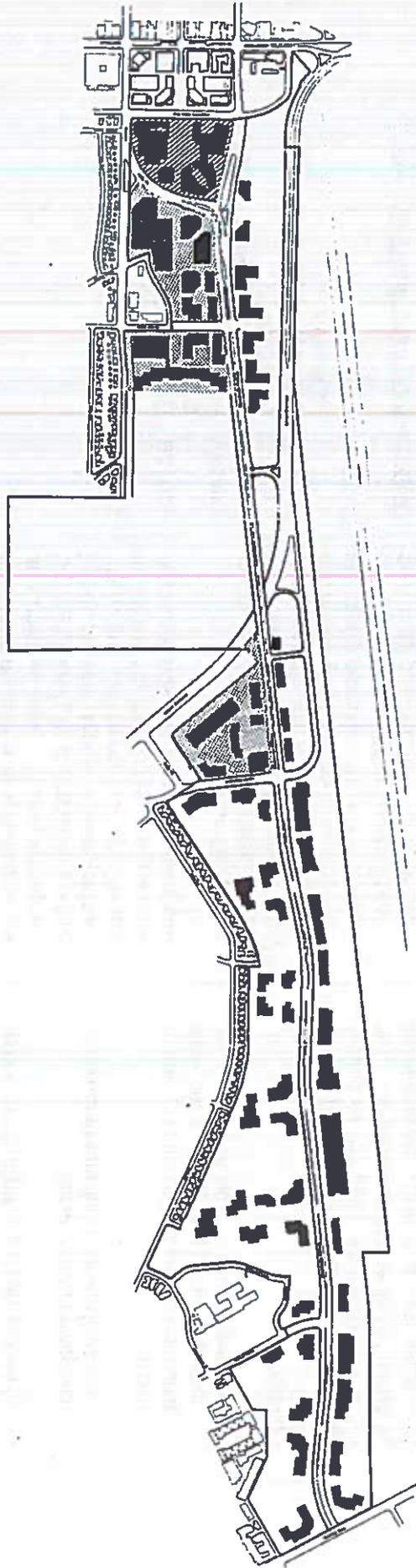
Research Park Land Uses

Lot 1	Research Park	21.3 Ac*
Lot 2	Research Park	69.5 Ac
Lot 3	Office, Hotel	
Lot 4	Retail/Restaurant	12.5 Ac
Lot 5	Airport-Related Uses	57.0 Ac
	Office	6.0 Ac

Westchester Center Land Uses

Lot 9	Office	
	Restaurant	
Lot 11	Airport View Site	3.0 Ac*
	Mixed Use:	
	Office, Hotel, Retail	11.4 Ac
Lot 12A	Hotel Restaurant	
	Office	24.0 Ac
Lot 12B	Golf Course	78.0 Ac

* All acreages approximate 116.4 Ac



-  Moderate F.A.R. 1.5
-  Low/Moderate F.A.R. .5 -1.5
-  Low F.A.R. Less than .5

Building Density

3.2 Density

Background

The zoning conditions for the site set a project-wide limit on density based on traffic generation. Project generated traffic will be limited to 7,000 project-related outbound daily trips in the p.m. peak hour, and 6340 project-related inbound daily trips in the a.m. peak hour. Based on traffic generation estimates used in the EIR, this limits development to around 4,500,000 s.f., or an average floor area ratio of 0.43 for the development parcels. Effective Transportation Systems Management measures will be required in order to limit traffic generation to the estimated volumes.

Actual trip generation will be monitored after 3,500,000 s.f. of development has occurred. Measured traffic generation will be used as the determination of how much development beyond that level will be permitted. If effective measures are implemented to limit peak hour traffic generation, then more than 4,500,000 may be permitted. If traffic reduction measures are ineffective, then it is possible that development will be limited to some point between 3,500,000 and 4,500,000 s.f.

The proposed light rail station adjacent to Lot 11 offers an opportunity for a greater density of uses within walking distance of the station.

LAX Northside is located in Height District 1, and no single lot may exceed a floor area ratio of 1.5:1, according to the City of Los Angeles Zoning Code.

Other factors which affect density are the zoning conditions and FAA setbacks and height limits. See Sections 2.2, 2.3, and 5.2.

Objectives

- To create a critical mass which will give the Westchester Center an identity as a center, and provides the volume of activity to support service facilities such as retail and restaurants.
- To locate the highest density uses adjacent to existing and planned public transportation facilities
- To maintain lower densities in the portions of the project adjacent to residential neighborhoods.

General Guidelines

1. Within the overall density cap placed on the site by the "Q" conditions, Westchester Centers should be built at relatively urban densities, while the Business Park should be at a much lower density.
2. The following program shown in Table 1 is a recommendation of uses by floor area and by lot. Some variation is possible based on market conditions and the needs of individual users. Any deviation from the program must not increase the total projected traffic generation. The traffic generation factors used in the EIR are shown in Table 2.

Westchester Center Guidelines

1. Lot 11 should contain a mix of office, hotel and retail uses. No single use should occupy more than 60% of the total floor area of the block. There should be a maximum of 50,000 s.f. of retail uses.
2. Retail uses should be concentrated in an enclosed court which offers some protection from airport noise, but maintains street visibility and has adjacency to the hotel, offices and transportation.

TRIP GENERATION FACTORS

LAND USE CATEGORY	TRAFFIC VOLUME PER 1,000 SQ. FT. OF FLOOR AREA					
	AM Peak Hour		PM Peak Hour		Daily 2-Way Volume	
	IN	OUT	IN	OUT		
1. Low-Rise Office	1.90	0.10	0.20	1.90	12.00	
2. Airport-Related	1.30	0.30	0.70	1.60	10.30	
3. Research Park	1.20	0.10	0.30	1.20	8.20	
4. Hotel	0.80	0.80	0.70	0.70	20.00	
5. Sit-Down Restaurant	0.80	0.50	7.30	4.70	105.00	
6. Specialty Retail	0.30	0.30	2.50	2.50	50.00	
7. Moderate-Rise Office	2.20	0.10	0.30	2.30	14.00	

Source: Final EIR, LAX North Side Development Project

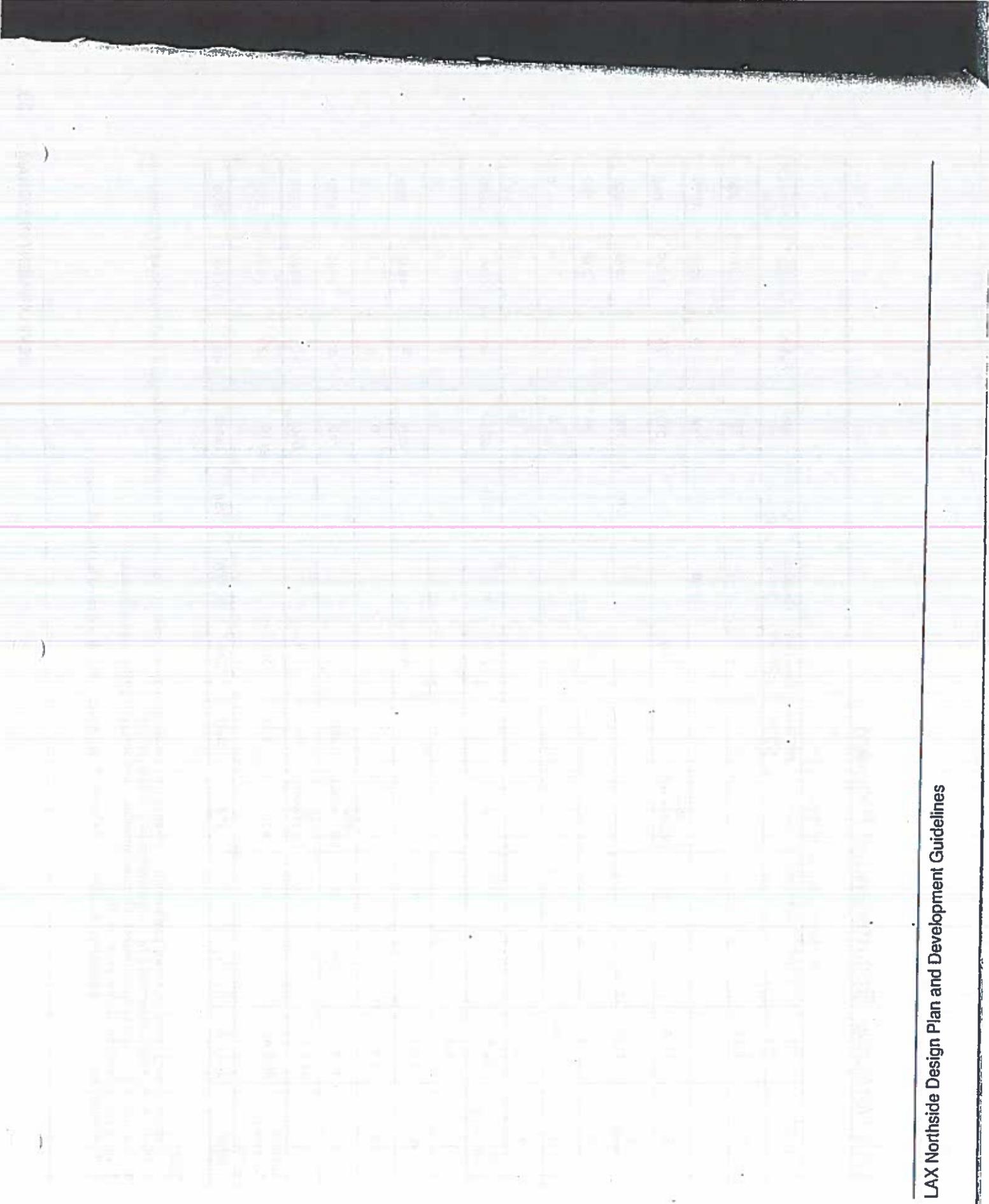
LAX Northside Recommended Program

Program Uses In Thousands of Square Feet

Lot No.	Site Area	Retail	Restaurant	Hotel	Mid-rise Office	Low-Rise Office	Research Park	Airport Support	Total	F.A.R.	Parking 1	PM Peak 2 Hour
1	21.3 Ac						270		270	.3	810	320
2	69.5 Ac						900		900	.3	2700	1080
3	12.5 Ac	10	20	220 (400 Rooms)		140			390	.75	1290	540
4	57 Ac ³							750	750	.3	1875	1200
5	6.0 Ac					80			80	.3	270	150
6	3.1 Ac								0	0	0	0
7	1.0 Ac								0	0	0	0
Subtotal Business Park	170.4 Ac	10	20	220		220	1170	750	2390	.32	6945	3290
8	3.0 Ac								0	0	0	0
9	28.2 Ac		20			400			420	.3	1640 ⁴	855
10	0.7 Ac								0	0	0	0
11	11.4 Ac	50	20	390 (600 Rooms)	280				740	1.5	2145	1130
12	24.0 Ac ⁵		10	260 (400 Rooms)	580	100			950	.84	2870	1785
Subtotal Westchester Center	69.3 Ac	50	50	650	860	500			2110	.7	6655	3825
Total	239.7 Ac	60	70	870	860	720	1170	750	4500	.43	13600	7030

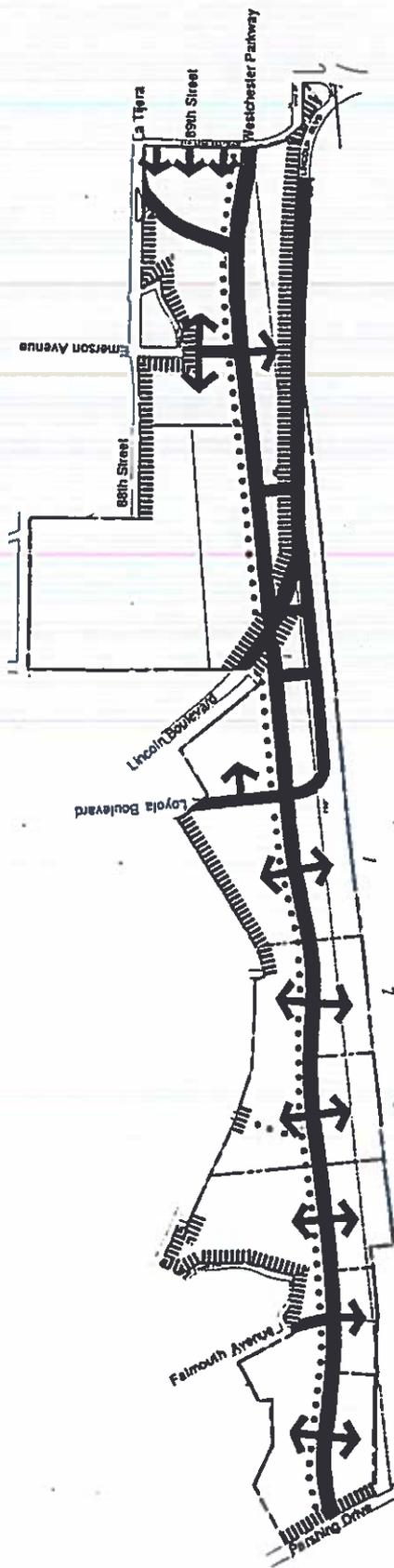
Notes:

1. Minimum parking based on zoning requirements. Changes in City of Los Angeles parking requirements are under consideration, and may increase requirement.
2. PM Peak Hour traffic determined by trip generation factors used in EIR.
3. Total acreage of Lot 4 is approximately 64 acres, of which 7 are occupied by the drainage channel.
4. Does not include 250-500 car Park and Ride.
5. Total Acreage of Lot 12 is approximately 102 acres, of which 78 are assumed to be allocated for the golf course.



4.0 SITE ACCESS GUIDELINES





Prohibited Access 
 Major Access Point 
 Bikeway 

Vehicular Access 

4.1 Vehicle Access, Driveways, and Parking

Background

A number of factors determine and limit vehicular access to the development parcels. The zoning conditions prohibit direct site access from Lincoln Boulevard, Pershing Drive, and all the local streets along the north edge of the site, except for Falmouth Avenue. Emergency line will be blocked at the north property line to prevent through traffic.

La Tijera Boulevard curves as it passes through the site, making left turn access to adjoining sites undesirable.

Major access drives, allowing left turns, along Westchester Parkway will be limited to enhance traffic flow and to reduce the disruption of the landscaping and medians.

Only Sepulveda Westway, Emerson Avenue from the south, Loyola Boulevard, and Falmouth Avenue, allow free access, and are of limited value to most development sites.

Major access drives along Westchester Parkway will provide the only access for most of the development sites within the research park. Each access will potentially serve several buildings, depending on the size of buildings and the final development plan. In addition to these major entries there may be additional entries allowing right turn only access to development sites.

Bikeways

Westchester Parkway is designated as a planned bicycle route within the City of Los Angeles Bicycle Plan. As part of the design of the road, a striped bike path within the roadway will be constructed. In addition, the "Q" conditions require that there be a bike path between Stanmoor Drive and Westchester Parkway, to the satisfaction of the Department of Transportation and the City Council Office.

Objectives

- To coordinate entrance drives with each other to create a simple, easily recognizable pattern of major and minor entries.
- To locate parking so that it is convenient and easy to find, but does not dominate the frontage along Westchester Parkway. Within the *Business Park* most of the parking will be located in large surface lots; within *Westchester Center* parking will be located in above-or-below grade structures, in small short-term lots north of Westchester Parkway and in surface lots within a no-building zone south of Westchester Parkway.

General Guidelines

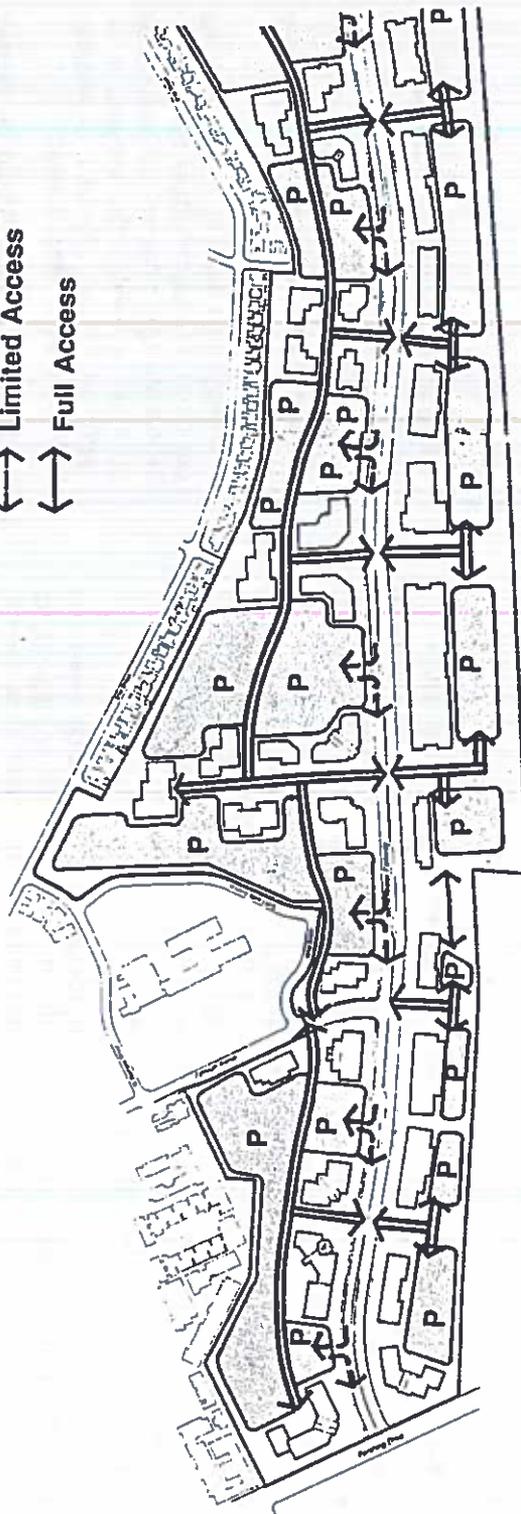
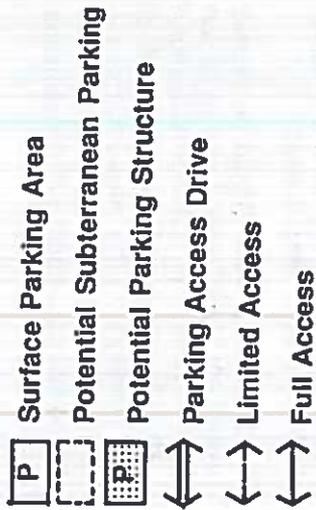
1. Major access points will be coordinated with median openings which will occur at intervals of 850 to 1,000 feet. Between the major access points or street intersections, there may be up to two minor access points designated for right turns only.

These minor access points should be no closer than 250 feet to a major access point or street intersection. All access drives will need to be approved by the City of Los Angeles Department of Transportation.

2. Driveways and parking areas should be designed to accommodate efficient vehicle stacking during peak periods, based on a site specific traffic analysis, to be approved by the City of Los Angeles Department of Transportation.
3. Large parking areas should be located away from Westchester Parkway, i.e., on the north side of the buildings on the north side of the street, and on the south side of the buildings on the south side of the street. Parking will not be permitted between buildings fronting on Westchester Parkway and the street.
4. All parking facilities should give priority to the convenient location of adequate van, carpool and bicycle parking.
5. Bicycle parking areas with racks having locking capabilities shall be provided. Retail or restaurant uses should provide bicycle parking near entrances in highly visible areas. Office or research buildings should provide secured bicycle parking near employee entrances.

Westchester Center Guidelines

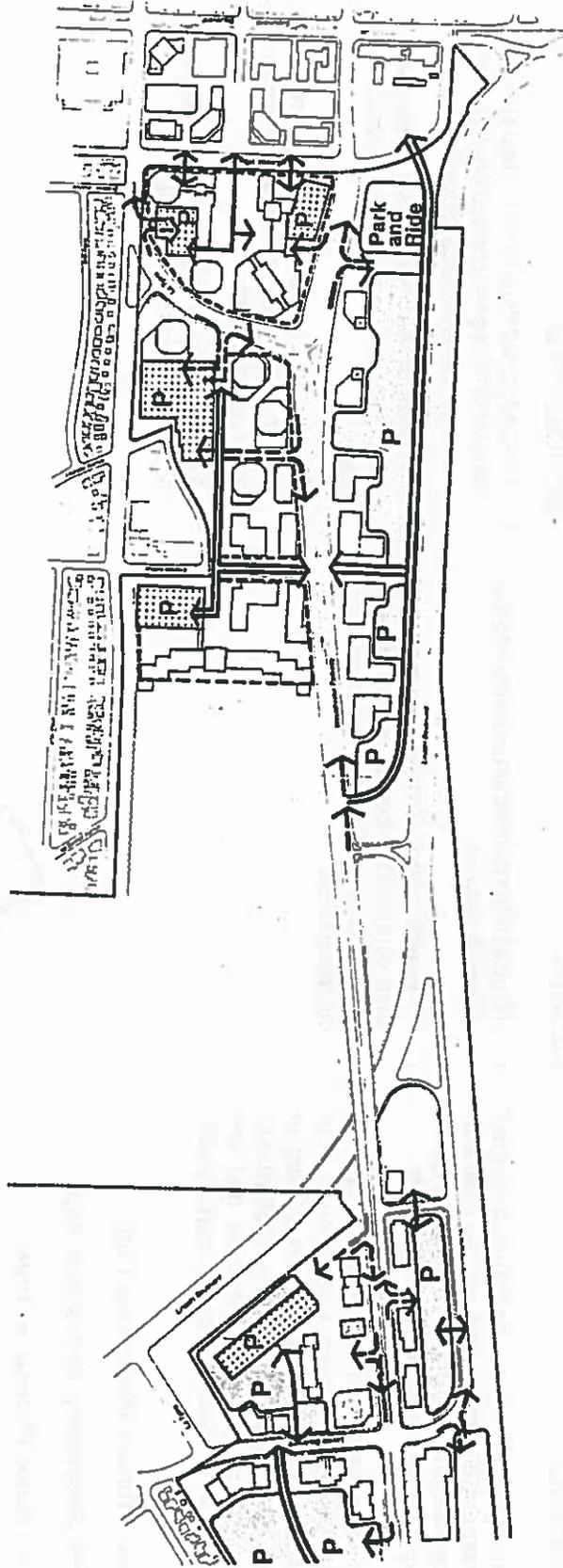
1. Parking will be provided primarily in above and below-grade structures, north of Westchester Parkway. Parking structures should be located so that they do not create the primary frontage on Westchester Parkway, Sepulveda Westway, or La Tijera Boulevard.
2. Access to parking structures will be primarily from Sepulveda Westway and Emerson Avenue, with minor access from Westchester Parkway and La Tijera.
3. Access to parking should be clearly marked but should not dominate the street frontages. Building entrances and pedestrian zones should create the primary focus from the streets.



Vehicular Access Concept

Business Park Guidelines

1. Full access drives, providing left and right turns into parcels, will be limited to not more than four drives between Loyola Boulevard and Falmouth Avenue, and one drive between Falmouth and Pershing Drive. No full access drives will be permitted between Lincoln and Loyola Boulevards.
2. An additional minor access point providing right turn only access may be provided at mid-blocks and between full access points.
3. Access drives parallel to Westchester Parkway should provide connections between full and minor access points, interior parking areas, and any buildings located between major access drives.
4. Direct vehicular access between airport related users and the airport road system may be permitted under some circumstances. Airport security and safety requirements must be met. Bridging of the existing drainage channel may be necessary and will require approval of the Department of Airports.



4.2 Pedestrian Circulation

Background

Westchester Parkway and other dedicated streets within the project will be built with sidewalks on both sides to City of Los Angeles Department of Transportation standards.

An adequate pedestrian circulation system, linking buildings, streets, parking areas, and public transit stops is essential in creating an environment which encourages employees to give up their private automobiles and use public transportation, vanpools, and carpools.

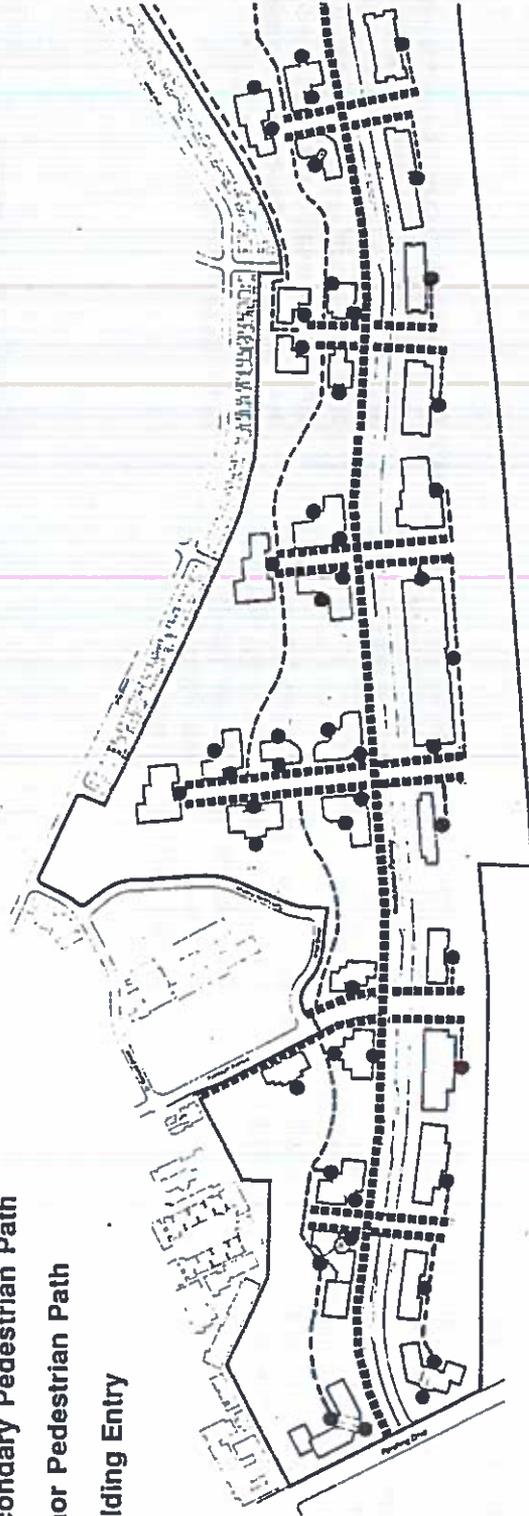
Objectives

- To tie all other pedestrian systems into the sidewalk network.
- To provide convenient and direct pedestrian connections between work places, public transportation, and services such as restaurants.

General Guidelines

1. The pedestrian circulation system should provide direct, paved connections between parking areas and the buildings they serve.
2. Retail and publicly oriented service should be concentrated along primary pedestrian paths.
3. Pedestrian amenities and building openings should be used to reinforce primary and secondary paths.
4. All pedestrian areas and destinations should be handicap-accessible.

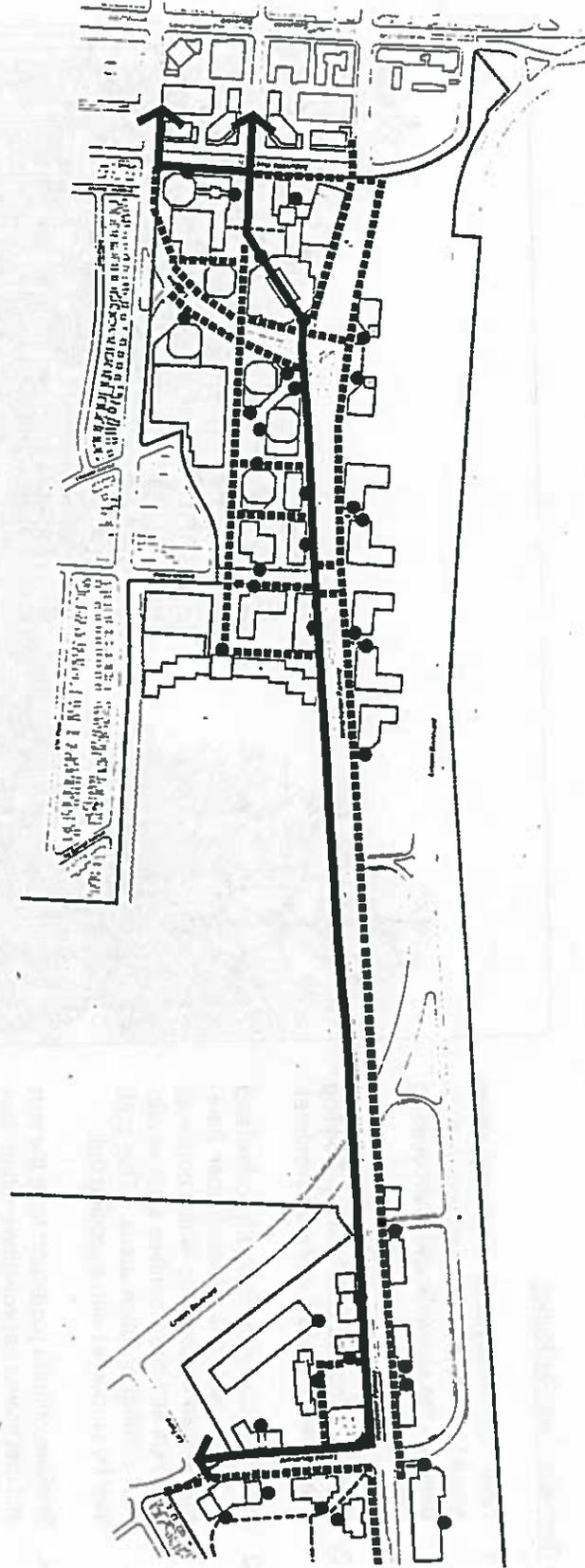
- Primary Pedestrian Path
- - - - Secondary Pedestrian Path
- - - - Minor Pedestrian Path
- Building Entry



Westchester Center Pedestrian Circulation Concept

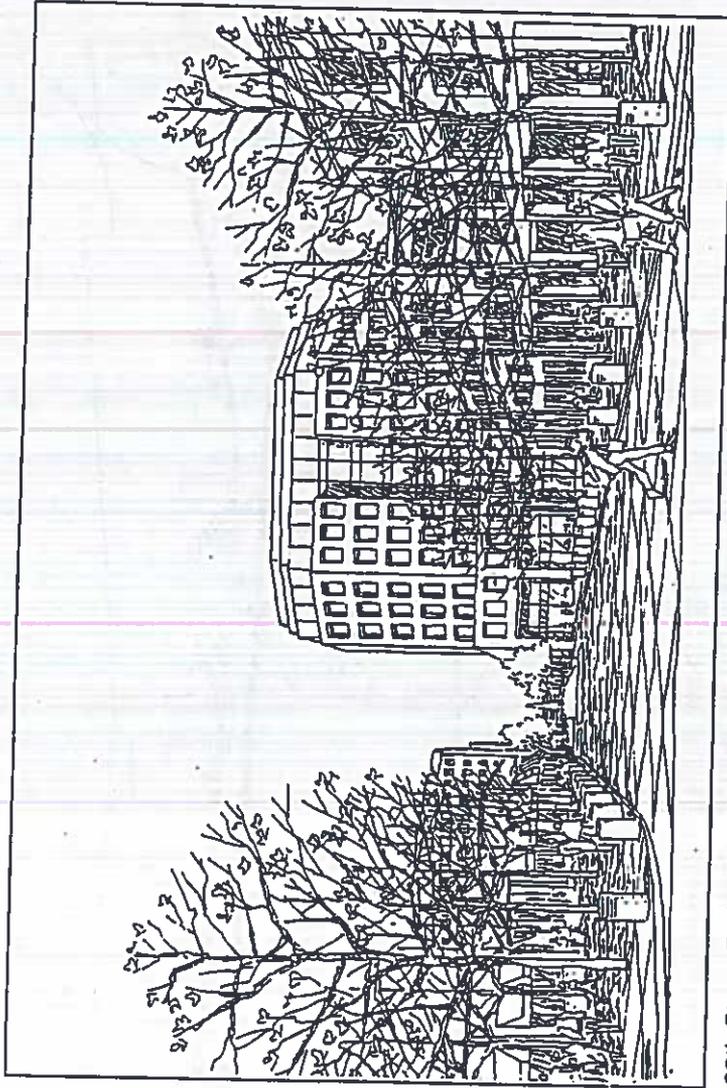
Westchester Center Guidelines

1. The sidewalk system should provide the basic framework of the pedestrian network. However, there should be additional paths which run through the center of the sites and provide more direct connections between buildings. These paths should be clearly designated by special landscaping and paving materials.
2. There should be pedestrian paths through Lot 11 connecting the transit station with the Westchester Business District to the north and east. These paths should be direct, but may cross through the public spaces of buildings, such as the retail court. The paths should be broken into a series of smaller segments, and should not pass through service areas or alley-like spaces.
3. Every major building should have a main entrance oriented toward the street-side of the building, connecting with the side-walk. There may be additional entries oriented toward interior courts and pedestrian paths.



Business Park Guidelines

1. North of Westchester Parkway there should be direct pedestrian connections between the sidewalk and main building entrances.
2. South of Westchester Parkway, building entrances may face away from the street.
3. A continuous pedestrian path north of and generally parallel to Westchester Parkway should be provided which connects buildings and site amenities such as outdoor seating or lunch areas. This path may be combined with a jogging trail.
4. Shelters offering protection from the sun and rain as well as providing seating shall be provided at all bus stops as part of the pedestrian system and landscaping of adjacent development.



89th Street Pedestrian Axis

5.0 ARCHITECTURAL GUIDELINES

5.1 Building Setbacks, Streetwall and Orientation

Background

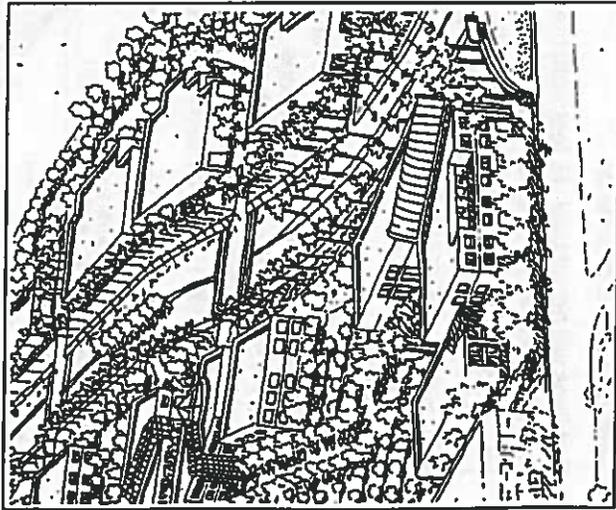
The purpose of the architectural guidelines is to establish a unifying design theme within which individual variations may occur. The overall effect of a group of buildings united by common frontage, related massing, colors and materials is a much more cohesive effect than a series of special object buildings typically found in office parks and commercial developments.

As part of the zoning conditions a number of setbacks from the northern property line are required. These setbacks establish the minimum buffer required between new development and existing neighborhoods. (See Section 2.2)

The FAA requires that all structures be setback a minimum of 750' from the centerline of the north runway. (See Section 2.3)

It is the intention that structures along the parkway will form a unified streetwall, broken only for access roads and view corridors. This streetwall will be the most important architectural element in unifying separate developments and relating buildings to the streets they face. Streetwalls are the most important tool for keeping any one building from dominating the street. The walls provide continuity to the street and at the same time allow decorative touches and changes which guarantee individual expression and distinction to each building.

In *Westchester Center*, the wall will be quite continuous, with breaks minimized. In the *Business Park*, the wall will be much less continuous, but will be used to define the space of the parkway as a linear park. Buildings will front onto Westchester Parkway, which will unify and give identity to the entire development.



Streetwalls

Objectives

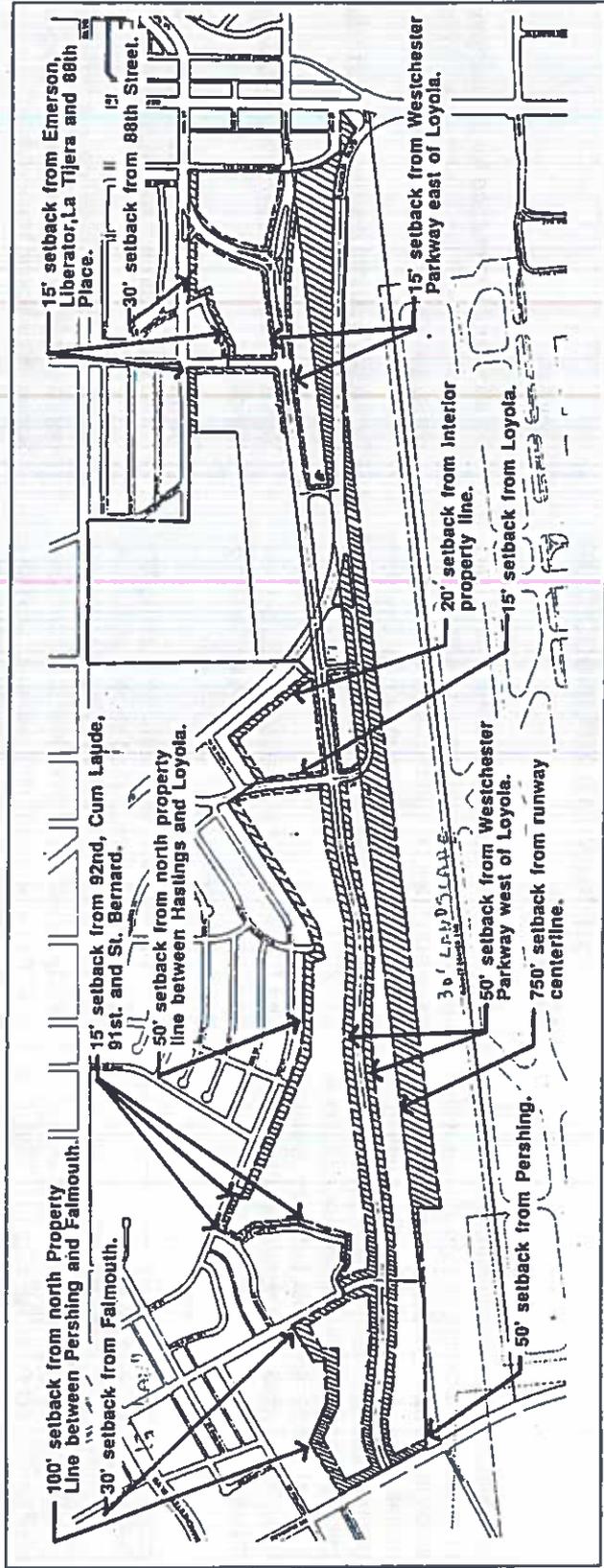
- To allow sufficient space for landscaping and active use of the space along Westchester Parkway.
- To allow views into deeper portions of the site at key places.
- To allow views between buildings toward the airport.
- To provide for buffers between buildings and adjacent streets and development.
- To use buildings to define a consistent front along Westchester Parkway and give spatial definition to the street.

General Guidelines

1. All buildings shall be setback a minimum of 750 feet from the centerline of the north runway, as required by the FAA.
2. All buildings shall conform to setback requirements established by the "Q" conditions.

Westchester Center Guidelines

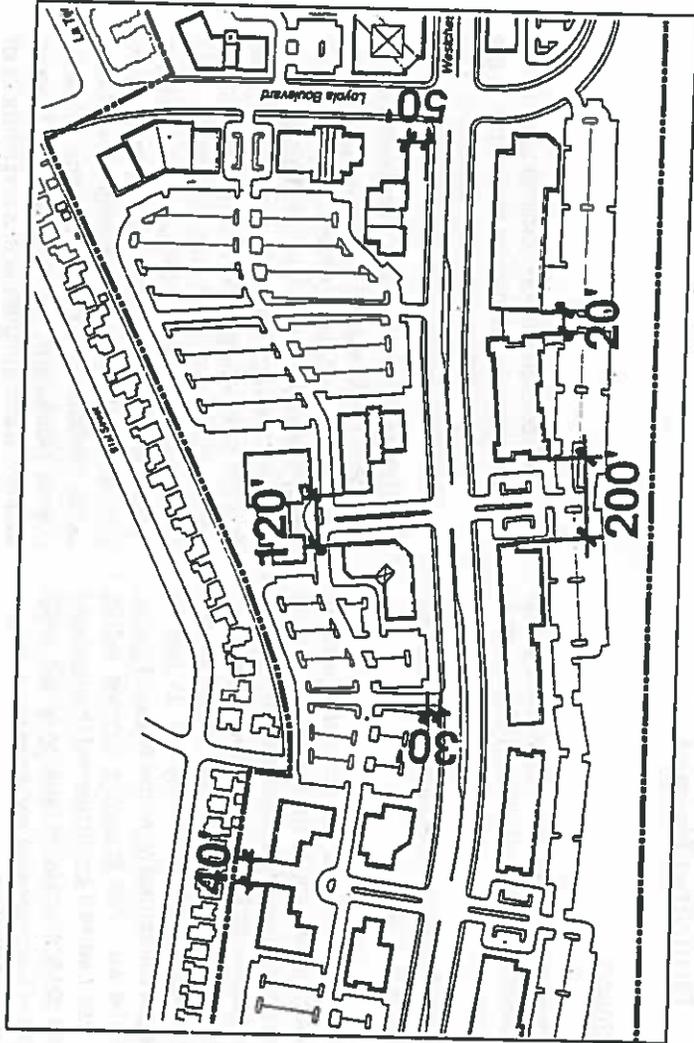
1. Buildings will be set back 15 feet from the property line along Westchester Parkway, La Tijera Boulevard and Emerson Avenue. Buildings need not follow this line, but they should come out to it unless prevented from doing so by the proposed light rail alignment.
2. At the intersection of La Tijera Boulevard and Westchester Parkway, buildings on the south side of Westchester Parkway should be set back a minimum of 100 feet from an extension of the centerline of La Tijera Boulevard. This area will be used as a special site for recreational viewing of the airport operations.
3. A 10 foot setback will be required along Sepulveda Westway.
4. FAA requirements will establish setbacks from Lincoln Boulevard.
5. In the areas adjoining the Westchester Business District, major building forms should be oriented so that they are orthogonal to (lined up with) the existing street grid, rather than following the irregular alignment of Westchester Parkway and La Tijera.
6. The relationship of Westchester Center buildings to the street is intended to be consistent. Buildings should be built to the setback lines described above. This pattern should not be broken except in the case of carefully selected open spaces.



Building Setback Summary

Business Park Guidelines

1. Buildings should be set back a minimum of 50 feet from Pershing Drive and Westchester Parkway west of Loyola Boulevard. Within this setback, a minimum of 30 feet adjoining the right of way should be landscaped; the remaining 20 feet may be used for parking or service areas.
2. East of Loyola Boulevard the buildings should be set back 15 feet from the property line of Westchester Parkway.
3. Buildings shall be set back a minimum of 30 feet from the final right-of-way of Falmouth Avenue. Of this, a minimum of 15 feet adjoining the right-of-way should be landscaped.
4. It is desirable that all buildings facing Westchester Parkway should bring their front walls as close as possible to the setback line, and should have their long walls parallel to the north runway of LAX.
5. Buildings may vary from the setback line as long as the spatial definition established by the entire surface of the building along the setback line is maintained.
6. Buildings and parking should be set back a minimum of 15 feet from Loyola Boulevard.
7. Buildings south of Westchester Parkway should be set back a minimum of 100 feet from the extension of the centerline of Falmouth Avenue, and from the centerline of each major entry drive. Buildings north of Westchester Parkway should be set back a minimum of 60 feet from the centerline of each major entry drive into the site, perpendicular to Westchester Parkway.
8. Buildings should be set back a minimum of 20 feet from all interior property lines, except on the south side of Westchester Parkway in LAX Northside and west, where buildings may have a minimum of 10' site setback from interior property lines.
9. Buildings should in general be oriented at right angles to the main entrance drives, which will be perpendicular to Westchester Parkway.



Business Park Building Separations

5.2 Building Height

Background

Two significant factors, other than these guidelines, specifically control building heights within LAX Northside.

- The "Q" conditions restrict building heights in Lots 1, 2 and 4.

In "Parcels 1 and 4A (Parcel 4 west of Falmouth Avenue) no structure located within 200 feet of the north airport property line between Falmouth Avenue and Pershing Drive shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot. No structure located within Areas 1 and 4A south thereof shall exceed four stories including parking levels or 55 feet in height measured from the finished grade of the lot."

In "Parcel 2, no structure located within 100 feet of the north airport property line between Loyola Boulevard and Hastings Avenue shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot". (from "Q" Condition No. 3)

Mechanical and other accessory facilities located on the roof are exempted from the height restriction but must not be visible from nearby residential properties to the north.

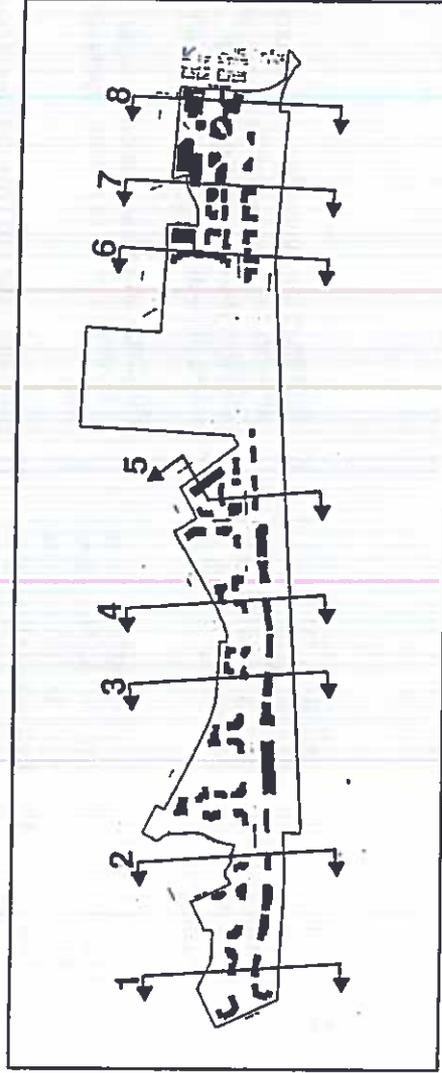
- The Federal Aviation Administration (FAA) regulates the heights of structures in the vicinity of the airport. See Section 2.3 for a description of these restrictions.

The design concept is that the buildings in the Business Park, except for the area between Loyola and Lincoln Boulevards, will be uniformly low and evenly distributed along Westchester Parkway. The buildings will help to define the landscape band along the parkway, and be closely related to the landscape. The buildings will, for the most part, be similar in height to the trees along Westchester Parkway, and at their maximum height, maintain a close relationship to the landscape. East of Loyola Boulevard, the proximity to Lincoln Boulevard and the greater mix and intensity of

uses, make greater building heights desirable. A varied and sculptured skyline, seen from Lincoln Boulevard and the airport runways, would make a significant addition to LAX Northside image.

Objectives

- To establish relatively uniform building heights west of Lincoln Boulevard.
- To allow maximum building heights within existing constraints, east of Loyola Boulevard.
- To establish a relatively uniform streetwall height in Westchester Center, but encourage an articulated skyline behind the streetwall.



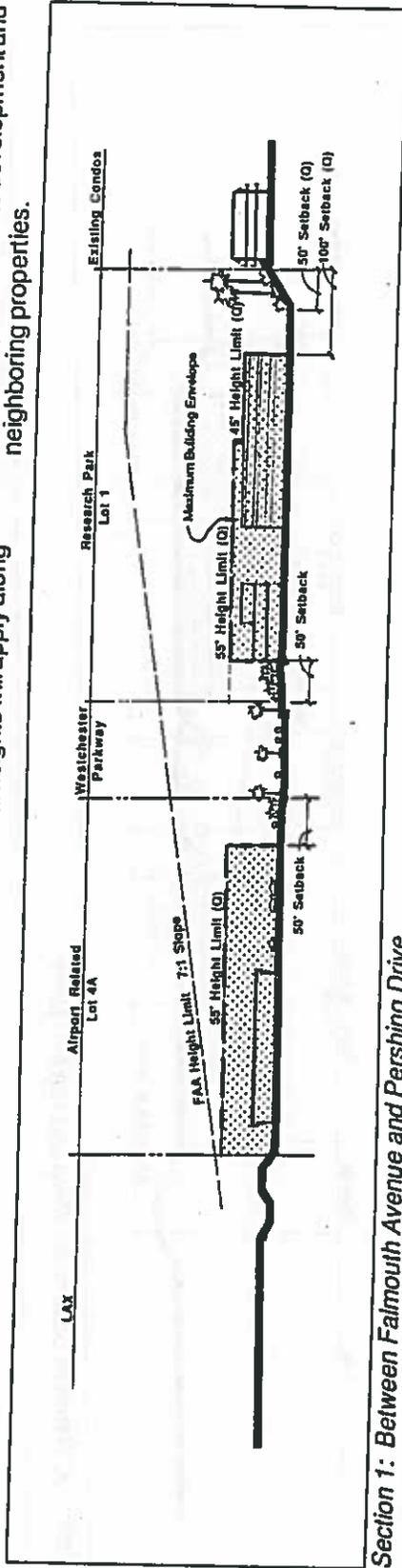
Key Map

General Guidelines

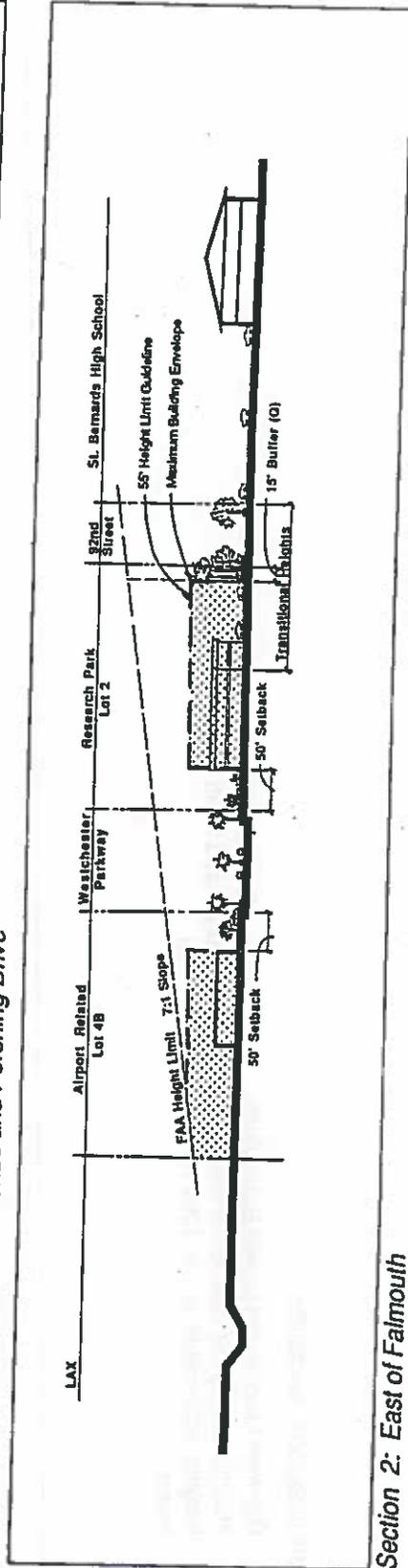
1. All structures shall conform with the requirements of the "Q" conditions.
2. All improvements shall conform to FAA regulations.
3. As required by City of Los Angeles Zoning Code, all buildings should not exceed the following transitional heights, when located within the distances specified from a single family residential neighborhood:

Distance	Height
0-49 feet	25 feet
50-99 feet	33 feet
100-199 feet	61 feet
4. The cross-sections which follow should be used as a guideline in determining maximum building heights, and the desired relationship between site development and neighboring properties.

These transitional heights will apply along



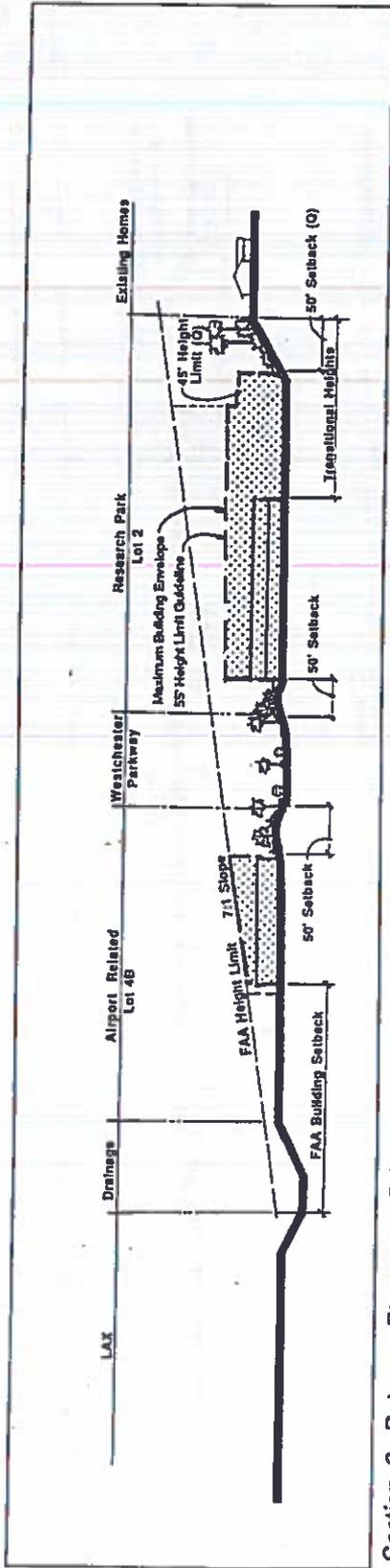
Section 1: Between Falmouth Avenue and Pershing Drive



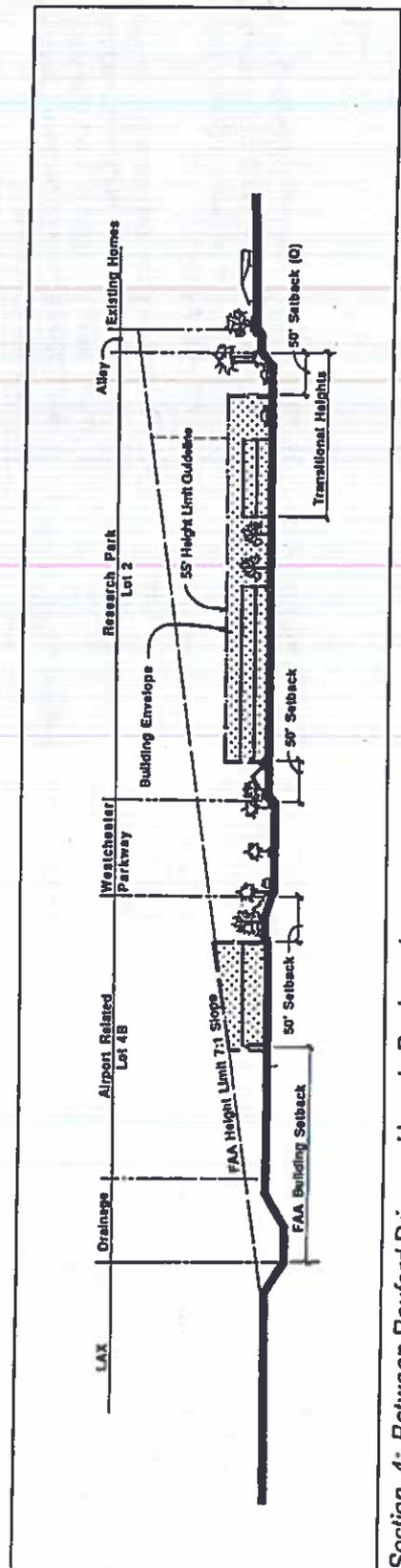
Section 2: East of Falmouth

Business Park Guidelines

1. Between Lincoln and Loyola Boulevards, structures may be built to the maximum heights achievable under FAA requirements.
2. West of Loyola Boulevard, no building should exceed three stories or 55 feet in height.



Section 3: Between Stanmoore Drive and Rayford Drive

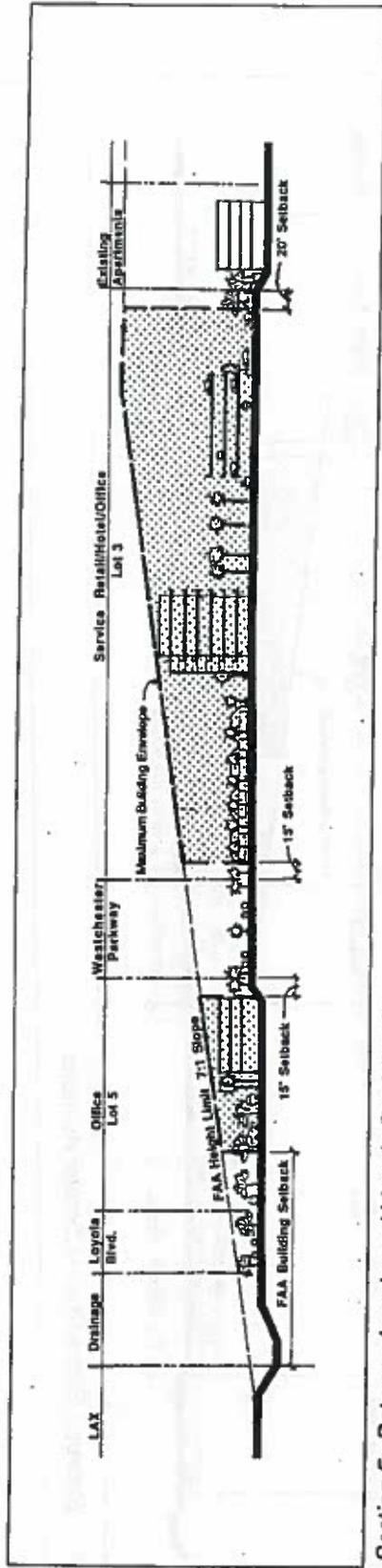


Section 4: Between Rayford Drive and Loyola Boulevard

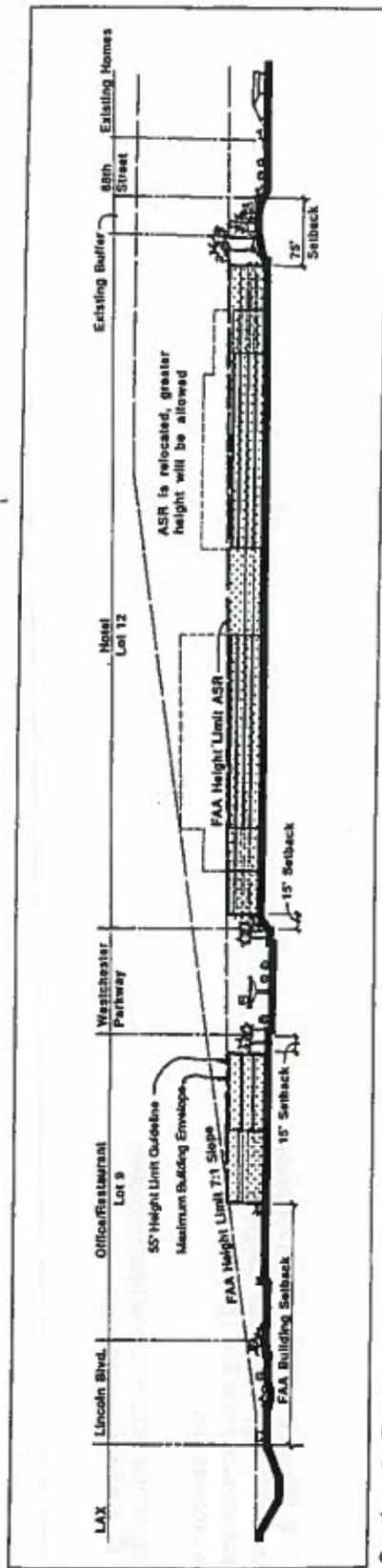
Westchester Center Guidelines

1. In order to ensure uniformity along the streetwalls of Westchester Parkway, all structures along Westchester Parkway are restricted to a maximum height of 45 feet measured along the 15 foot setback line.
2. If greater height is desired and permitted under FAA regulations, an additional setback of at least 10 feet is required. Behind this setback, building to the maximum height allowed by the FAA is encouraged.

The greatest opportunity for individual expression are major recesses, balconies, roof tops and setbacks is above this 45 foot height limit. The changes would be primarily noticed from longer views and contribute to the skyline of the project.



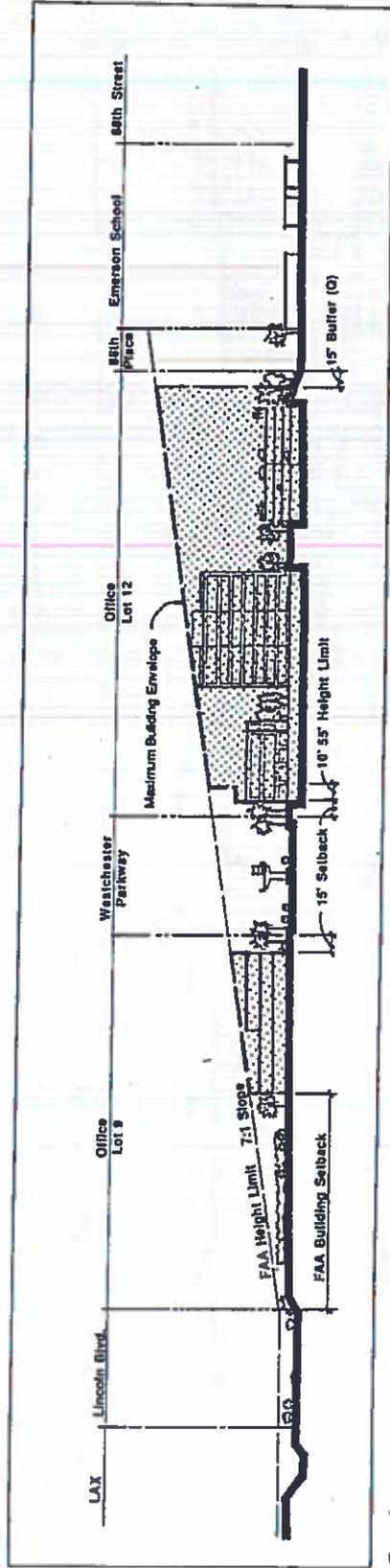
Section 5: Between Loyola and Lincoln Boulevards



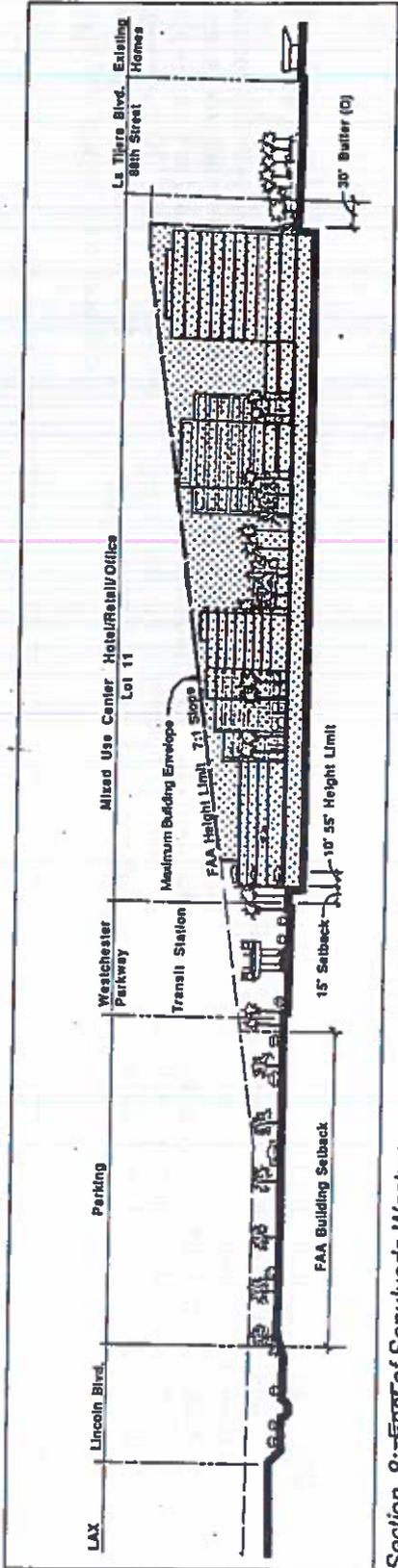
Section 6: Between Lincoln and Emerson

- Structures south of Westchester Parkway are restricted to a maximum height of 45 feet measured at a line 15 feet back from the property line.

Heights of structures are further controlled by FAA height regulations.



Section 7: Between Emerson and Liberator Avenues



Section 8: East of Sepulveda Westway West

5.3 Facade Articulation

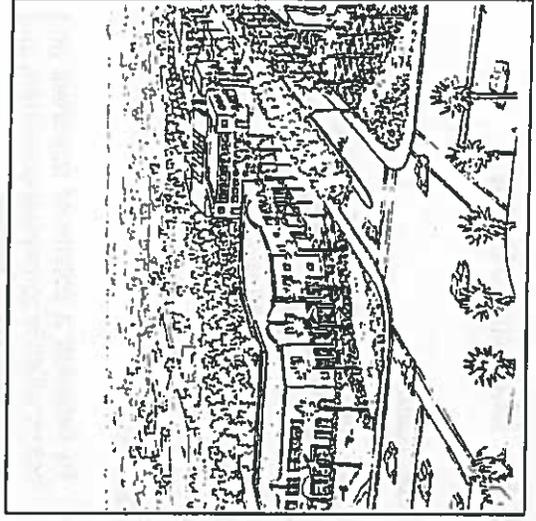
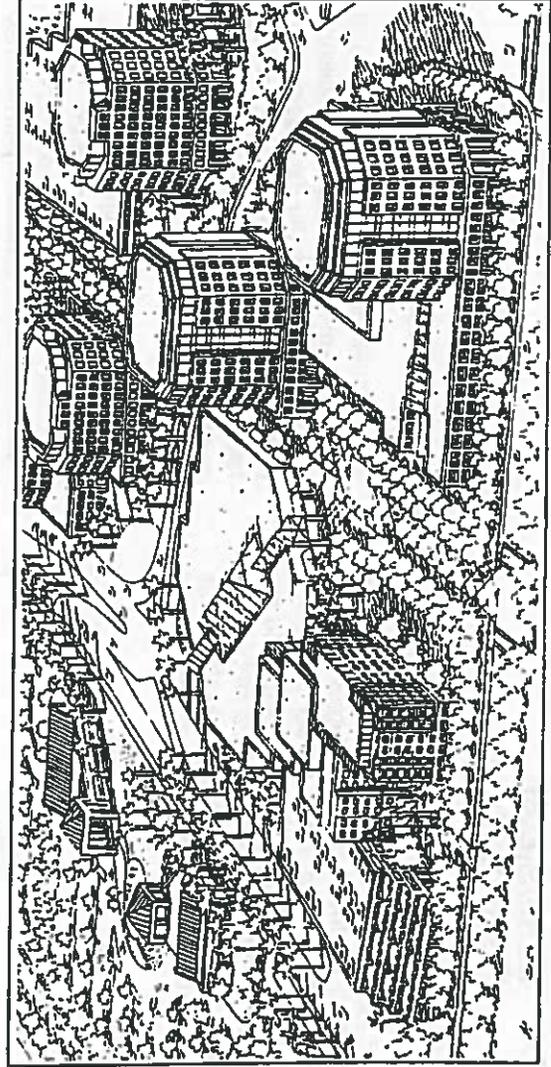
Westchester Center Guidelines

1. On most blocks a rhythm will be set either by lot widths or the structural bay widths of buildings. Standard proportions of the buildings in the area will be determined by prevailing street wall height and building widths.
2. Because of the large lot sizes, and relatively wide and low proportions of buildings, it may be necessary to break up facades into discrete elements, to ensure that a streetscape rhythm is created with an established set of proportions.

3. To reduce conflicts between adjacent buildings and minimize contrasts between individual architectural solutions, building designs should pay attention to the boundary shared with another building. Architectural features such as strongly contrasting window types that exaggerate the changes are discouraged at boundary lines. Other architectural concerns at the boundary lines between parcels, such as the base, choice of materials and colors, should be addressed in order to assure that adjacent structures relate to each other.

Business Park Guidelines

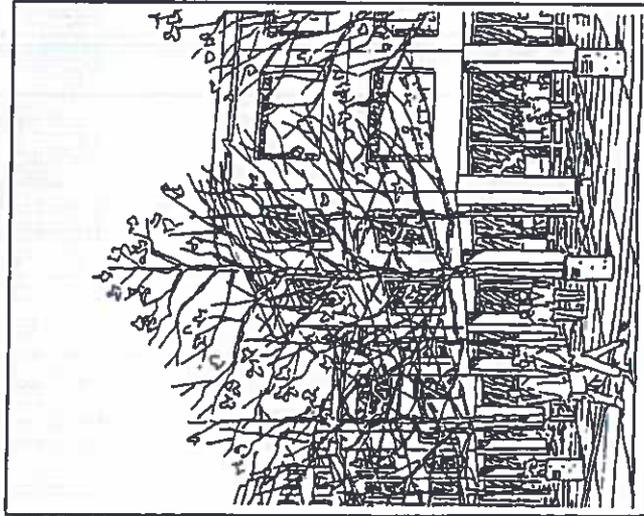
1. On the north side of Westchester Parkway flat, unbroken or blank wall surfaces facing the parkway are discouraged. A rhythmic break up and three dimensional modulation of wall surfaces through windows, structural bay expression, projected structural elements and recessed wall openings is highly desirable.
2. On the south side of Westchester Parkway, west of Loyola Boulevard, facades should be generally neutral, and blank walls will be permitted.
3. Highly contrasting, graphic patterns on the facades will not be allowed.



5.4 Base/Street Relationships

Background

The base of the building is generally understood to be the first or combined first and second floors and is the critical element in giving the building a positive relation to the street. It contains building entries, lobbies and areas of higher activity, and in taller buildings establishes the architectural base for the upper floors.



Westchester Center Guidelines

1. There should be a distinctive architectural expression of the building's base utilizing techniques such as the expression of a cornice line, changes in materials or surface texture, geometry, proportions of openings, setbacks and recesses that create shadows.
2. The base should be built of permanent masonry materials such as stone, block, brick, tile or integrally colored concrete. Neither metal nor stucco are permitted as predominant materials.
3. Blank walls at ground level are discouraged along retail frontages. A maximum number of windows is encouraged.
4. Ground level windows must be transparent. Very dark or mirrored glass is prohibited within the base of the building.
5. Sun shielding should be accomplished architecturally either by recessing or by means of awnings.
6. Buildings' ground floor levels should match at the adjoining sidewalk level.

5.5 Materials/Colors

Background

Light colors are effective in reflecting light, reducing heat gain in buildings and make an effective backdrop for landscaping. For these reasons lightly colored buildings tend to dominate in southern, warm climates such as that of Los Angeles.

Objectives

- To establish a palette of materials and colors which is somewhat consistent and responsive to the climate.
- To minimize marked contrasts between adjacent buildings.

General Guidelines

1. The predominant colors of exterior building materials should be light in tone. Neutral, warm earth tones and pastels of very light value are preferred.
2. At the ground level bright accent colors associated with pedestrian activity are encouraged.
3. Mirror glass and highly reflective surfaces will not be allowed as dominant building materials.
4. Tilt-up or pre-cast concrete with integral color, and metal panels with a matte silicone polyester or Kynar finish are desirable.

5.6 Roofs

Background

Most of the building will be low, with large expanses of roof areas. Roof design becomes important when mechanical equipment located on them might be visible from adjacent property or when the roofs themselves are visible from nearby residential buildings at a higher grade or from nearby, taller buildings.

General Guideline

1. All mechanical roof top equipment should be screened from view from nearby streets and property.

Westchester Center Guideline

1. The roofs of the lower buildings within *Westchester Centers* should be interesting to look at from nearby, taller buildings.

LA Business Park Guideline

1. The roofs of buildings in Lots 1 and 4A shall be specifically designed to minimize impact on views from nearby residences.

5.7 Parking Structures

Background

Parking structures will be designed to the highest standards of quality, and should comply with the other architectural guidelines in this section, but additional requirements pertain because of the nature of the building type.

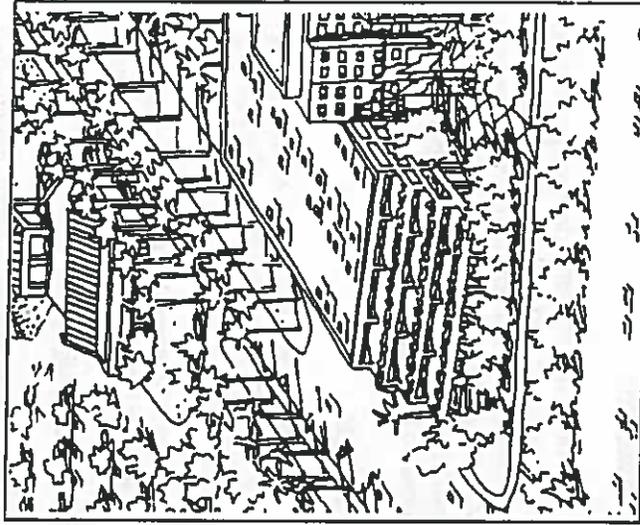
General Guidelines

1. Sloped ramps are to be located within the structure and away from any street elevations.
2. Ground floor areas adjacent to primary pedestrian paths should be occupied with retail or service uses. Where this occurs there should be special architectural treatments of the commercial frontage.
3. Pedestrian entries to parking garages should be at ground level. Upper level connections between parking structures and the buildings they serve are discouraged.
4. Colorful canopies, awnings and porte cocheres should be used to mark both vehicular and pedestrian entries to the garage.
5. All lighting within and on the roof of the structure should be shielded so that the light sources are not visible from adjacent property or rights of way.

5.8 Ancillary Buildings

General Guidelines

1. Ancillary buildings should relate in style and materials to the main buildings with which they are associated. They should be built of permanent materials.
2. Ancillary buildings, including gate houses, should conform to all setback guidelines.



5.9 Service, Loading and Utilities

Objectives

- To provide service and mechanical elements in a simple, efficient way which does not interfere with the building's function.

General Guidelines

1. Loading areas should be accommodated entirely on-site.
2. Parallel parking space for delivery trucks should be provided along service drives or in specially designated courts or loading docks.
3. Loading docks and trash storage should not be located along major street frontages and should be screened from view by walls and doors. Service elements such as loading doors should be integrated with the facade design in an organized manner.
4. Service areas should be located in a manner that does not conflict with auto or pedestrian movements.
5. No materials, supplies or equipment, including trucks or other motor vehicles, are to be stored on-site unless inside a closed

building or behind an architectural screening, except for airport-related uses located in Lot 4, out of the view of public streets.

6. Exterior utilities such as water, gas, sewage, electrical, and communication lines must be installed underground.

7. All mechanical equipment, utility meters, storage tanks, air conditioning equipment, and similar equipment shall be screened from view by landscaping or attractive architectural features integrated into the structure, except that in lot 4, such equipment located on the backside of buildings, and out of the view from public streets, need not be screened.

5.10 Exterior Walls and Fences

Background

Along the north property line walls and fences are required as part of the buffer system, but elsewhere they will not be needed except to screen specific areas of uses, define outdoor courtyards, or for security of airport-related uses and the airport.

Objectives

- To design walls and fences which integrate with and complement the building architecture.
- To avoid interrupting the continuity of the linear open space along the parkway.

General Guidelines

1. No walls or fences shall be constructed within the building setback along Westchester Parkway, La Tijera Boulevard, Loyola Boulevard, Falmouth Avenue or Pershing Drive.
2. Walls and fences will not be permitted along interior lot lines, except in Lot 4.
3. Perimeter fences and walls required by the zoning conditions shall be provided. (See Section 6.0, Landscape Guidelines.)

6.0 LANDSCAPE GUIDELINES



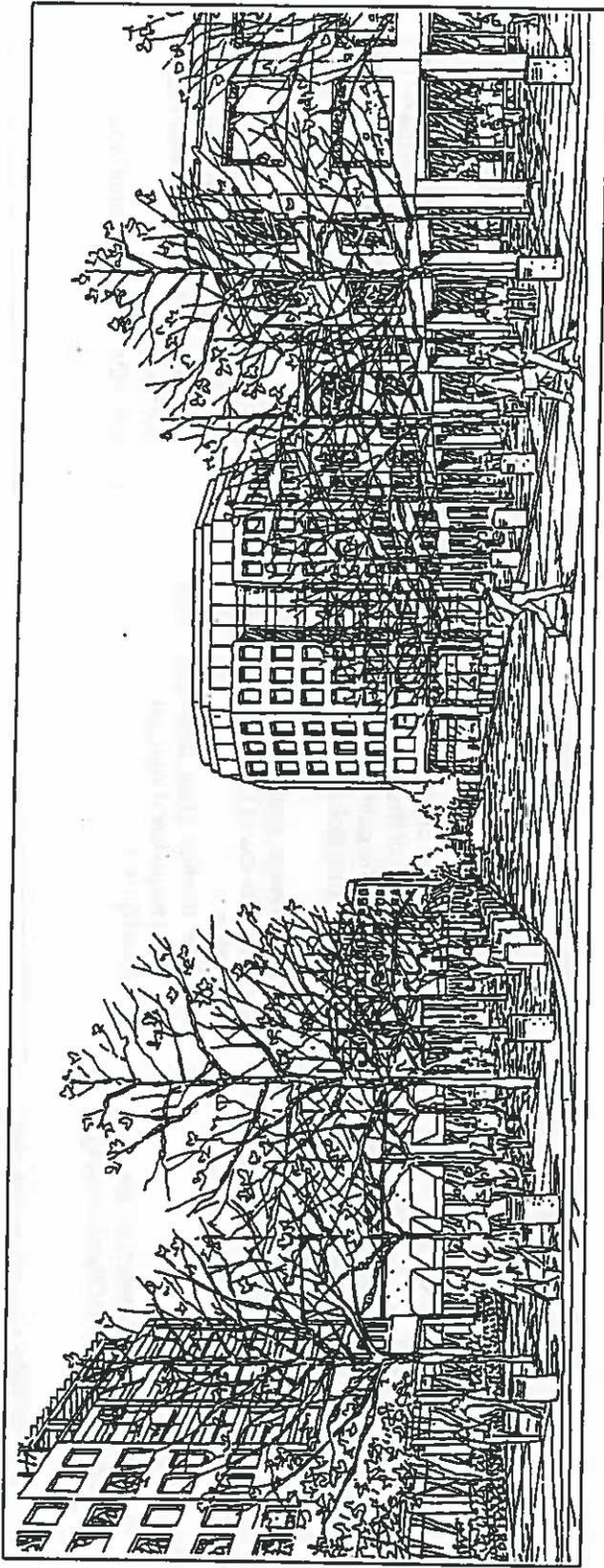
The "Landscape" as described in this section refers to all those elements that give form to the exterior environment of LAX Northside business development. These include such items as streets and building setbacks, the variety and placement of plant materials, walkways, signing, lighting, site furnishings, and arrangement of major functional elements including project entries, parking lots, buildings, service areas, etc. The objective of these Landscape Guidelines is to ensure that while project architecture and building sites within

the business community vary in type and size, landscaping as a design element will play the key role in creating and conveying the overall character of LAX Northside development.

The landscape design and guidelines support the overall development concept of the two zones, as defined in Section 1.3. The first zone is *Westchester Center* in which the landscape exhibits an urban character. The second zone is located on the western portion of the site and is called the *Business Park*. The concept for

the *Business Park* is to create a "parklike" suburban working environment. The two zones, although contrasting, will be linked together by the street landscaping and the landscape buffer on the north property boundary.

The landscape guidelines are organized into three prominent landscape areas: 1) Public Streetscape, 2) Common Areas, and 3) Parcels. Following each section of the three landscape areas will be a selected list of plant material which is appropriate to that zone.



Street Landscaping

6.1 Public Streetscape

The Public Streetscape includes street medians and parkways; these elements form the overall project area image. Within LAX Northside development there are eight streets which have general and specific landscape requirements. The eight streets are:

- 1) Westchester Parkway, 2) La Tijera Boulevard, 3) Falmouth Avenue, 4) Pershing Drive, 5) Loyola Boulevard, 6) Lincoln Boulevard, 7) Sepulveda Westway, and 8) Emerson Avenue. Each street will address, if applicable, the requirements for the landscape median and the 10' landscape parkway.

General Guidelines

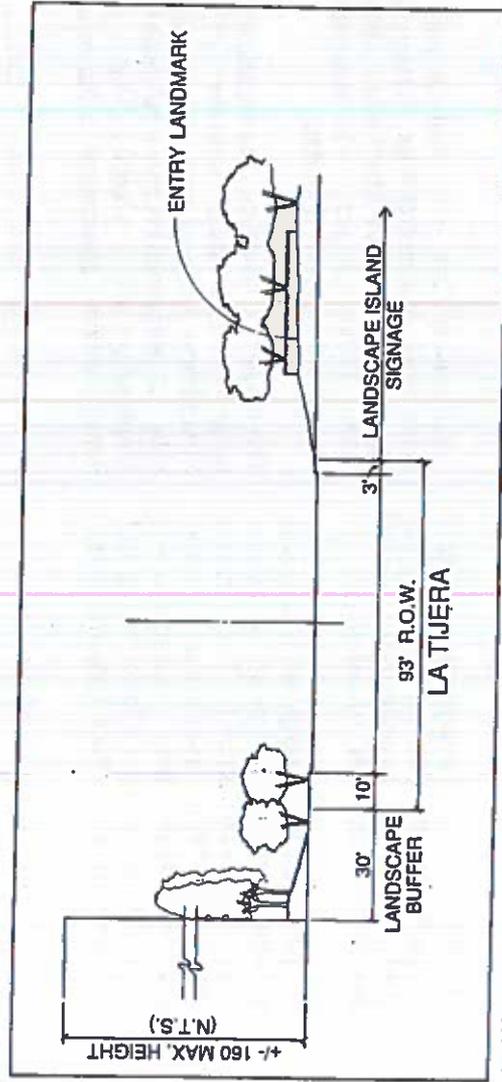
1. **Tree Grates:** Tree grates shall be cast iron, as approved by the City of Los Angeles standard.
2. **Concrete Sidewalks:** Concrete walks shall be ten feet (10') wide, per City of Los Angeles standard.
3. **Handicap Ramps:** Handicap ramps shall be provided at all locations where sidewalks meet street corners or driveways and shall be designed consistent with Federal, State and City handicap requirements. Refer to the Los Angeles City Standard.
4. **Street Trees:** Street trees shall be spaced per City of Los Angeles Street Trees Standard available through Public Works. Designated street trees are indicated below.

Westchester Center Guidelines

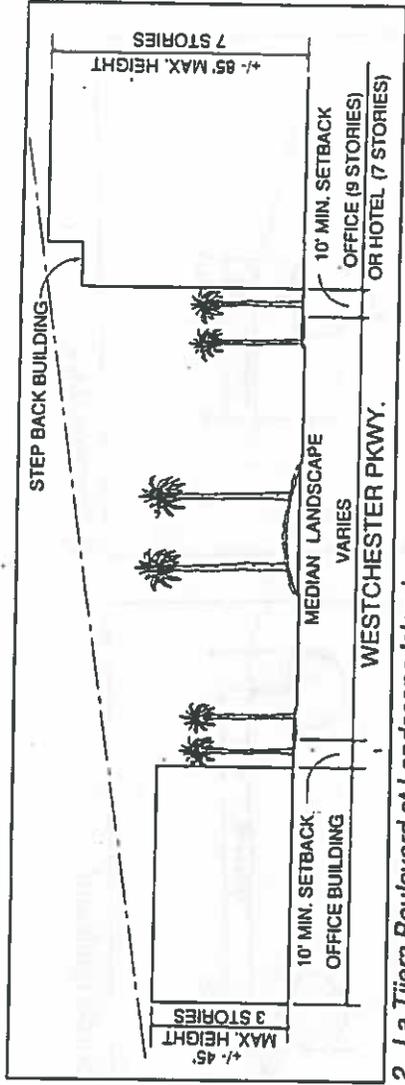
1. **Westchester Parkway at Westchester Center.**
 - a) A single row of Queen Palms shall line both sides of the street @ 40 feet on center. The palms shall be located behind the back of the curb with a tree grate.
 - b) The median shall be landscaped with a double row of Canary Island Date Palms.
 - c) The median shall have a 5:1 maximum slope berm with turf.
 - d) See figure 1.

2. La Tijera Boulevard:

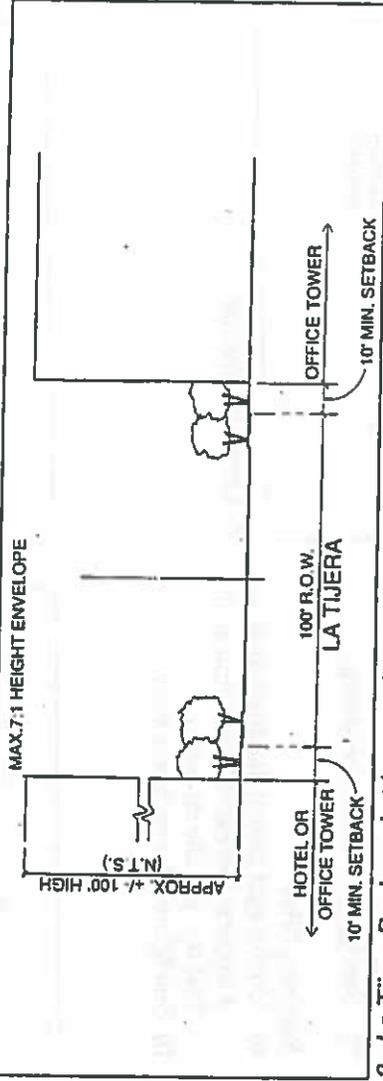
- a) A single row of Carrotwood trees shall be planted @ 40 feet on center, on both side as illustrated.
 - b) See "Common Areas" section for the landscape requirements of the 30 foot landscaped setback buffer and La Tijera gateway.
 - c) See figure 2-4.
3. **Sepulveda Westway:**
 - a) A single row of Magnolia Grandiflora Majestic Beauty shall be planted on each side of Sepulveda Westway.
 - b) See figure 5 for sectional view.



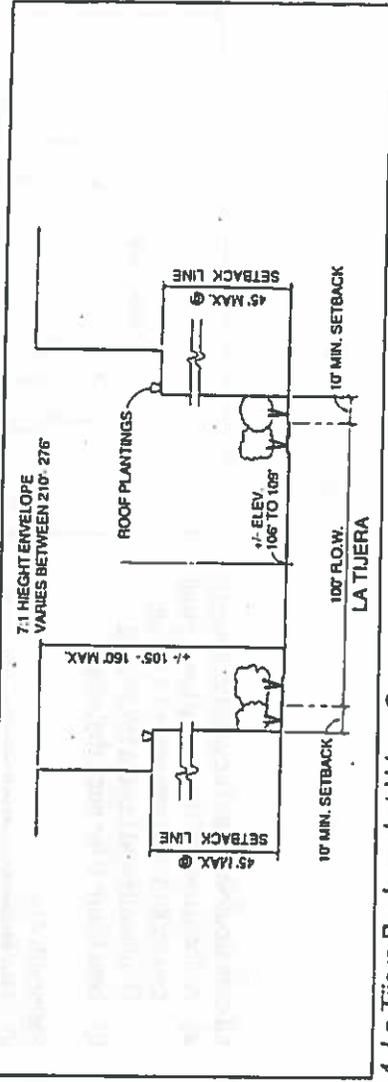
1. Westchester Parkway at Westchester Center



2. La Tijera Boulevard at Landscape Island



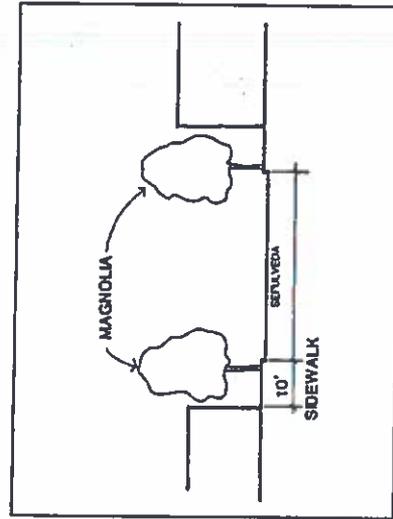
3. La Tijera Boulevard at Intersection Tower



4. La Tijera Boulevard at Urban Gateway

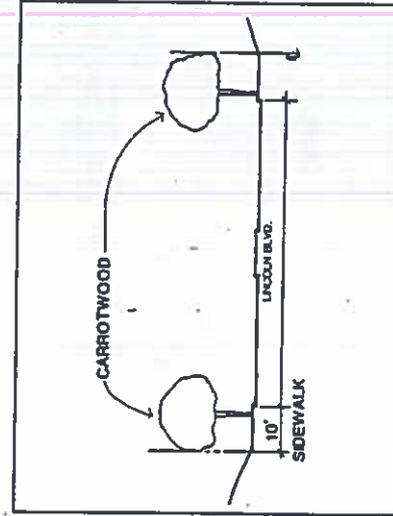
Business Park Guidelines

1. Westchester Parkway at the Business Park
 - a) A single row of Queen Palms shall be placed adjacent to the curb on both sides of LAX Northside at 40' on center.
 - b) The landscape median shall be planted with alternating rows of Canary Island Date Palms and Coral Trees.
 - c) A row of shrubs shall be located down the center of the median with Hedera Heix Needlepoint as the common ground cover. The row of shrubs shall be low varieties of Nerium Oleander and Raphiolepis Indica.
 - d) See figure 5.

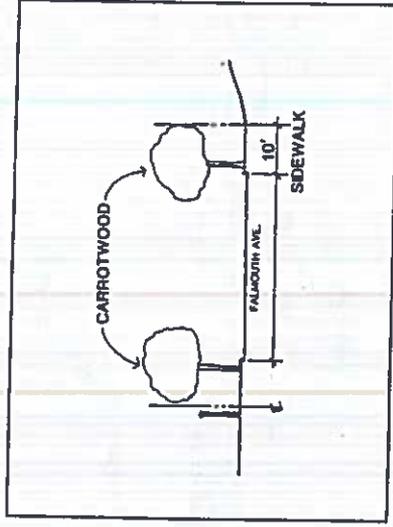


5. Sepulveda Westway

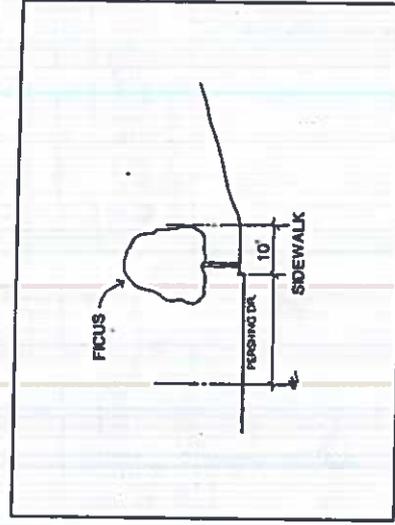
2. Lincoln Boulevard and Loyola Boulevard:
 - a) A single row of Carrotwood trees shall be planted on each side of Lincoln Boulevard and Loyola Boulevard.
 - b) See figure 6 for sectional view.
3. Falmouth Ave
 - a) The street tree shall be Carrotwood tree.
 - b) See figure 7 for sectional view.
4. Pershing Drive
 - a) On the east side of the drive shall be a single row of Ficus Microcarpa at 40 feet o.c. per city standard.
 - b) See figure 8 for sectional view.



6. Lincoln Boulevard



7. Falmouth Avenue



8. Pershing Drive

6.2 Common Areas

The "Common Areas", as defined in the guidelines, are key visual landscape areas that give the project identity when one arrives by air and drives through the project. The elements of the Common Areas include:

- 1) Landscape Buffer Setback, 2) Gateways, and 3) Primary and Secondary Intersections. (See Figure 9).

Landscape Buffer Setback

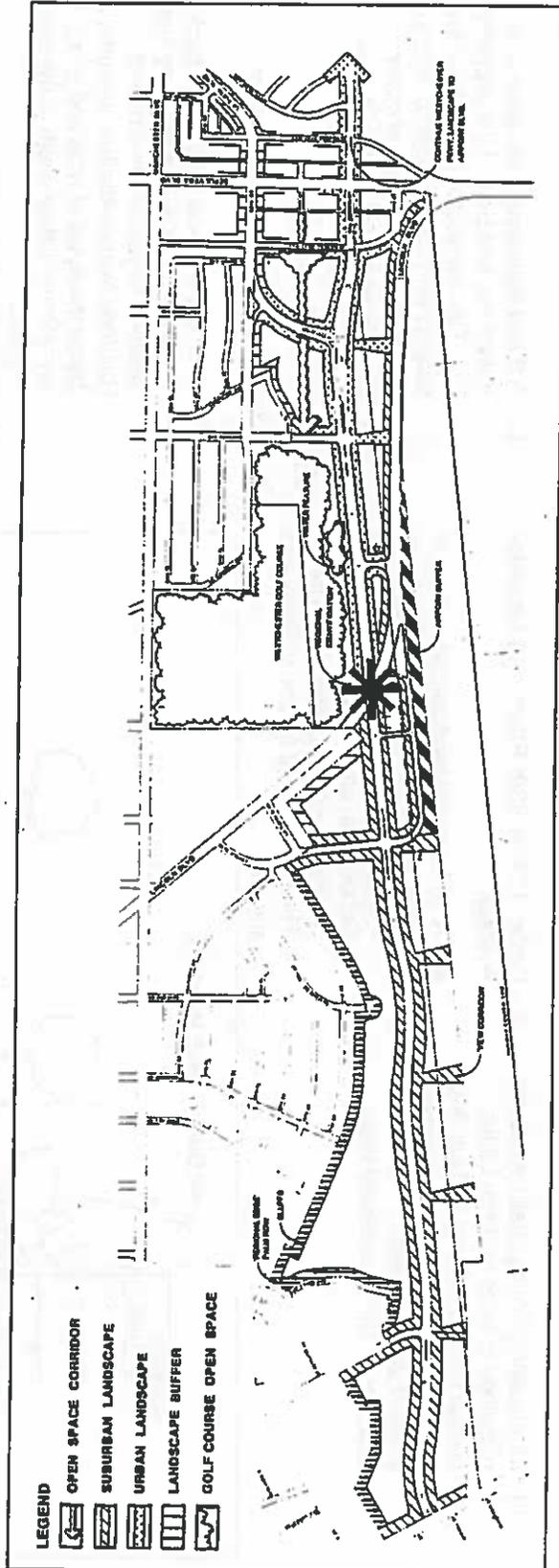
Landscape buffer setbacks are a transition zone which screen and buffer the residential uses from the project development. The buffers reflect the implementation of the project's zoning requirements.

General Guidelines

1. Since the common area landscape will not be installed at one time, each project shall adhere to the guidelines set forth and if a project is already in place, the new project should design its landscape to relate to it. The landscape should appear to be continuous and uniform.
2. Refer to Figure 9 for landscape setback requirements.

Westchester Center Guidelines

1. Landscape Buffer Setback at Parcels 11 and 12A along La Tijera Boulevard and 88th Street:
 - a) A 30 foot wide landscape buffer setback along 88th street between Sepulveda Westway and Liberator Avenue shall be required. No building or structures are allowed within the setback. Screen walls are permitted, but shall not exceed 6 feet in height from street elevation. Walls and fences shall integrate with architectural design theme.
 - b) Refer to Figure 9 for landscape setback requirements.



9. Open Space Diagram

- c) Landscaping planting shall be informal massings of trees and shrubs that screen surface parking and parking structures and reduce the perceived scale of development.
 - d) See Figure 10 for sectional view.
2. Parcel 12A at 88th Place and Liberator Avenue:
- a) A 15 foot wide landscaped buffer set back along Liberator Avenue and 88th Place (adjacent to the Emerson Manor School site) shall be planted with a dense row of evergreen screen trees. The trees should screen visibility into the site.

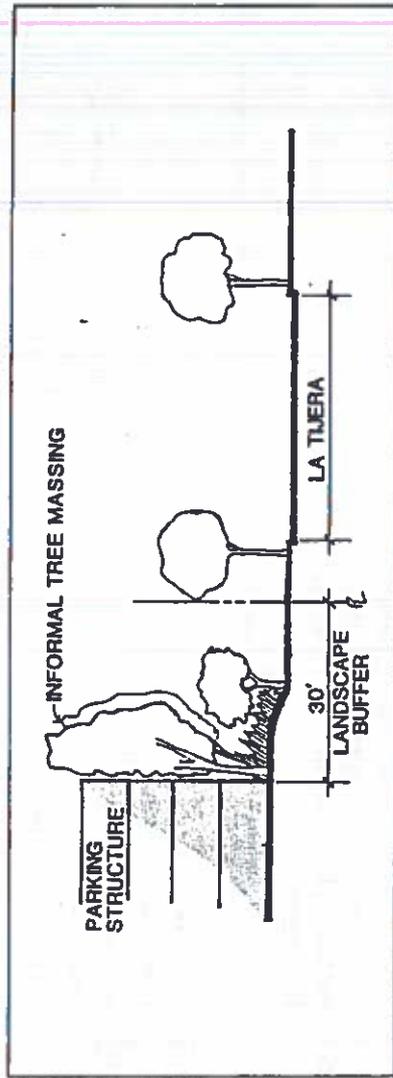


Figure 10

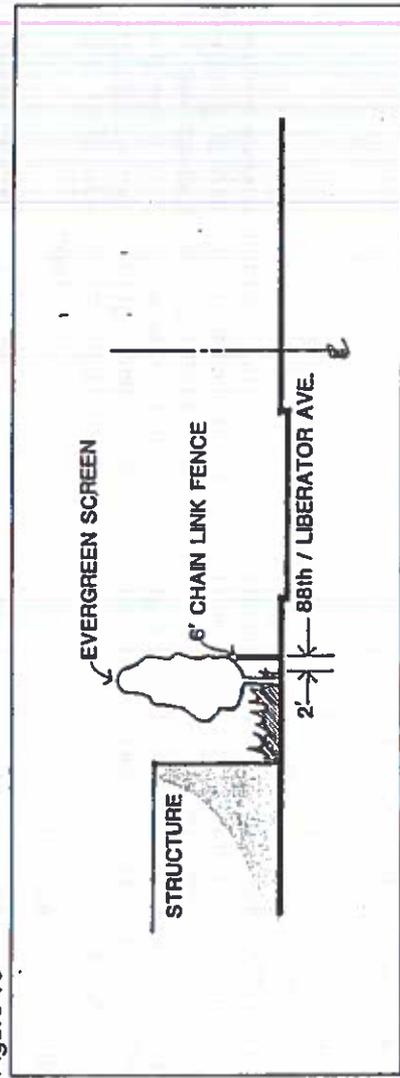


Figure 11

- b) A 6 foot high chain link fence is required two feet behind the property line. This two foot area shall be landscaped with massings and layers of shrubs, vines and ground cover.
- c) See Figure 11 for sectional view.

Business Park Guidelines

1. Parcel 2 along north property line:
- a) A 15 foot landscape buffer setback along the north airport property line between Loyola Boulevard and Hastings Avenue shall be planted with dense trees, tall shrubs and ground cover immediately south of the property line.
 - b) Where the landscape buffer is on a slope condition, the proposed planting should effectively control erosion problems.
 - c) An 8 foot high masonry block wall is required along the north property boundary. The wall shall be measured in height from the finished grade of the alley between Loyola Boulevard and Rayford Drive. The masonry wall shall be constructed out of slump stone. The color shall be light in value.
 - d) See Figure 12 for sectional view.

2. Parcel 2 along Cum Laude Avenue and 92nd Street:

- a) A 10 foot landscape buffer along the property line shall be planted with a dense informal grove of evergreen trees. The visibility into the site from this edge shall be screened.
- b) A 6 foot high chain link fence is required along the property line. The fence area shall be heavily planted with shrubs, vines and ground cover.
- c) See Figure 13 for this sectional view.

3. Parcel 1 along the north boundary between Falmouth Avenue and Pershing Drive:

- a) The 50 foot wide landscaped buffer setback shall be planted with low shrubs and ground cover that will not obstruct views from the residential units.
- b) A 6 foot high chain link fence is required along the property line.
- c) See Figure 14 for this sectional view.

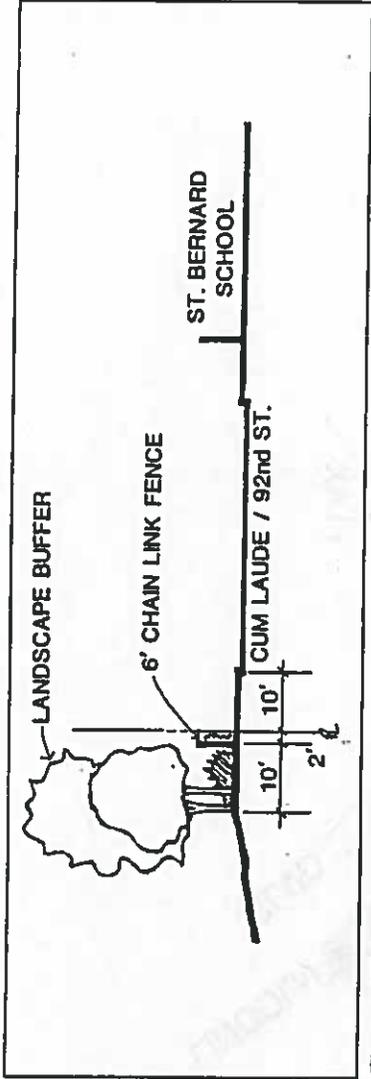


Figure 12

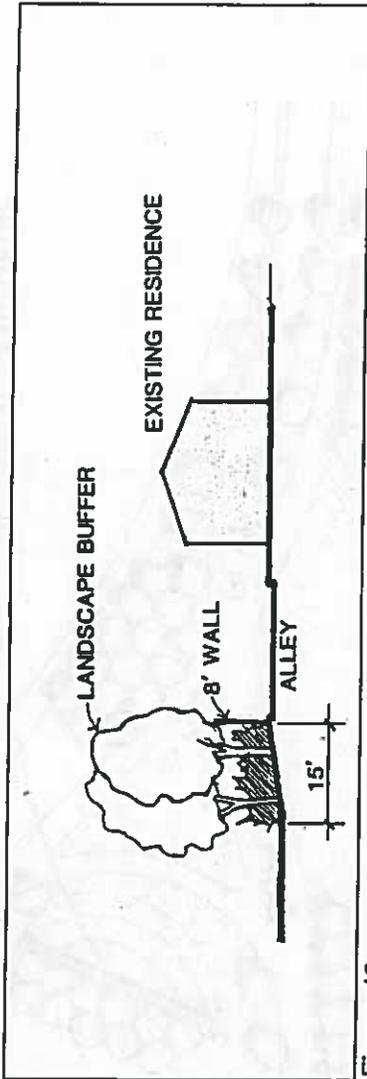


Figure 13

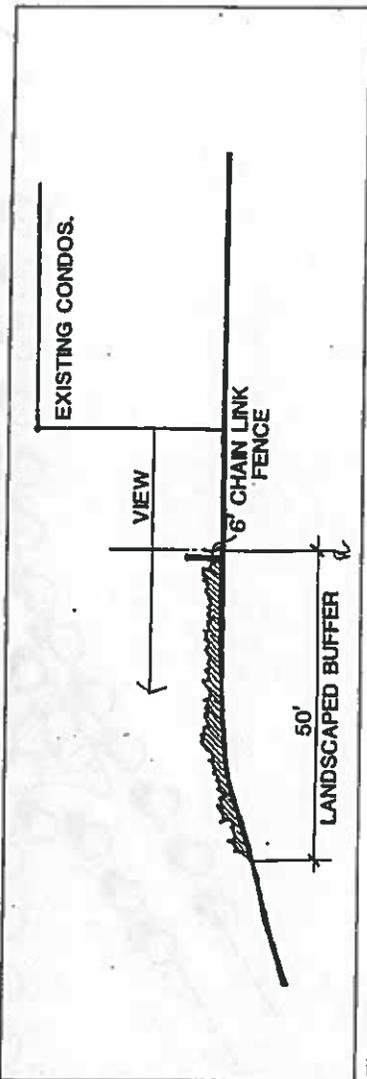
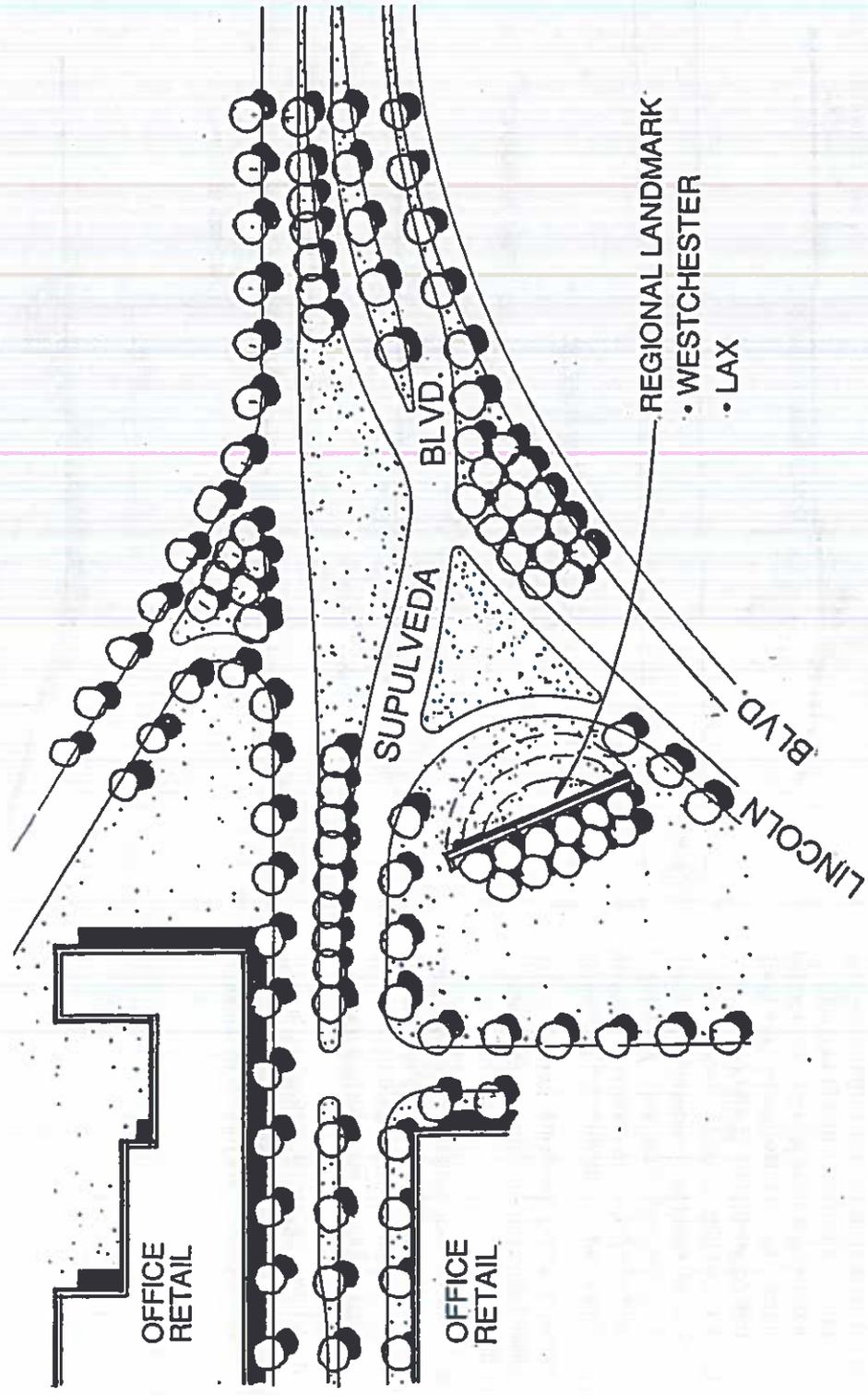


Figure 14



Regional Gateway at Sepulveda

Gateways

The gateways into LAX Northside are located at: 1) Lot 10 Gateway @ Sepulveda Boulevard and Lincoln Boulevard, 2) La Tijera Boulevard; 3) Westchester Parkway @ Sepulveda Westway; and 4) Westchester Parkway @ Pershing Drive.

General Guidelines

1. Each gateway will include project identification and directional signing. See section 6.0 for general and specific signing guidelines.

Westchester Center Guidelines

1. Lot 10 Gateway at Sepulveda Blvd and Lincoln Boulevard
 - a) The entry trees shall be Coral Tree.
 - b) The sign wall, as shown in the signing guidelines section, shall be a large curvilinear wall which is integrated into the landscape berm.
 - c) The sign wall shall be visible from vehicles traveling north on Sepulveda Boulevard and Lincoln Boulevard
 - d) See Figure 16 for plan.
2. La Tijera Gateway:
 - a) The gateway shall be located on the island at the intersection of 88th Street and La Tijera Boulevard.
 - b) Coral Trees shall be planted in a grove behind the sign wall.

- c) The sign wall is low, horizontal and is integrated into a landscape berm.
 - d) See Figure 15 for plan.
3. Westchester Parkway at Sepulveda Westway:
 - a) The sign walls shall be located on the northwest and southwest corners of the intersection.
 - b) The planting in front of the sign wall shall be a flowering ground cover. A hedge shall be planted behind the wall.
 - c) Colored concrete paving shall be placed at the crosswalks on all four streets and extends further into Westchester Parkway on the west side.
 - d) See Figure 17 for plan.

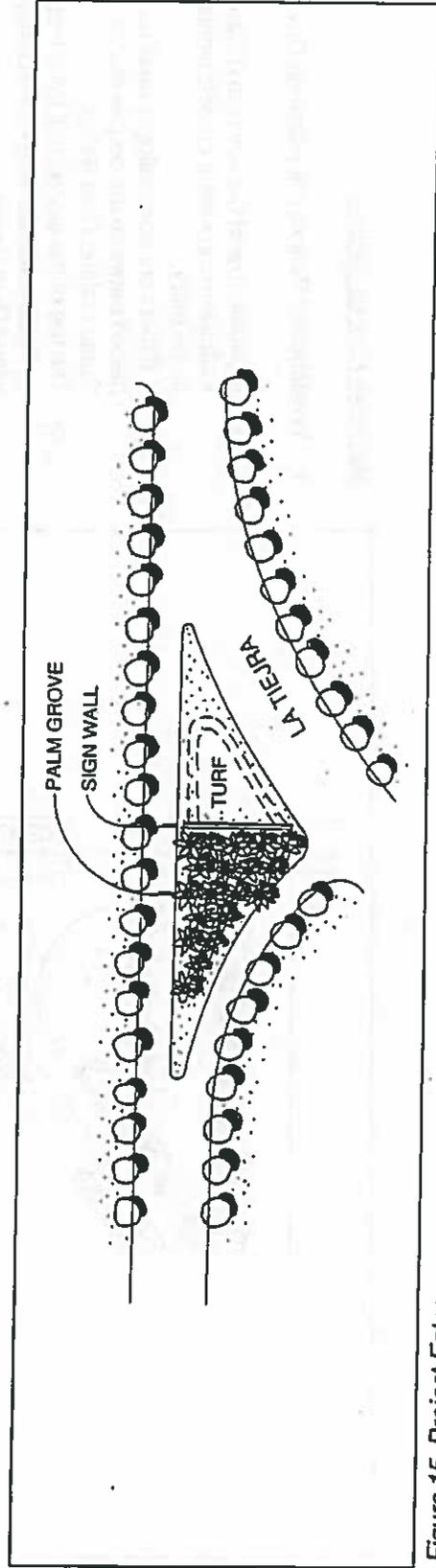


Figure 15 Project Entry

Business Park Guidelines

1. Westchester Parkway at Pershing Drive:

- a) A double row of Canary Island Date Palms forming a semi-circle is located at the entry.
- b) A 6 foot concrete walkway shall be placed between the double row of Canary Island Date Palms.
- c) The toe of the slope shall fall 5 feet behind the outside ring of the Canary Island Date Palms.
- d) The planting area inside the semi-circle shall be turf.
- e) The crosswalk shall be colored concrete paving, and shall extend up to the semi-circle area of palms.
- f) The signing shall be located per signing guidelines, Section 9.0
- g) See Figure 18.

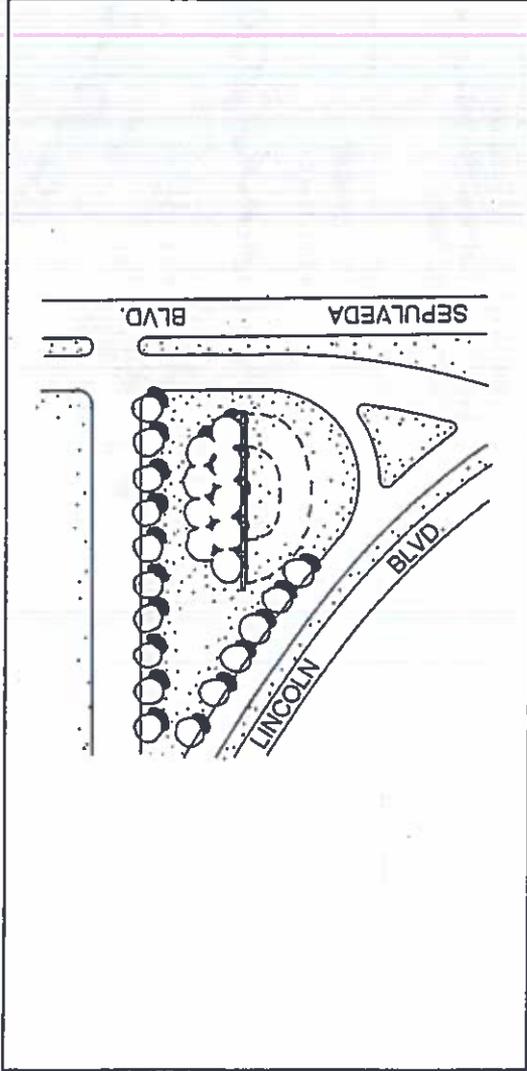


Figure 16 Project Entry

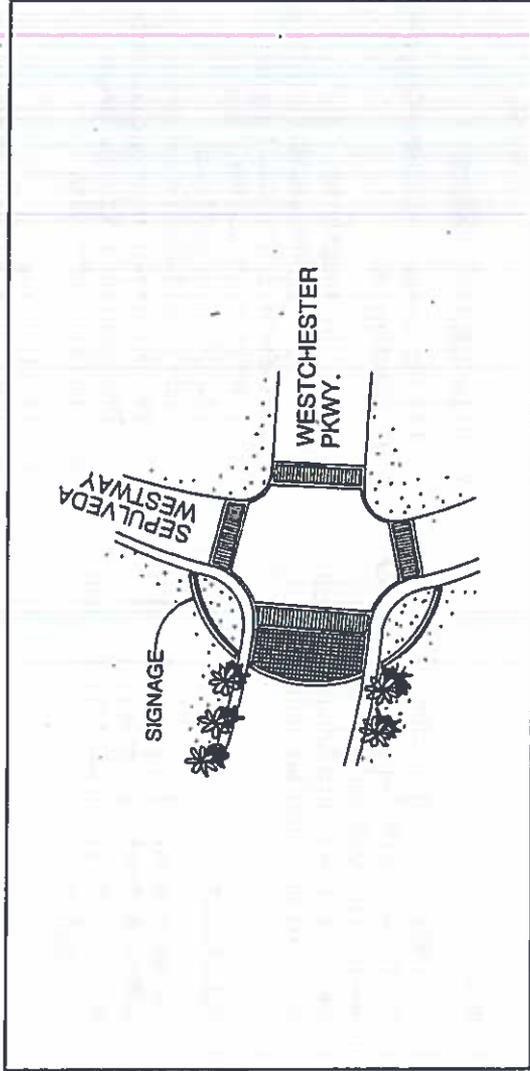


Figure 17 Project Entry Intersection

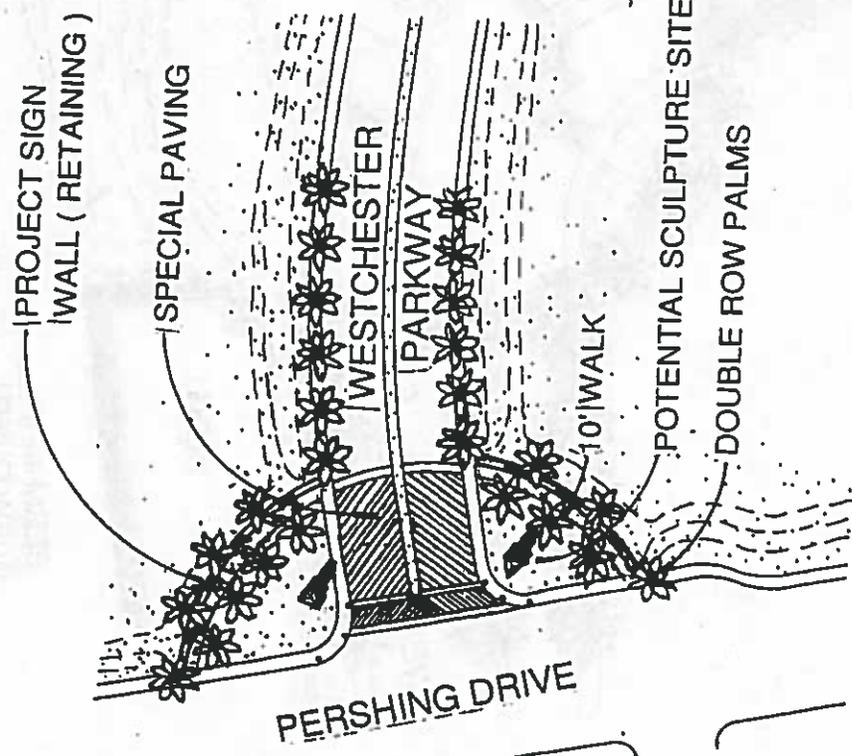
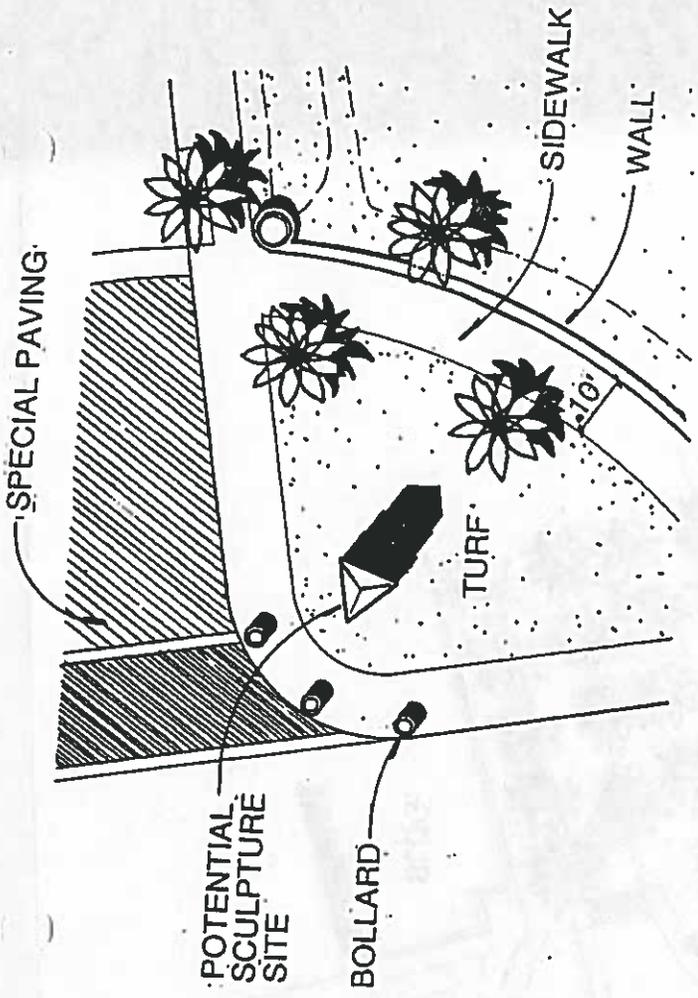


Figure 18 Entry at Pershing Drive

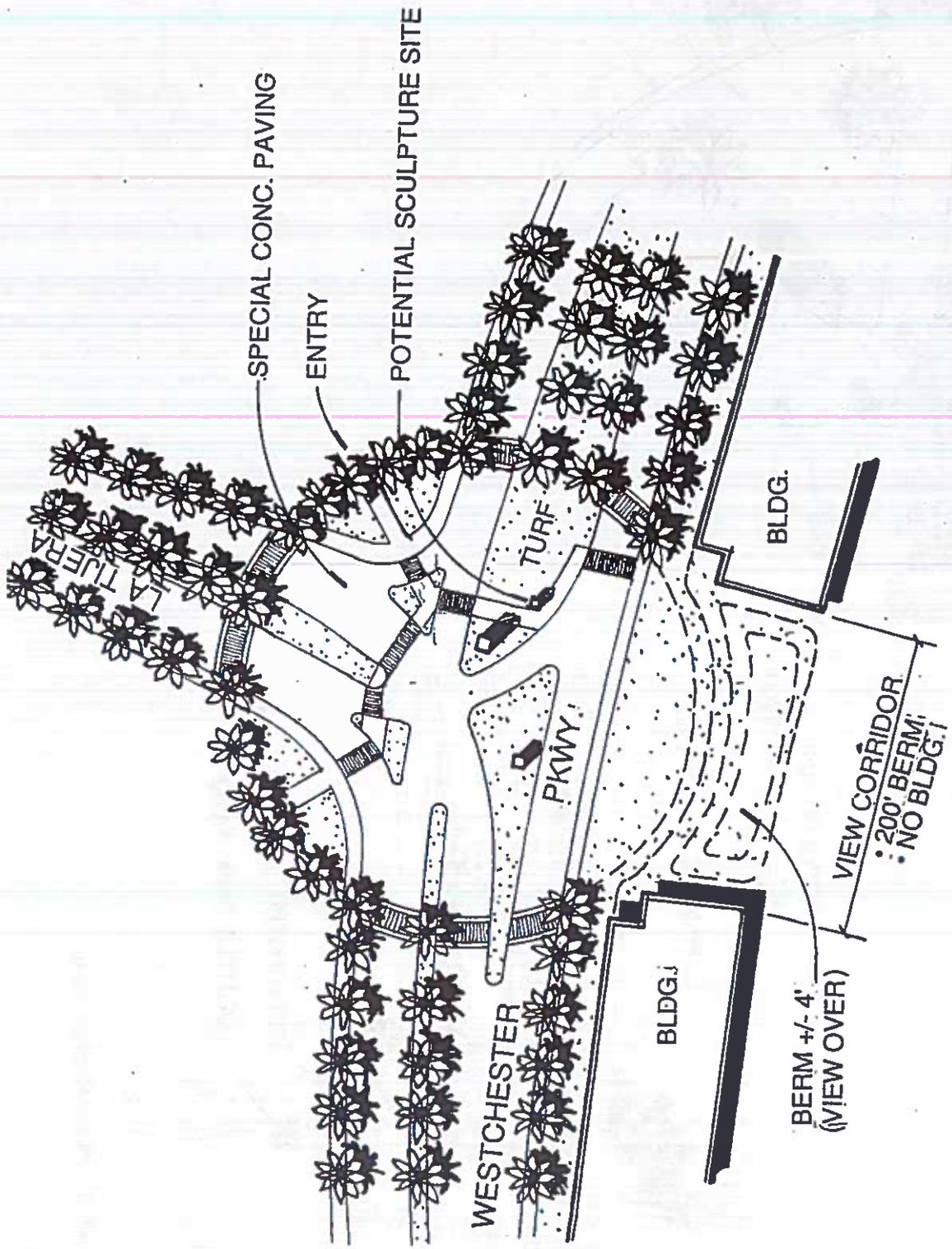


Figure 19 Urban Node

Primary and Secondary Intersection

The primary and secondary intersections are nodes of activity and vehicle arrival points. The primary intersections are Westchester Parkway and La Tijera Boulevard, and Westchester Parkway and the overpass at Lincoln Boulevard. The secondary intersections include the major parcel entries, as well as the intersections of Westchester Parkway at Loyola Boulevard and Falmouth Avenue.

General Guidelines

1. All colored concrete paving in roadway shall meet the Los Angeles Public Works Highway Standards.
2. The color of the concrete shall be L.M. Scofield C-11 Desert Sand Chromix add mixture or equal. The finish shall be a heavy broom finish.
3. All colored concrete shall have an 18" natural concrete band as a separation from asphalt paving.

Westchester Center Guidelines

1. La Tijera and Westchester Parkway intersection:
 - a) A single row of Canary Island Date Palms shall ring the space. For a focal point a sculpture or identification landmark shall be placed in the median.
 - b) See Figure 19 for plan view.
2. Westchester Parkway bridge and Lincoln Boulevard:
 - a) A double circular row of Canary Island Date Palms shall be placed surrounding the intersection.
 - b) The grading shall conform to the ring of palms.
 - c) See Figure 20 for plan and perspective views.

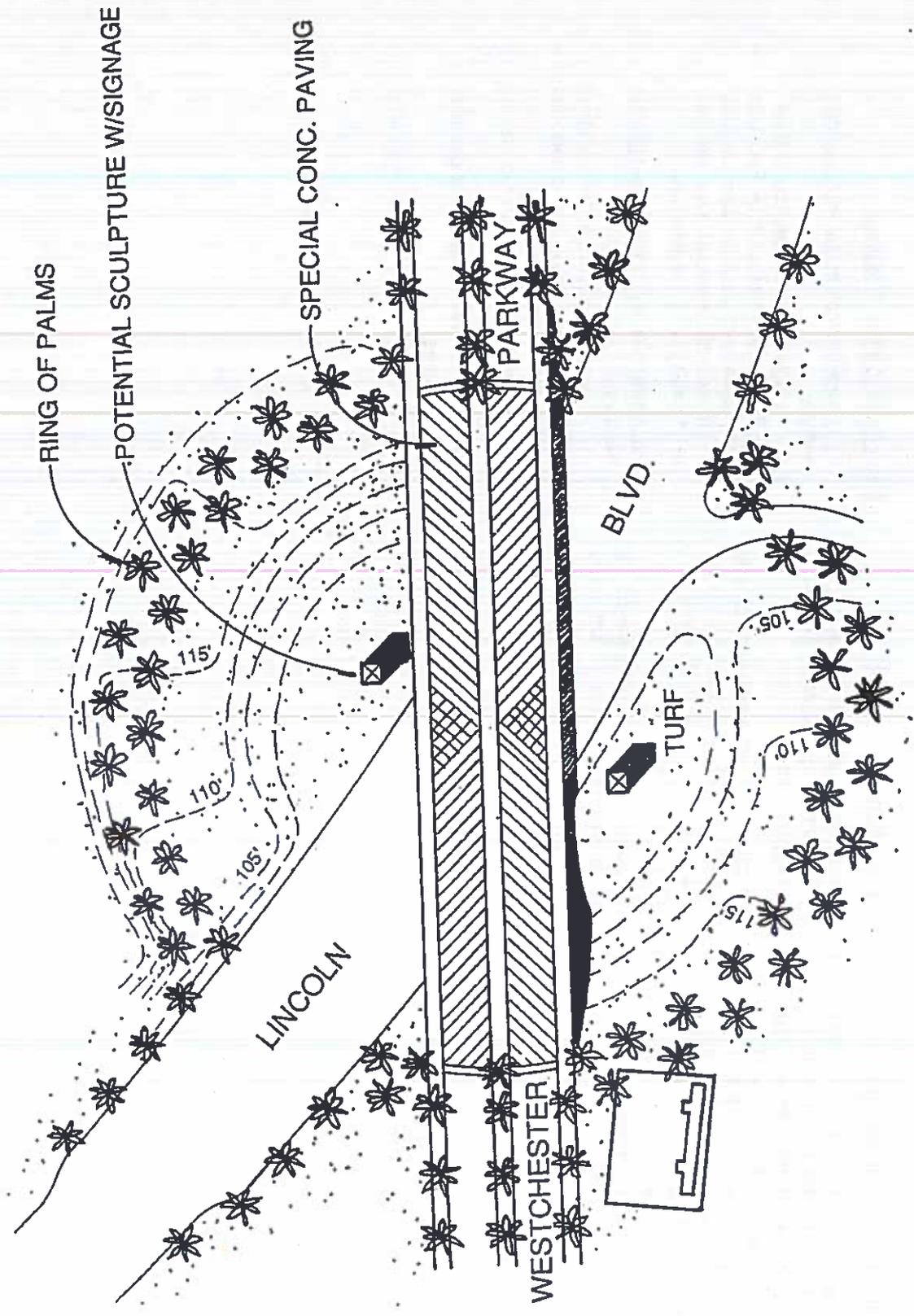


Figure 20 Bridge

Business Park Guidelines

1. Major parcel entries:
 - a) The parcel entries shall have a landscape median planted with a low flowering ground cover.
 - b) The entry sign shall be placed in the center of the median. The height should allow visibility over the sign to oncoming traffic.
2. Westchester Parkway at Loyola Boulevard and Falmouth Avenue:
 - a) The crosswalks shall be colored concrete paving.
 - b) The crosswalk shall be colored concrete paving.
 - c) Handicapped ramps at corners shall be required. See Figure 23 for plan and sectional views.
 - d) See Figure 21 & 22, plan view.

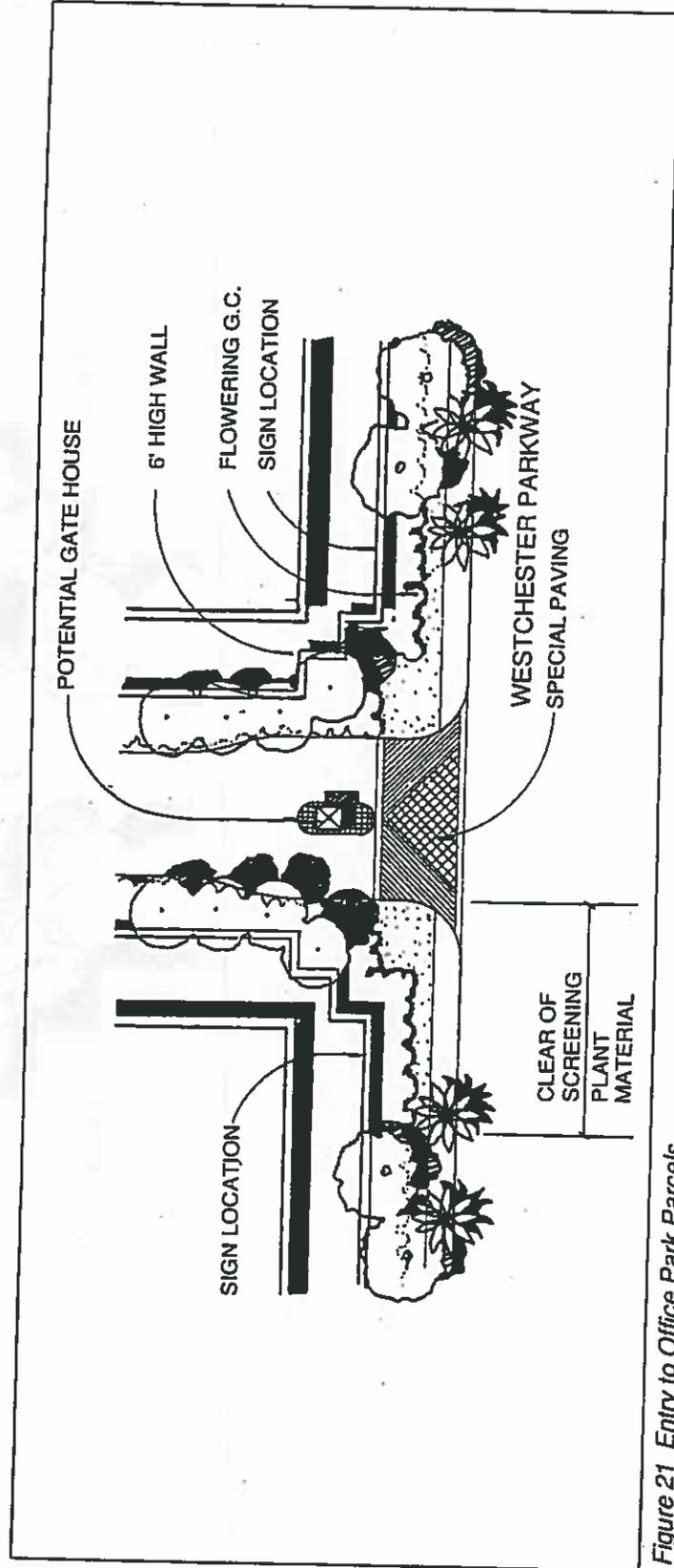


Figure 21 Entry to Office Park Parcels

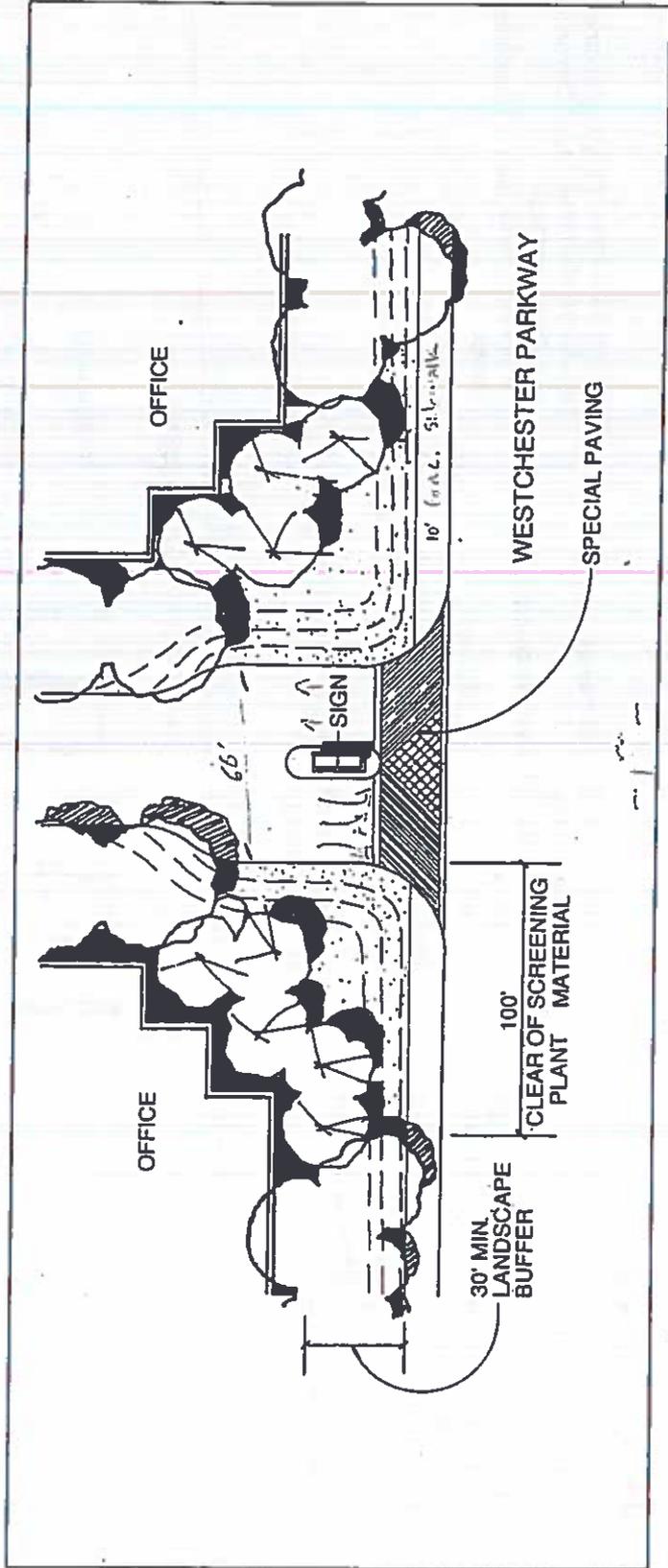


Figure 22 Entry to Airport Related Parcels

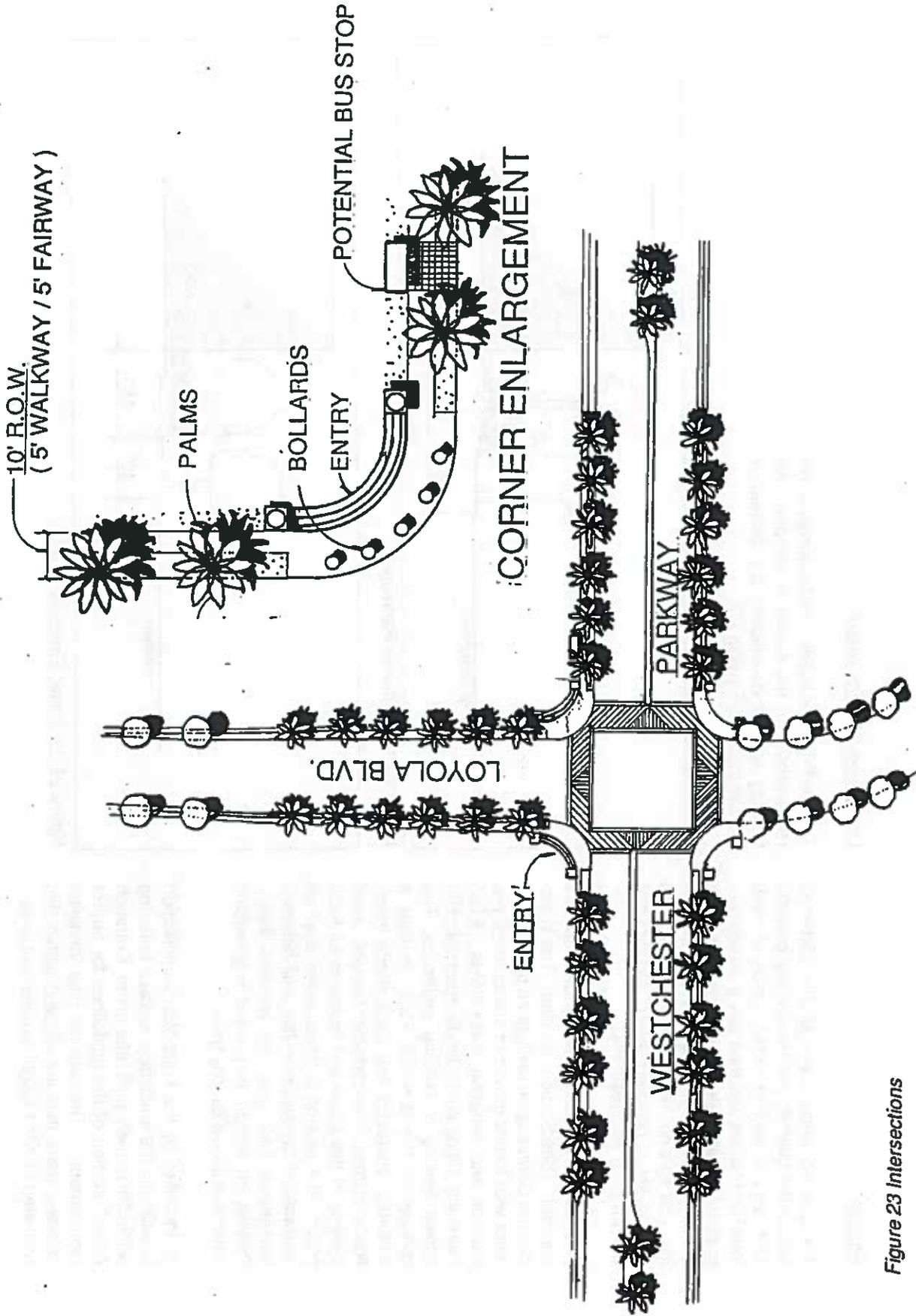


Figure 23 Intersections

Parcels

The parcels encompass all site elements except the building within individual parcels. This area is subject to the needs of each individual tenant and thus has a greater degree of flexibility. Elements within the parcels include: parking lot arrangements, landscaping, site lighting, regulatory and directional signing, service area screening, and side and rear property line treatment.

The parcels reflect the overall development concept. (See Section for plan) The *Westchester Center* zone exhibits an urban character with formal groves of shade trees, pedestrian and vehicular plaza areas, public fountains, sitting areas, landscaped pots and accent lighting at building entrances. The concept for the *Business Park* exhibits a suburban character with large shade trees, flowering trees, large massing of shrubs, broad sweeps of turf areas and fountains as focal points. The parking in these zones shall be orchard-like and/or grove-like infill between hedgerows (side and rear property lines). Parking lots should be planted in geometric patterns with low canopy trees.

As identified by the Tract Map Qualification Conditions, the landscape setback buffering within the parcels shall refer to the "Common Areas" section of the guidelines for further requirements. The tree list also identifies selected trees that are allowed within this restricted FAA 7:1 height requirement area.

Landscape Setback Areas

The planting in the building setback area is the responsibility of the parcel developers. All building setback dimensions are defined in the site planning Section 2.7.

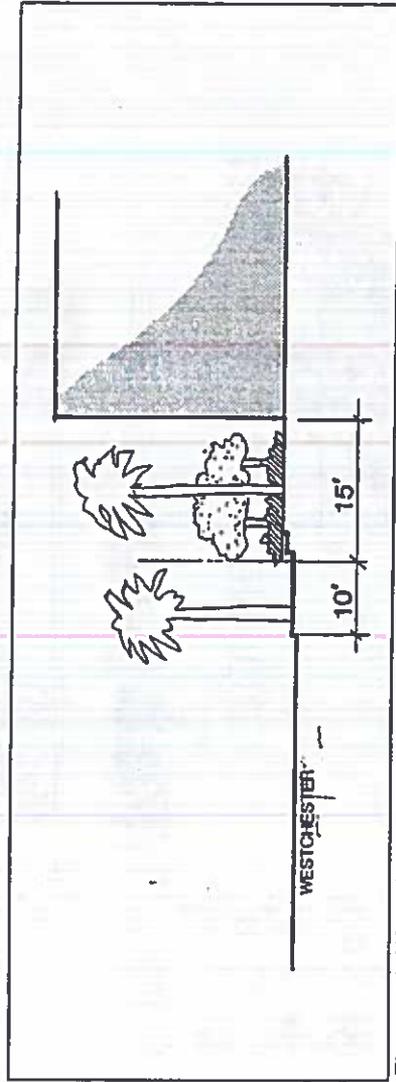


Figure 24 Westchester Parkway

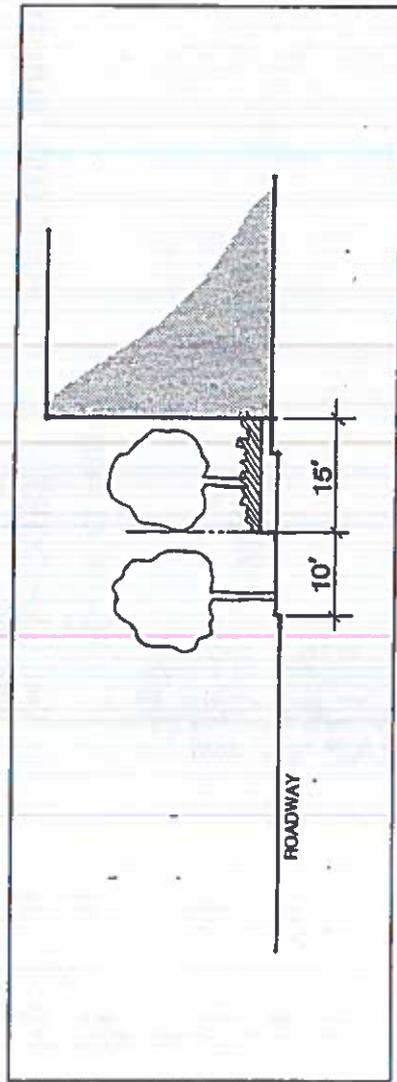


Figure 25 La Tijera, Emerson, Loyola

General Guidelines

1. The objective of the landscape setback is to reinforce the overall development area concepts.
2. All landscape elements shall be designed and coordinated with adjacent properties so that there is a smooth transition from one property to the next.
3. Transition from the public streetscape to the parcel landscape areas shall be coordinated with grades not exceeding 2:1 and continuous planting next to the property line.
4. Surface parking areas within the building setback zone shall be a minimum of 3 feet below the top elevation of berms.

Westchester Center Guidelines

1. Westchester Parkway, La Tijera Boulevard and Emerson Avenue.
 - a) The 15 foot building setback shall be primarily paved areas with raised planting areas for trees, shrubs and ground cover, special paving materials (ie. brick, concrete) and vertical changes where applicable (i.e., steps and walls).
 - b) A row of Queen Palms along Westchester Parkway shall be located five feet behind the property line and should align with palms in sidewalk.
 - c) See Figures 24 and 25 for sectional views.

Business Park Guidelines

1. Westchester The Business Park of Loyola Boulevard and Pershing Drive:
 - a) Within the 50 foot building setback a 30 foot landscape area shall be planted with informal accent massing of deciduous and flowering trees with a backdrop of evergreen trees which together create a "parklike" atmosphere.
 - b) The primary landscape cover shall be turf.
 - c) The berms shall be smooth, consistent and integrated with the surrounding landscape.

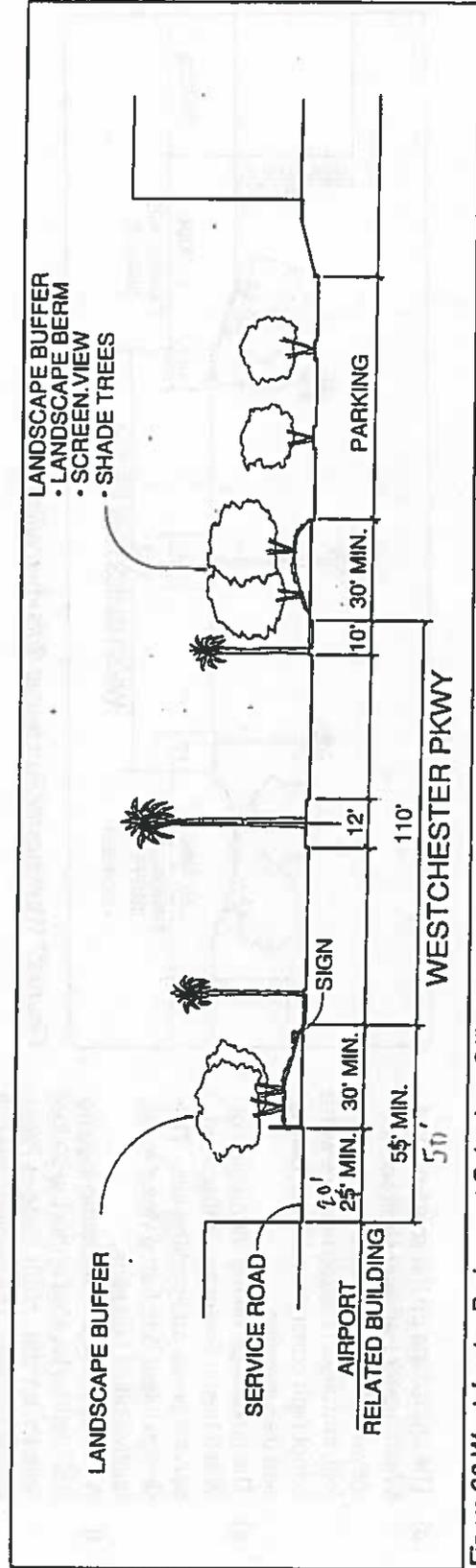


Figure 26 Westchester Parkway at Suburban Office

- d) The landscape on the south side of Westchester Parkway shall screen views such as service areas, parking lots, and airport related facilities with a 6 foot high concrete wall, turf berms and tree massing.
- e) The landscape on the north side of Westchester Parkway shall screen service areas and parking lots. The design intent is to frame views to distinctive office buildings.
- f) A four foot wide meandering jogging trail shall be located within the 50 foot setback on the north side of Westchester Parkway. The jogging trail shall be integrated with the turf berms.
- g) See Figures 26 and 27 for sectional views.

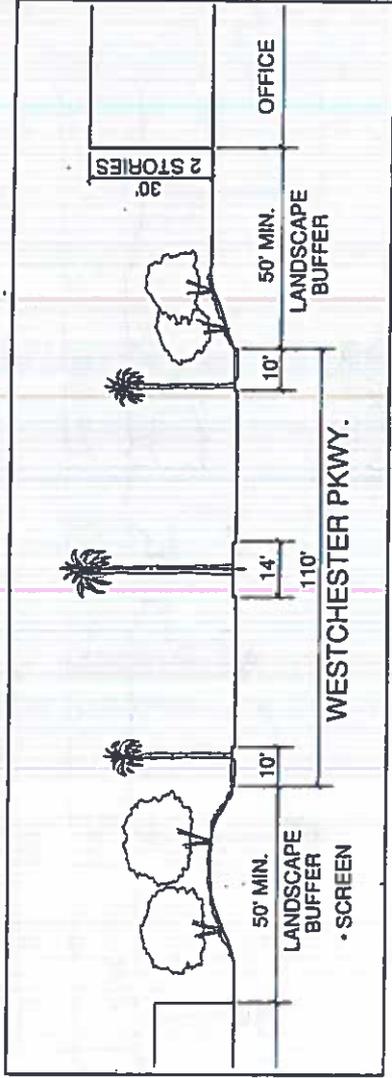


Figure 27 Westchester Parkway at Suburban office

- 2. Falmouth Avenue at The Business Park:
 - a) Within the 30 foot building setback the fifteen foot landscape setback shall be planted with evergreen trees, medium shrubs and ground cover on a 3' berm.
 - b) The objective is to screen parking and service areas.
 - c) See Figure 28 for sectional view.

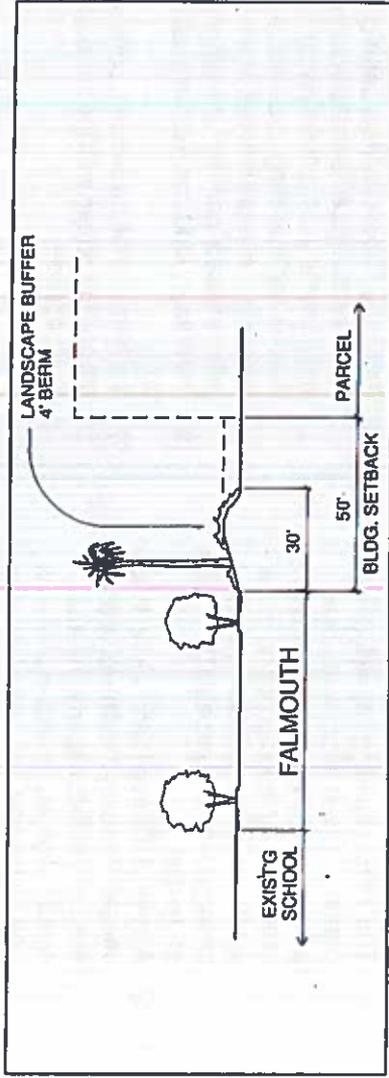


Figure 28 Landscape Buffer at Falmouth

6.3 Parking Lot Landscape

Requirements

Landscape guidelines for parking lots shall be used for the overall development.

General Guidelines

1. A minimum of one 24" box tree is required per each six parking stalls to be placed at the intersections and/or immediately adjacent to the stalls.
2. Planting between parking aisles shall have a minimum of a four foot planter area.

a) See Figure 29 for sectional view.

3. Tree wells and planter areas within paved parking areas shall be diamond shaped and provide a minimum 5 foot planting space.

a) See Figure 30 for sectional view.

4. All tree wells and planting aisles shall use curbs in lieu of wheel stops.

- 5) A single species tree is to be used for each parking compound, but the tree species may differ in separated parking compounds

a) Additionally a planting drainage test is recommended prior to selecting a tree species.

- 6) All parking structures shall be screened

with a 10 foot landscape buffer. The planting shall be a combination of vertical evergreen trees with medium size shrub planting at the base.

a) See Figure 31 for sectional view.

Side and Rear Yard Requirements

Landscape requirements for both side and rear yards shall apply to the overall development plan.

General Guidelines

1. A 5 foot minimum planting strip is to be provided continuously along and adjacent to all interior property lines so that where two properties adjoin, there shall be a 10 foot zone.

2. A maximum slope of 2:1 is allowed within the 5 foot planting zone. A one foot minimum flat transition area shall be provided at the top and bottom of all slopes within this zone.

3. No walls or fences exceeding 30 inches in height shall be permitted within the required 5 foot planting zone.

4. Either shrub or tree plantings as specified

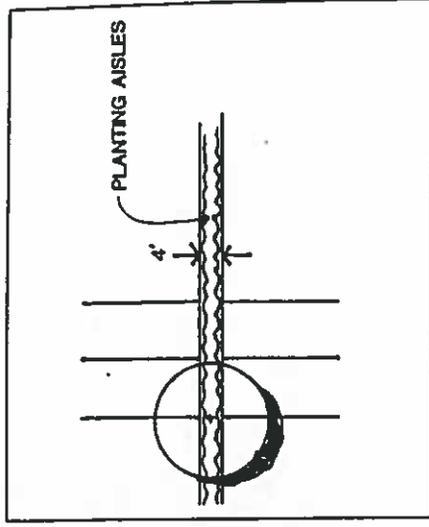


Figure 29 Container Planting

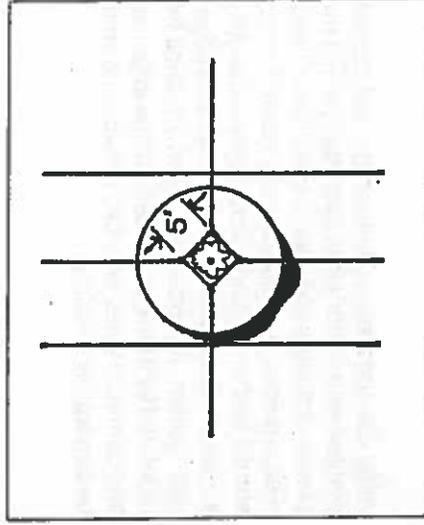


Figure 30 Planter & Wheel Stop

in the Plant Materials List shall be required in the 5 foot zone. Where two properties adjoin, shrub planting is required on one side of the property line and tree planting is required on the other. The first property developer to receive plan approval shall be required to plant the required trees. The second property developer then must plant the required shrubs.

5. Landscaping adjacent to a common access drive shall be coordinated so that both sides of the drive feature the same plant materials and include small to medium shrubs and trees on each side to screen adjacent parking. See parcel entry section of these guidelines.

Screening Utility Service Areas

General Guidelines

Screening of utility service areas shall apply to the overall development concept.

1. All utility service including meters, vaults, sprinkler risers, vacuum breakers, etc. and all service and trash areas shall be screened by a combination of walls and landscape materials including trees, shrubs, vines and groundcovers.

Planting Requirements

All landscape areas within each parcel shall be planted with materials in accordance with the planting palettes and criteria identified in these design guidelines including trees, ground cover, shrubs and vines, and such areas shall be fully irrigated with an automatic underground irrigation system, the operation of which shall be regulated by a time clock. Landscape irrigation and domestic water services to each site shall be separately metered.

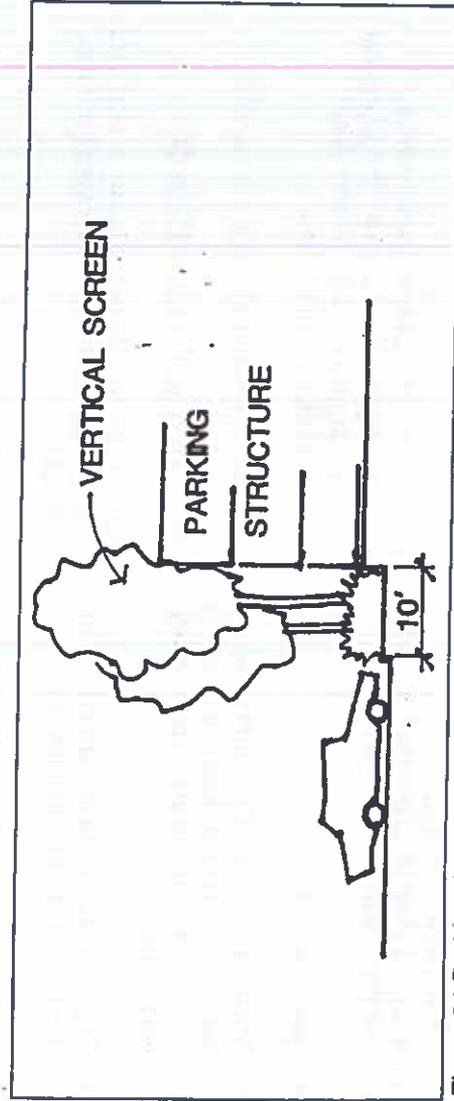


Figure 31 Parking Structure Screening

Site Maintenance

These maintenance guidelines shall apply to individual parcel developers.

1. All areas of each lot not used for structures, walkway, paved driveways, parking or storage areas shall be at all times maintained by a professional landscape engineer or gardener in a fully and well-kept landscaped condition and according to maintenance specifications to be provided by the lessee.
2. The lessee of each lot shall at all times properly maintain and keep the entire premises in a safe, clean and sightly condition, in a good state of repair, and shall comply in all respects with all governmental, health, fire and police requirements and regulations.

Implementation

The following matrix recommends responsibility for implementing the landscape elements described in these guidelines.

3. The lessee of each lot shall, at his own expense, remove any rubbish of any character whatsoever which may accumulate on such lot.
4. Each lessee shall be responsible for the cleaning, maintenance and retamping of any external lighting fixtures excluding those street lights included in the Public R.O.W. and Common Areas.
5. Undeveloped lots shall be maintained in a weed-free condition.

1. Interior Zone Tree Selection List

A. Building Setback Landscape

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
<i>Arecastrum romanzoffianum</i>	Queen Palm	20' Trunk
<i>Phoenix dactylifera</i>	Date Palm	20' Trunk
<i>Phoenix canariensis</i>	Canary Island Date Palm	20' Trunk
<i>Erythrina calfra</i>	Kaifirboom Coral Tree	48" box
<i>Erythrina humeana</i>	Natal Coral Tree	48" Box
<i>Archontophoenix cunninghamiana</i>	King Plam	20' Trunk

B. Parking Lot

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
<i>Erythrina calfra</i>	Kaifirboom Coral Tree	48" Box
<i>Erythrina humeana</i>	Natal Coral Tree	48" Box
<i>Ficus microcarpa</i>	Indian Laurel Fig	24" Box
<i>Cupaniopsis anacardioides</i>	Carrot Wood	24" Box
<i>Magnolia grandiflora</i>	Southern Magnolia	24" Box

C. Rear and Side Yard

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
<i>Arecastrum romanzoffianum</i>	Queen Palm	20' Trunk
<i>Phoenix dactylifera</i>	Date Plam	20' Trunk
<i>Phoenix canariensis</i>	Canary Island Date Plam	20' Trunk
<i>Erythrina calfra</i>	Kaifirboom Coral Tree	48" Box
<i>Erythrina humeana</i>	Natal Coral Tree	48" Box
<i>Ficus microcarpa</i>	Indian Laurel Fig	24" Box
<i>Magnolia grandiflora</i>	Southern Magnolia	24" Box
<i>Metrosideros excelsus</i>	New Zealand Christmas Tree	24" Box
<i>Cupaniopsis anacardioides</i>	Carrot Wood	24" Box

D. Building Entries

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
<i>Arecastrum romanzoffianum</i>	Queen Palm	20' Trunk
<i>Phoenix canariensis</i>	Canary Island Date Palm	20' Trunk
<i>Phoenix dactylifera</i>	Date Palm	20' Trunk
<i>Erythrina caffra</i>	Kaffirboom Coral Tree	48" Box
<i>Erythrina humeana</i>	Natal Coral Tree	48 Box
<i>Ficus microcarpa</i>	Indian Laurel Fig	24" Box
<i>Archontio phoenix "Cunninghamiana"</i>	King Palm	20' Trunk

2. Parcel Shrubs and Small Trees (Minimum 5 gallon size)

<u>Species</u>	<u>Common Name</u>
<i>Magnolia soulangiana</i>	Saucer Magnolia
<i>Melaleuca nesophila</i>	Pink Melaleuca
<i>Comarostaphyus diversifolia</i>	Summer Holly
<i>Eriobotrya japonica</i>	Loquat
<i>Punica granatum</i>	Pomegranate
<i>Nerium oleander</i>	Oleander
<i>Rhapiolepis indica</i>	Indian Hawthorn
<i>Abelia grandiflora</i>	Glossy Abelia
<i>Xylosma congestum</i>	Xylosma
<i>Rhododendron Sp.</i>	Azalea
<i>Pittosporum tobira "carpa"</i>	Wheeler's Dwarf - Tobira
<i>Carissa macrocarpa</i>	Natal Plum
<i>Escallonia exoniensis</i>	Dwarf Escallonia

4. Parcel Ground Covers and Turf

<u>Species</u>	<u>Common Name</u>	<u>Size</u>
Trachelosperum jasminoides	Star Jasmine	1 Gallon
Agapanthus africanus	Lilly of the Nile	1 Gallon
Hedera helix "Needle Point"	English Ivy	Flats
Liriope Muscari	Lilly Turf	1 Galon
Gazania Sp.	Gazania	Flats
Ophiopogon japonica	Mondo Grass	1 Gallon
Festuca eleitor	Tall Fescue	Seed/sod
Cynadon Sp.	Bermuda Grass	Seed/sod
Stenotaphrum secundatum	St. Augustine Grass	Seed/sod



7.0 Lighting Guidelines

The positive nighttime image of LAX Northside is important because it conveys a safe, secure, well designed, and organized development area. Special lighting of areas such as key intersections, transit stops and public plazas will greatly enhance the aesthetic character of the development area. The use of special lighting will be accomplished without impacting the surrounding neighborhoods or airport operations.

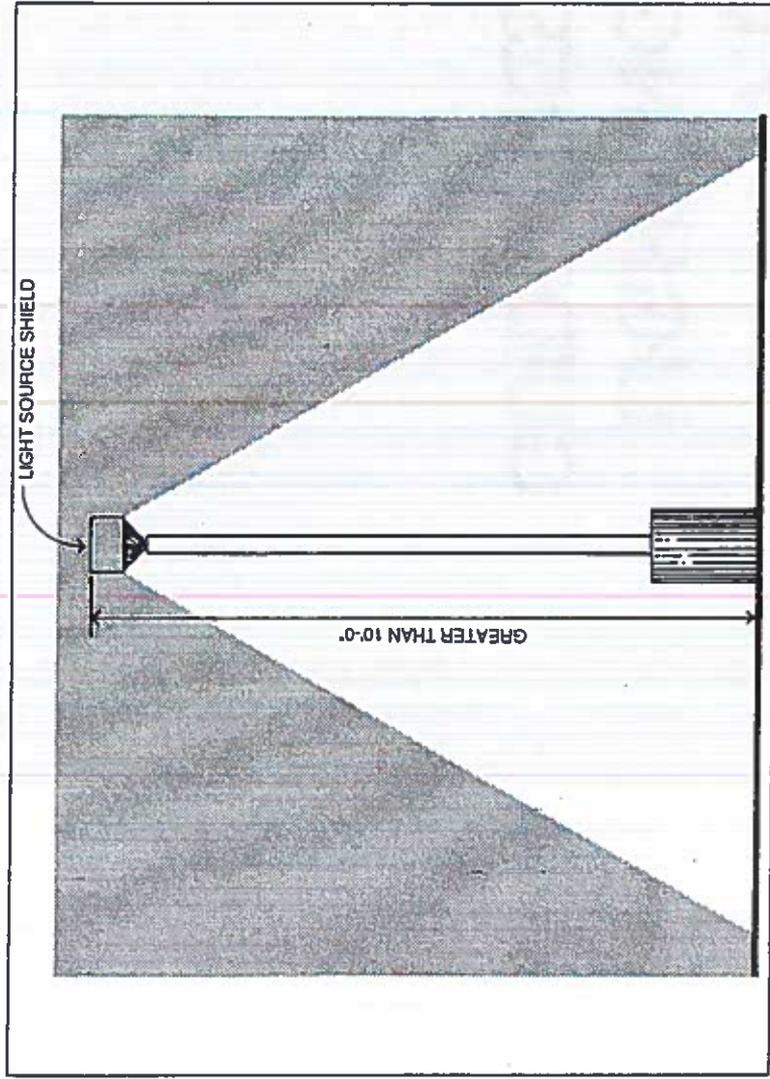
7.1 Parcel Site Lighting

Objectives

- To have on-site lighting contribute to the safe and efficient use of a development site.
- To have on-site lighting contribute to site security.
- To have on-site lighting complement and reinforce the architectural and site design character.
- To have on-site parking lot lighting fixtures and illumination levels be consistent throughout LAX Northside development.
- To prevent on-site lighting from casting glare onto adjacent streets and property.
- To encourage lighting design that is in conformance with energy saving guidelines.

General Guidelines

1. Cut-off Lighting
 - a) All lighting potentially visible from an adjacent street except bollard or pole up to 10 feet in height shall be indirect or shall incorporate a full cut-off shield type fixture.
2. Parking Lot
 - a) Parking lot lighting shall include: parking areas, access drives, and internal vehicular circulation areas.



Cut-Off Shield Lighting

- b) As a minimum, site lighting on all lots shall provide side and rear property line pole lighting mounted on a three foot cylindrical concrete base with a fixture height sufficient to provide the minimum standard site lighting set forth herein. The concrete base finish shall be L.M. Scofield Co. T-21SD fractured finish grooved or equivalent.
- c) All such lighting fixtures shall be Kim SBC (Square Beam Cut-Off) or equivalent. The light source shall be a 150 watt color corrected high pressure sodium lamp.
- d) The parking lot illumination level shall achieve a uniformity ratio of 3 to 1 (average to minimum) with a maintained average of 1 foot candle and a minimum of .3 foot candle.

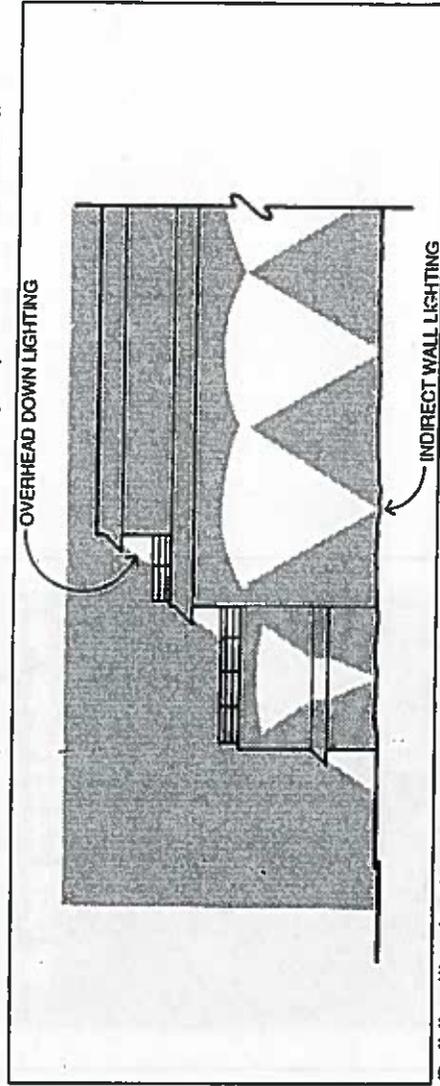
3. Service Area

- a) Service area lighting shall be contained within the service yard boundaries and enclosure walls. No light spillover should occur outside the service area. The light source should not be visible from the street.

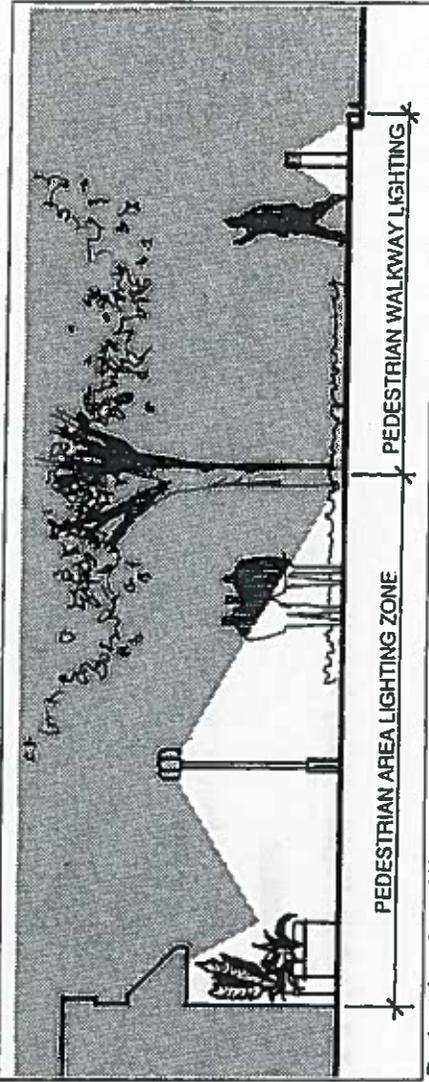
4. Building Illumination

- a) Building illumination and architectural lighting shall be indirect in character (no light source visible). Indirect wall lighting or "wall washing", overhead down lighting, or interior illumination

which spills outside is encouraged. Architectural lighting should articulate and animate the particular building design as well as provide the required functional lighting for safety and clarity of pedestrian movement.



Building Illumination



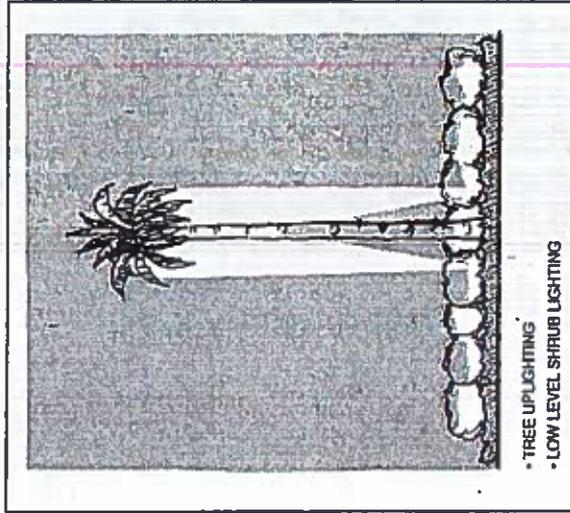
Pedestrian Area Walkway Lighting

5. Pedestrian Area Walkway
 - a) Pedestrian walk and area lighting is segmented into two zones. The first zone is pedestrian area lighting. This includes outdoor pedestrian use areas such as courtyard, entryway, etc. Pedestrian area lighting should achieve a uniformity ratio of 3.5 to 1 average to minimum, with an average illumination of .60 foot candles and a minimum of .18 foot candles. The second zone is pedestrian walk lighting where point to point lighting is acceptable with no specific illumination levels required. The main emphasis in this zone should be to clearly identify the pedestrian walkway and direction of travel. These fixtures shall be of a consistent design within the specific project. Recommended fixtures for the two zones are Bega Series 9900 and 95903, respectively.

7.2 Public Right of Way Lighting

General Guidelines

All streets shall conform to the City of Los Angeles street lighting standards.



Landscape Accent Lighting

6. Landscape Accent Lighting

Landscape accent lighting can be provided at particular accent or focal point with up lighting of trees. All accent lighting shall be directed away from vehicular traffic.

8.0 STREET FURNITURE



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8.0 Street Furniture

Street furniture elements include bench seating, bollards, planters, and trash receptacles located in public rights-of-way at locations such as transit stations, bus shelters, street intersections, and public plazas where high numbers of pedestrians commonly congregate. The street furniture shall be a unified system which reinforces a common design theme through standard color, materials, form, and high quality.

Objectives

- To provide street furniture that is functional, durable, and easily maintained.
- To provide a coordinated and consistent high quality design theme which supports the visual image of *LAX Northside*.
- To implement street furniture which provides access and ease of use to handicapped persons.
- To provide amenities required to promote safe, visually pleasing, comfortable pedestrian environments.

General Guidelines

1. Bench Seating

- a. Benches shall be located adjacent to walkways, with a maximum distance of 40' between each seating area. In addition, bench seating shall be located in appropriate quantities to respond to users needs at transit stations, bus shelters, street intersections, and public plazas.

with other street furniture such as benches, bollards, or trash receptacles. Planters should be located in high amenity zones where large concentrations of pedestrians are likely to frequent.

- b. All such planters shall be Ultrum UL-3000 series consistent in color with benches.

- b. All such benches shall be Ultrum metal series benches, or equivalent. Bench color shall be consistent with other street furniture.

- c. All planters shall have permanently installed irrigation systems. Plant material shall be replaced as necessary in order to maintain a high quality appearance.

2. Bollards

- a. Bollards shall be located at street intersections where they will be used to define the boundary between pedestrian and vehicular zones. Bollards may also be used to delineate pedestrian walkways.

4. Trash Receptacles

- a. Trash receptacles shall be located along walkways, near parcel entrances, seating areas, transit stops, public plazas, and other pedestrian gathering areas.

- b. All such bollards shall be Bega 95905 or equivalent with black finish and anchored with No. 896 system.

- b. All such receptacles shall be Ultrum UL-3000 series receptacles or equivalent, and consistent in color with benches.

3. Planters

- a. Planters may be used at public entrances to parcels or in conjunction

- c. All receptacles shall be located for ease of pick-up/trash removal.

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9.0 SIGNING AND GRAPHICS

9.0 Signing and Graphics

The signing standards contained in these guidelines will be used to regulate all signs in the Project. Any signing in the Project must comply with these guidelines in order to receive approval by the Department of Airports, which must approve all signing within airport property. Details of the submittal process are outlined in Section 8.6. In addition to conforming with these guidelines, all signs must comply with any codes, regulations and/or ordinances already imposed on this area. Permits must also be obtained through the usual civic channels.

These guidelines have several basic purposes:

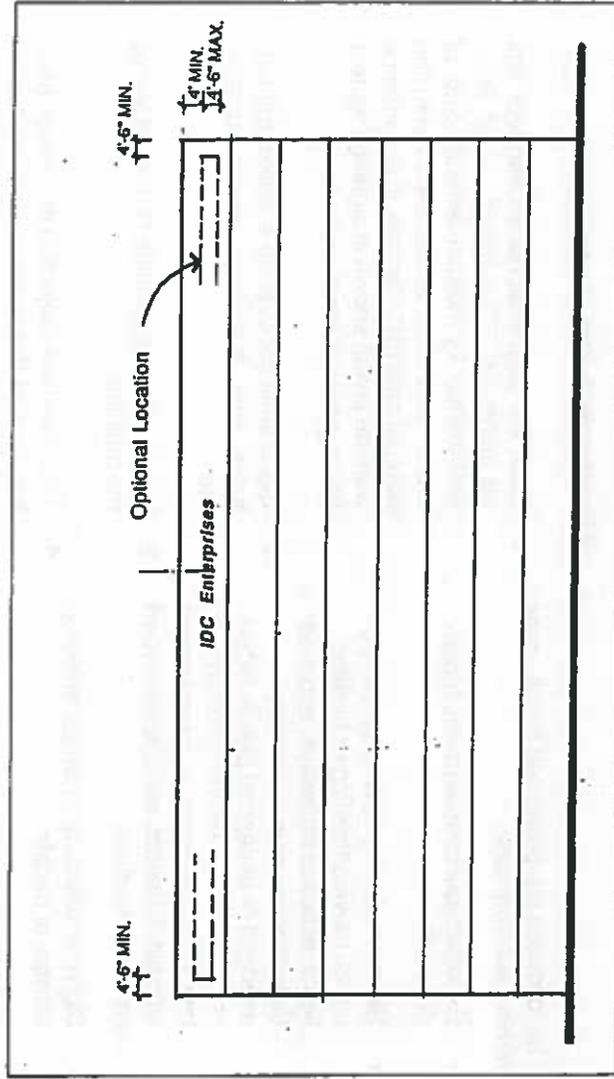
- To create an image and identity for the project through signing that is consistent, compatible and harmonious.
- To establish a high level of aesthetic design quality and a method of reviewing signs for conformance with that quality.
- To reinforce the park-like atmosphere of the project and create a feeling of community pride within the project.
- To create a positive contribution to the environment.
- To minimize delays in the design process
- To create a sense of order for all project signing elements.

These guidelines are divided into four sections based on the area of application or, in the case of retail guidelines, the type of business.

In addition to general guidelines, *Westchester Center Guidelines* and *Business Park Guidelines* this section also includes Retail Guidelines which apply to restaurants, hotels, markets, or any other project support businesses whom the DOA deems qualifies for these specific guidelines.

9.1 Tenant Identification Signs

Identification (I.D.) as is, refers to, and shall be limited to, tenant's trade name only. Tenant's customary signature or logo, hallmark, insignia, or other trade identification may be permitted at the DOA's sole discretion.



Tenant Identification Sign Location

General Guidelines

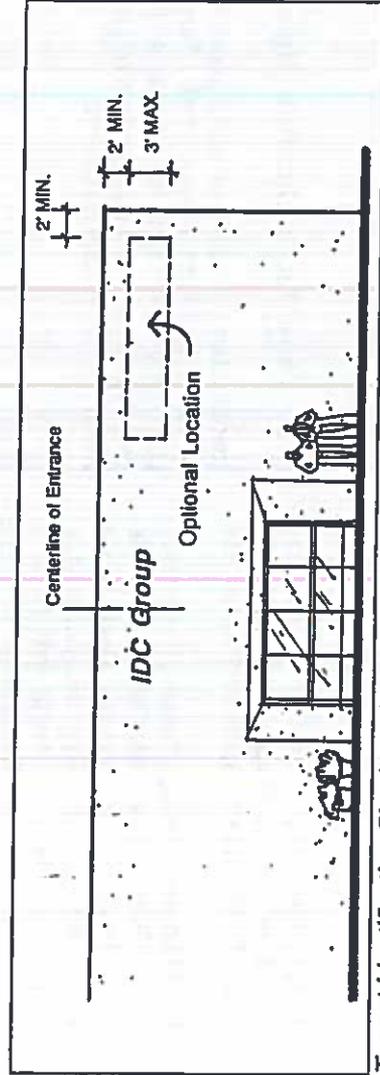
1. Tenant shall be permitted to install two (2) illuminated identification signs on two separate elevations of the building.
2. No signing shall be visible from the immediately adjacent Westchester residential areas.
3. Illumination brightness of signs is subject to approval by the DOA and must comply with all applicable building and electrical codes. Surface brightness of all translucent materials shall be consistent in all components of sign. Light leaks will not be permitted. All conduits, lamps or transformers shall be entirely concealed within the sign and not visible from the surface.
4. Tenant shall be responsible for the cost of signs fabricated and/or installed which do not conform to these criteria/guidelines, and the cost of their removal.

5. Suggested sign styles for tenant identification signs:
 - Individual, self-contained, internally illuminated letter forms.
 - Individual reverse channel, neon illuminated letterforms (halo effect).

6. The following are prohibited for all areas within LAX Northside.
 - Exposed light source (neon, incandescent).
 - Signs employing animated components, moving/flashing or blinking lights, exposed raceways, exposed ballast boxes or transformers, unedged or uncapped plastic letters or letters with no returns and exposed fastenings, luminous-vacuum formed type plastic letters, sandblasted wood type construction.
 - Sign manufacturer's names, stickers, stamps or decals.
 - Exposed fasteners.
 - Simulated materials (i.e., wood grained plastic laminate, etc.)

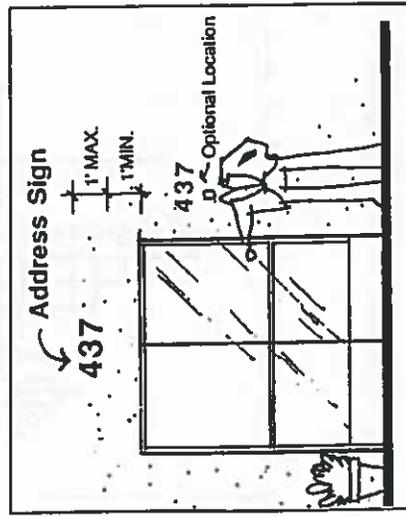
Westchester Center Guidelines

1. Each I.D. sign shall not exceed four feet six inches (4'- 6") in height, length to be determined by normal letter spacing of name, and must be installed within the parapet area of the facade, in accordance with the layout shown in signing Figure 1 (three possible locations shown)
2. Signs shall not overlap architectural features, such as mullions, window breaks, etc.
3. Signs will not be permitted to project above the building.
4. The maximum projection of the sign from the face of the building shall be twelve inches (12").



Business Park Guidelines

1. Each I.D. sign shall not exceed three feet (3') in height, length to be determined by normal letter-spacing of name, and must be installed within the parapet area of the facade either centered on the facade, or near the corner of the facade, as shown in Figure 2.
2. Signs shall not overlap any major architectural features, such as mullions, window breaks, etc.
3. Signs may not project above the building.
4. The maximum projection of the sign from the face of the building shall be eight inches (8").



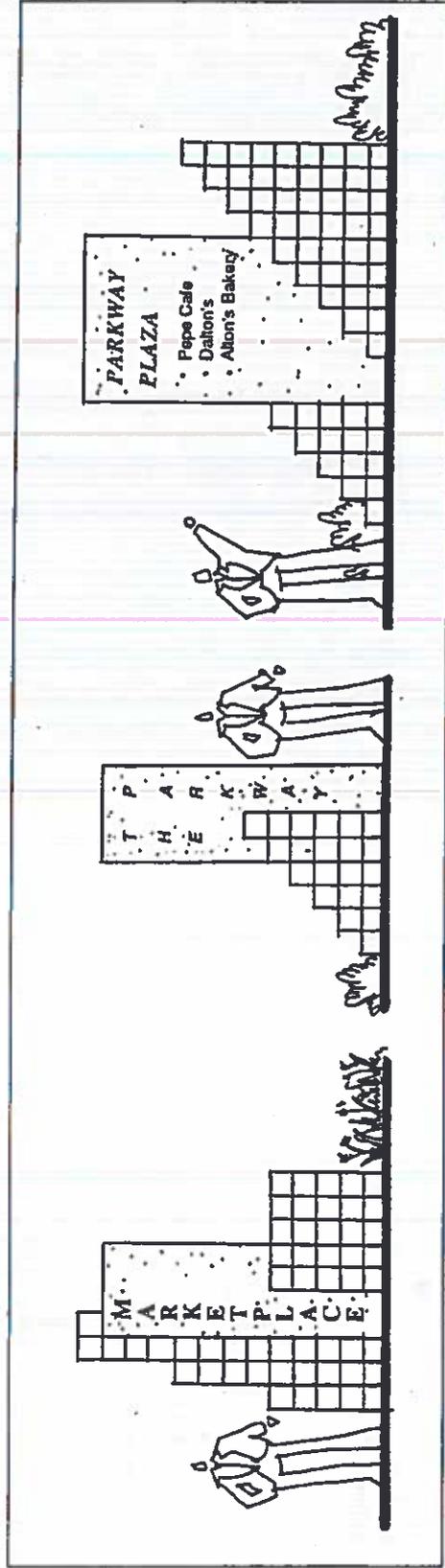
Tenant Address Sign

Retail Guidelines

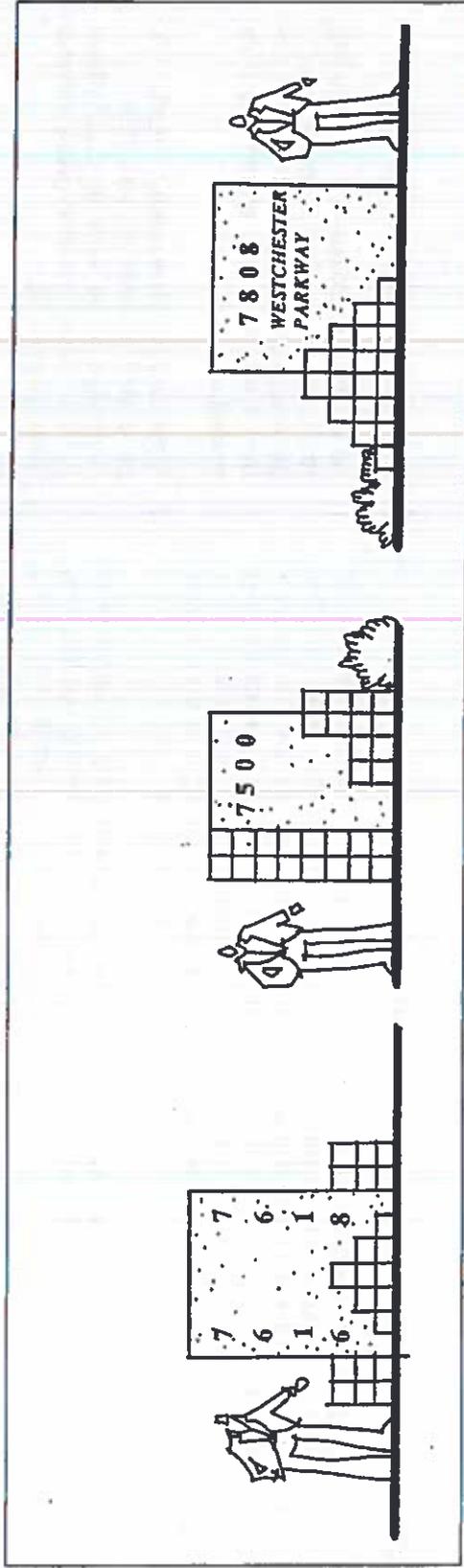
1. Tenant shall be permitted to install two (2) I.D. signs on facades of the building (maximum of 1 per facade can be illuminated), or if retail building is free standing, may choose to erect retail ground monument identification signs in any combination (maximum of 3).
2. If four or more retailers are attached together in a free standing building, they (together) shall be permitted to erect a project/complex ground monument sign.

9.2 Tenant Address Signs

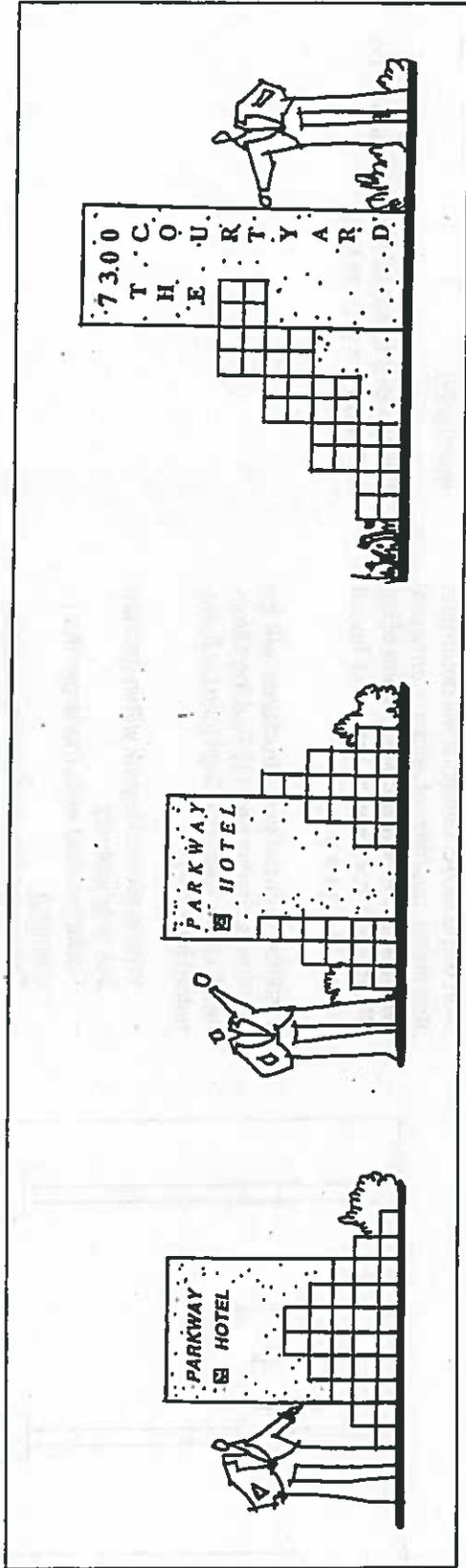
1. All tenants (regardless of area, including retail) will be required to install one (1) address sign (numbers only) not more than 1 foot in height, adjacent to main entrance.
2. Materials should be same as used in I.D. signs, and in same type style (font). Sign need not be illuminated, but should be highly visible for emergency vehicles as well as visitors.



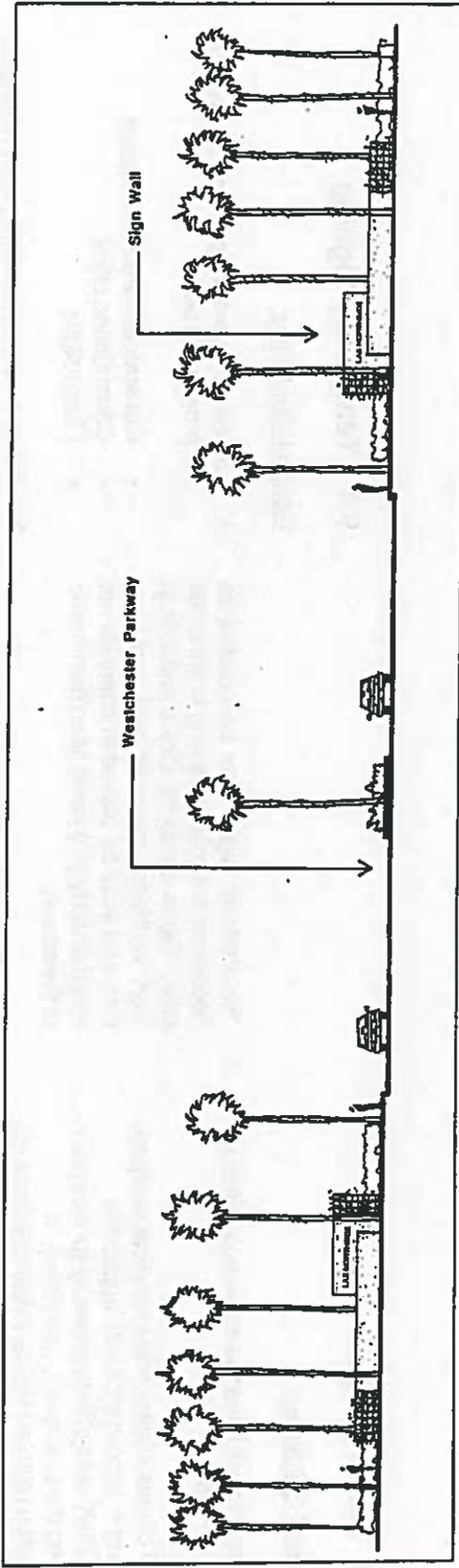
Retail Ground Monument Signs



Office Building Ground Monument Signs



Hotel Ground Monument Signs



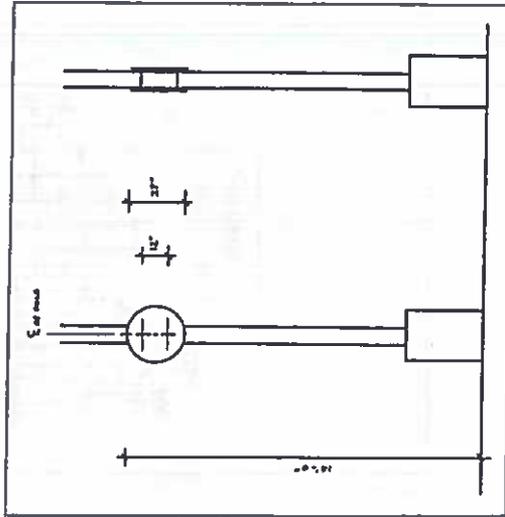
Entrance Sign Wall

9.3 Parking Signs

General Guidelines

All tenants will have the same guidelines for parking signs.

1. Tenants shall provide handicapped parking in accordance with applicable City/County/State codes by surface painting the pictogram, (provided), in each required space, to the size and location indicated.



Parking Area Designation Signs

9.4 Temporary Signing

General Guidelines

1. The only temporary signing allowed within the project will be:
 - Construction walls/barricades
 - Construction signs
 - Lease signs
2. The overall project name, *LAX Northside* shall be given equal mention, each time the specific development name is mentioned.
3. Lease signs attached to buildings shall be limited to the ground floor.

2. "No Parking" signs may be erected as necessary to prohibit parking in fire lanes only. Signs are to be City standards in size, verbiage, materials and construction, and shall be placed in intervals not less than forty (40) yards apart (same side of the street).

3. Area Designation Signs are to be used only in large surface lot area by permission of the DOA. Landlord will determine maximum quantity of signs permitted, according to size and configuration of lot. Signs shall conform in size and layout shown in Figure 8.

4. Tenants with parking structures will be required to install one (1) Parking Structure Exterior Entrance Sign (can be illuminated) indicating:
 - Who is allowed to park within (month-lies only/visitors)
 - Entrances and exits (no larger than opening)
 - Clearance information (immediately above entries)
 - Rates (if applicable)

Materials should be the same as used on I.D. signs.

9.5 Flags, Banners and Pennants

General Guidelines

1. A maximum of three (3) flag poles and flags will be allowed for each building. Flag poles should be no taller than 30 feet. In no case, however, should flag poles exceed FAA height restrictions. Flags should be no larger than 6' x 9'. Banners and pennants shall only be permitted in interior court areas and not visible from building exterior, or by permission of the DOA only.
2. Special occasion banners or pennants may be temporarily erected only with the Landlord's (DOA) permission, obtained prior to installation.

Submission (All Signs)

Tenant shall submit for approval three (3) complete sets of sign drawings to the DOA (Department of Airports), prior to sign fabrication and installation.

In addition to detailed sign drawings, such submission must include:

- A. Appropriate elevations of buildings showing design, location, size, message and layout of signs drawn to scale indicating dimensions, attachment devices and construction details.

9.6 Maintenance

General Guidelines

- B. Sample board showing colors and materials of signs and adjacent surfaces.
- C. Section through signs showing projection from face of building, indicating dimensions.
Submission will be reviewed and either approved as submitted, approved with conditions or not approved.
1. Every sign shall be maintained in a clean, safe and good working condition, including the replacement of defective parts, defaced or broken faces, lighting and other acts required for maintenance of the sign. The display surfaces shall be kept neatly painted or finished at all times.
2. The base of any sign erected on the ground shall be kept clear of weeds, rubbish or other combustible material at all times.
3. Not more than ninety (90) days after the cessation of a business activity, service or product, the related signs shall be removed, or the face of said signs shall be removed and replaced with blank panels painted to match adjacent background colors.
4. If a tenant is negligent in maintaining or repairing or removing signing, the DOA will have necessary work performed at the lessee's expense.



10.0 DESIGN REVIEW PROCESS



10.0 Design Review Process and Developer Participation

A design review process will assist developers in identifying issues and requirements ahead of time before costly investments in plans are made and will ensure that development at LAX Northside meets the requirements of these guidelines. This review process will satisfy the requirements of the Department of Airports only. Additional review by the FAA, the City of Los Angeles, Cultural Affairs Commission, and the City of Los Angeles Departments of Planning and Building and Safety will be required. The Department of Airports will assist the developer in scheduling these reviews.

The Department of Airports review will take place in four steps:

- Pre-design
- Conceptual Site Plan Review
- Design Development Review
- Construction Document Review

At each review step, the project will be reviewed and be approved as submitted, approved with conditions, or not accepted.

Pre-Design

The developer will confer informally with Department of Airports staff to review the developer's proposed program and to identify po-

tential issues. At this meeting detailed requirements for future review will be established. A proposed program and generalized building location will be reviewed and approved for compatibility with adjacent development and conformance with these guidelines.

Conceptual Site Plan Review

At this stage, a conceptual site plan will be submitted which shows the location, footprint, and floor area of all buildings; building heights, vehicular access and circulation, parking areas including provisions for van and carpool spaces, pedestrian circulation, landscape and hardscape elements, service areas, and any other information determined to be needed. Preliminary architectural concepts may be submitted at this time.

Design Development Review

This review will evaluate a specific design for compliance with the design guidelines, zoning conditions, and any conditions imposed during the first two reviews. These plans should be a development of the already approved site plan.

Drawings submitted at this stage should include a site plan, architectural, landscape, signing and lighting plans sufficient to illustrate design intent and the methods which are being used to satisfy the requirements of these guidelines.

Construction Documents

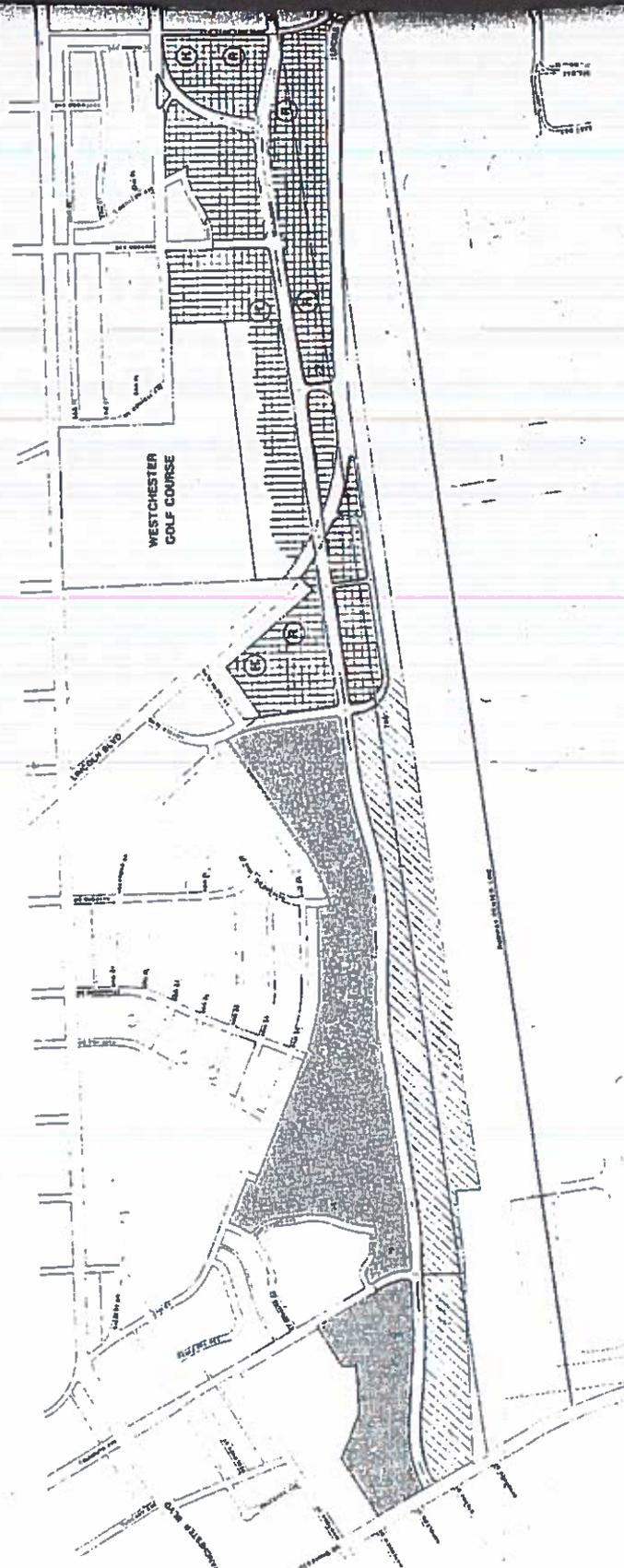
All construction documents will be reviewed to assure that they conform with the approved design plan and that any conditions of approval have been satisfied.

Compliance Verification

Construction will be monitored to verify compliance with the Design Guidelines and the approved construction drawings.



11.0 ZONE CHANGE CONDITIONS



LEGEND

- BUSINESS/RESEARCH PARK
- AIRPORT RELATED
- OFFICE
- HOTEL
- RETAIL/RESTAURANT

"Q" QUALIFICATIONS

- PARCEL 1 OFFICES, BUSINESS PARK AND RESEARCH CENTER
- PARCEL 2 OFFICES, BUSINESS PARK AND RESEARCH AND DEVELOPMENT CENTER
- PARCEL 3 COMMERCIAL USES, INCLUDING HOTEL, OFFICES, RESTAURANTS, SERVICE AND RETAIL USES
- PARCEL 4A LIGHT INDUSTRIAL USES, AIRLINE AND AIRPORT SUPPORT SERVICES, BUSINESS PARK, AND DEVELOPMENT CENTER; AND OFFICES WITHIN 400 FEET OF PERSHING DRIVE

- PARCEL 4B LIGHT INDUSTRIAL USES, AIRLINE AND AIRPORT SERVICES, BUSINESS PARK AND RESEARCH AND DEVELOPMENT CENTER; AND OFFICES WITHIN 400 FEET WEST OF LOTOLA BOULEVARD
- PARCELS 5, 6 AND 7 OFFICES, BUSINESS PARK AND RESEARCH AND DEVELOPMENT CENTER; AIRLINE AND AIRPORT SUPPORT AND ACCESSORY USES; AIRPORT VIEW SITE
- PARCEL 8 COMMERCIAL USES, INCLUDING OFFICE AND RESTAURANT USES; AND AUTOMOBILE SERVICE STATION

- PARCEL 9 COMMERCIAL USES, INCLUDING OFFICE AND RESTAURANT USES; AUTOMOBILE SERVICE STATION; WESTERN PORTION WESTERN PUBLIC AUTO. MOBILE PARKING IN THE EASTERN PORTION ONLY; AIRPORT VIEW SITE
- PARCEL 10 PUBLIC AUTOMOBILE PARKING
- PARCEL 11 COMMERCIAL USES, INCLUDING HOTEL, OFFICE, RESTAURANT, SERVICE AND RETAIL USES; AIRPORT VIEW SITE; AIRPORT VIEW SITE
- PARCEL 12A COMMERCIAL USES, INCLUDING OFFICES, HOTEL, RESTAURANT, SERVICE AND RETAIL USES
- PARCEL 12B COMMERCIAL GOLF COURSE

Permitted Uses

UN W NYC
PARCELS

11.0 Zone Change Conditions

(Q) Qualifications - Conditions of Approval

1. The subject property shall be used for those principal and accessory uses in development areas as indicated on the attached map. (Exhibit E-1), as follows:

- a. Area East of Lincoln Boulevard
 - Parcel 8 - Commercial uses, including office and restaurant uses; and automobile service station.
 - Parcel 9 - Commercial uses, including office and restaurant uses; automobile service station in the western portion only; public automobile parking in the eastern portion only; and an airport view site.
 - Parcel 10 - Public automobile parking.
 - Parcel 11 - Commercial uses, including hotel, office, restaurant, service and retail uses, and a movie theater complex.
 - Parcel 12A - Commercial uses, including offices, hotel, restaurant, service and retail uses.
 - Parcel 12B - A commercial golf course, including golf driving tees and ranges and similar commercial golf uses.
- b. Area Between Lincoln Boulevard and Falmouth Avenue (Extended)

Parcel 2 - Offices, business park and research and development center.

Parcel 3 - Commercial uses, including hotel, offices, restaurants, service and retail uses.

Parcel 4B - Light industrial uses, airline and airport support services, including flight kitchens, caterers, ticket reservation centers, custom house brokers, warehouse, airport maintenance and ground services (excluding engine shops), freight forwarders, package delivery services, security services, import/export services and other similar accessory or support services; business park and research and development center; and offices within 400 feet west of the prolongation of Loyola Boulevard on the south side of Westchester Parkway.

Parcels 5, 6, and 7 - Offices, business park and research and development center; airport view site; airline and airport support and accessory uses.

c. Area West of Falmouth Avenue (Extended)

Parcel 1 - Offices, business park and research and development center.

Parcel 4A - Light industrial uses, airline and airport support services, including flight

kitchens, caterers, ticket reservation centers, custom house brokers, warehouse, airport maintenance and ground services (excluding engine shops), freight forwarders, package delivery services, security services, import/export services and other similar accessory or support services; business park and research and development center; and offices within 400 feet east of Pershing Drive on the south side of Westchester Parkway.

2. No aircraft or engine run-ups shall be permitted within the project boundaries.

3. The height of structures shall be controlled as follows:

a. Parcel 2 - No structure located within 100 feet of the north airport property line between Loyola Boulevard and Hasings Avenue shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot. All accessory facilities on the roofs of such buildings, such as air conditioning units and other equipment, shall not be visible from nearby residential properties to the north and are exempted from the height restriction.

b. Parcels 1 and 4A - No structure located within 200 feet of the north airport property

line between Falmouth Avenue and Pershing Drive shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot. No structure located within Areas 1 or 4A south thereof shall exceed four stories (including parking levels) or 55 feet in height measured from the finished grade of the lot. All accessory facilities on the roofs of buildings within Parcel 1, such as air conditioning units and other equipment, shall not be visible from nearby residential properties to the north and are exempted from the height restriction.

4. Structures shall observe minimum setbacks, as follows:
 - a. Parcel 2 - All structures shall observe a minimum setback of 50 feet from the north airport property line between Loyola Boulevard and Haslings Avenue. Open parking areas are permitted within the required building setback area, except as may be modified by required landscape buffer setbacks.
 - b. Parcel 1 - All structures shall observe a minimum setback of 100 feet from the north airport property line between Falmouth Avenue and Pershing Drive. Open parking areas are permitted within the required building setback area, but not within the required landscape buffer setback.

5. Landscaped buffer setbacks shall be required, as follows:

- a. Parcels 11 and 12A - A 30-foot-wide landscaped buffer setback along 88th Street between Sepulveda Westway and Liberator Avenue shall be required containing no buildings or structures, except for walls or fences, and include trees, 15 gallons and ten feet tall at the time of planting, planted at a maximum of 30 feet apart, of a type similar to those contained in Section 7 of the Planning Department's Technical Report of "Shrubs and Trees for Landscaping and Screening". Further, a 15-foot-wide landscaped buffer setback along Liberator Avenue and 88th Place (adjacent to the Emerson Manor School site) shall be required and include a dense screen of closely planted evergreen trees.
- b. Parcel 2 - A 15-foot-wide landscaped buffer setback along Cum Laude Avenue and 92nd Street (adjacent to the St. Bernard High School site) shall be required and include a dense screen of closely planted evergreen trees.
- c. Parcel 1 - A 50-foot-wide landscaped buffer setback along the north airport property line between Falmouth Avenue and Pershing Drive shall be required and include low, minimum-view-obstructing

shrubs and ground cover (no trees) which are compatible, where appropriate, with adjacent off-airport landscaping.

6. The total development of the subject property shall not generate more than 6,340 project-related inbound daily trips in the A.M. peak-hour, no more than 7,000 project-related outbound daily trips in the P.M. peak-hour (source: "Los Angeles International Airport Final Environmental Impact Report, LAX North Side Development Project, April 1983"). This determination shall be based on the highest average hourly A.M. and P.M. counts taken over a typical Monday through Friday period at exclusive project access drives and/or roadways. Such counts shall be taken at the expense of the Department of Airports of individual developers at intervals determined to be reasonable by the City Planning Department after consultation with the Department of Transportation; but in no event shall the first count be required before certificates of occupancy have been issued for buildings having a combined floor area in excess of 3,500,000 square feet, as defined in Section 12.21.1 (A, 5 and B, 4) of the Municipal Code.

7. Prior to the issuance of building permits, detailed development plans, including a complete landscape plan, shall be submitted to the Department of City Planning for approval.

8. Prior to the issuance of building permits, an on-site roadway/highway construction phasing plan and individual parcel access plan shall be submitted to the Department of City Planning for approval, and for coordination and review with the Department of Transportation and the Bureau of Engineering.

9. Prior to the recordation of a final tract map covering all or any portion of the subject property, and prior to the approval of any variance granting use of any portion of the property before recordation of a final tract map, the Board of Airport Commissioners shall cause to be prepared and shall approve a project design plan, together with developmental guidelines for the entire project area. The primary purposes of the design plan and guidelines shall be to provide future developers with a unifying theme and design handbook and to provide the Department of Airports with a basis for reviewing and coordinating project development plans. The project design plan shall establish a unifying architectural theme, while the elements of the developmental guidelines shall include consideration of: Building design, height, bulk, locations and interrelationships; building materials, colors and textures; public and private area landscaping; public and private signage and graphics; airport view sites; street furniture; exterior

lighting; internal pedestrian and vehicular circulation; parking area design; on- and off-airport interfaces; and other design considerations as may be appropriate. The project design plan and developmental guidelines shall be governed by the conditions of approval attached to this change of zone, wherever relevant.

10. The project design plan and developmental guidelines shall treat Parcel 11 as a single comprehensive planned unit, providing a compatible interface with the existing and potential future uses on the privately owned property adjoining the east side of Sepulveda Westway between La Tijera Boulevard and Will Rogers Street. Convenient pedestrian access to Parcel 11 shall be provided from Sepulveda Westway, and the Sepulveda Westway frontage shall be attractively landscaped. The design of Parcel 11 shall provide, to the extent practicable and feasible, for visual continuity and accessibility between Parcel 11 and the properties on the east side of Sepulveda Westway.

11. All open areas not used for buildings, driveways, parking areas, recreational facilities, or walks shall be attractively landscaped in accordance with a landscape development plan prepared by a licensed landscape architect or licensed architect. Approved copies of such plans shall be submitted to the Department of

Building and Safety before issuance of a building permit. All landscaped areas shall be equipped with automatic sprinklers and shall be maintained in a first-class condition at all times. All types of plants selected and required watering system for such landscaping shall, to the extent possible, conserve water and shall be consistent with any water conservation ordinance enacted by the City.

12. All open parking areas located within Parcel 1 shall devote at least 4 percent of the parking area to landscaping, with at least three-fourths of the 4 percent devoted to interior landscaping distributed throughout the parking area.

13. A 6-foot high chain link fence shall be provided along the airport property line in the following locations: Along Liberator Avenue and 88th Place (adjacent to the Emerson Manor School site in Parcel 12A); along Cum Laude Avenue and 92nd Street (adjacent to the St. Bernard Senior High School site in Parcel 2); and between Falmouth Avenue and Pershing Drive (along the north boundary of Parcel 1).

14. A solid 8-foot high masonry block wall shall be constructed along the north airport property line between Loyola Boulevard and Hastings Avenue (along the north

- boundary of Parcel 2) and trees and tall shrubs shall be planted immediately south of the wall on airport property to provide additional visual screening above the wall. The wall shall be installed and measured in height from the finished grade of the alley between Loyola Boulevard and Rayford Drive, and from the finished grade of the adjoining residential lots between Rayford Drive and Hastings Avenue.
15. All development on the property shall provide off-street parking on the following bases: One space for each guest room; One space for each guest room; one space for each 300 square feet of floor area for office uses and for business parks and research and development centers; one space for each 250 square feet of floor area for retail and service commercial uses; 16 spaces for each 1,000 square feet of floor area for restaurants; and not less than three stalls for each four employees on the main shift for light industrial uses. Floor area shall mean the area enclosed within the walls of buildings, exclusive of floor area devoted to off-street parking or accessory areas, as defined under Section 12.21-A, 4 of the Municipal Code. The Director of Planning may modify these parking standards where appropriate to permit reduced parking in mixed-use or joint-use facilities.
 16. The hours of operation for trash pick-up or freight deliveries or pick-up, within 300 feet of the north boundary line of the project, shall be limited to between 7 a.m. and 9 p.m.
 17. Adequate protection against exterior noise shall be included in the design and construction of hotels and motels. Adequate protection shall mean a noise reduction (exterior to interior) sufficient to insure that the interior community noise equivalent level (CNEL) in all habitable rooms does not exceed 45db during aircraft operations.
 18. All central air heating and/or air conditioning units shall be installed with an air filtration system (either charcoal or electronic) to improve the air quality effects on the project occupants. This requirement shall not preclude the installation of operable windows for passive or natural heating or cooling opportunities.
 19. The use of any outdoor public address or paging system shall be permitted only south of the Westchester Parkway and shall utilize: (1) a low-pressure speaker system with each speaker having an audible range limited to a 400-square-foot area and placed a maximum of 40 feet apart, or (2) a sound system designed by a qualified sound engineer so as to reduce the impulse noise level to inaudibility beyond the premises of the establishment.
 20. In all industrially zoned areas, all buildings, and enclosing walls or fences shall be constructed, the machinery and equipment shall be so installed and maintained and the activities shall be so conducted that all noise, vibration, dust, odor and other objectionable factors shall be confined or reduced to the extent that no reduction in the use of property will result to persons residing adjacent to the subject property. Whenever there is any difficulty in determining the application of these provisions to any specific case, the Department of Building and Safety shall make such determination.
 21. All building signs shall be designed by the architect of the building or facility. These building identification signs shall be wall signs designed for placement on the face of the proposed building and not projecting above the roof or parapet wall. Except that, free-standing identification or project directory signs not exceeding 50 square feet in area on each face and not exceeding 8 feet in height may be permitted, subject to individual review and approval by the Department of Airports.
 22. All lighting shall be directed onto the site and no flood-lighting shall be located as to

be seen directly by the adjacent residential areas. This condition shall not preclude the installation of low-level security lighting.

23. All buildings within 100 feet of the north airport property line between Loyola Boulevard and Hastings Avenue shall be designed so that all second-story or higher windows that overlook the adjacent single-family residential homes shall not be transparent below an eye level of 6 feet on any floor, but shall be translucent; or the building shall be designed in such a way that windows will not overlook the single-family residential properties to the north.

24. Vehicular driveway ingress and egress from the subject property shall be prohibited from Cum Laude Avenue and from 92nd Street (Parcel 2); from Lincoln Boulevard between Sepulveda Boulevard and the airport property line north of the Westchester Parkway (Parcel 6, 7, 8, 9, 10 and 12A); and from 88th Street, Liberator Avenue, 88th Place and Emerson Avenue (Parcel 12A).

25. In all industrially zoned areas, except Parcels 4A and 4B, all activities shall be conducted within an enclosed building. Within Parcels 4A and 4B, all outdoor activities shall be located on the southerly side of the buildings and screened from public streets.

26. All utilities within the project shall be installed underground.

27. All development within the project shall contain trash compactors to reduce the volume of solid waste to be removed from the site.

28. The Department of Airports and individual developers shall develop and implement a transportation system and parking management plan, which shall include the following as a minimum.

a. Individual developers shall encourage public transit use by offering subsidized transit passes to employees.

b. The Department of Airports shall evaluate the potential for subscription bus service for large concentrations of employees.

c. Each developer employing 500 or more permanent employees shall provide company-owned vans and/or offer subsidies to individual vanpool operators an encourage the formation of vanpools.

d. The Department of Airports shall make carpooling information available to building occupants and encourage formation of carpools and vanpools and shall establish a carpool-matching service.

e. Individual developers shall develop and implement programs to offer preferential parking to carpooling and vanpooling building occupants.

f. The Department of Airports shall make transit schedules, maps and other transit information available to building employees and users.

g. To the extent feasible, individual developers shall encourage flexible working hours.

h. The Department of Airports and each developer employing 500 or more permanent employees shall appoint a rideshare coordinator to administer the functions required by this condition.

29. Prior to major grading of the project site, a rodent control effort shall be undertaken to reduce the existing rodent population within the project area.

30. Developers of individual projects shall take the following steps relative to energy conservation:

a. Consult with the Los Angeles Department of Water and Power and Southern California Gas Company to determine feasible energy conservation features which could be incorporated into the design of structures, beyond those required by State and City laws.

- b. Utilize, where cost-effective, alternative sources of energy such as solar water and pool heating.
 - c. Utilize the Department of Water and Power's energy pre-audits on all new industrial developments.
31. Individual developers shall consult with the Los Angeles Police Department on the provision of security measures for developments, which measures shall include the following at the minimum:
- a. For each developer responsible for 200,000 or more square feet of total floor area (Los Angeles Municipal Code 12.21.1), guards shall patrol all levels of buildings and access to and from buildings shall be monitored by a guard on duty during nighttime and weekends.
 - b. Subterranean parking levels shall be lighted 24 hours a day.
 - c. All parking levels, corridors and arcades shall be lighted during the nighttime and weekends.
32. A qualified archaeologist shall be available, as needed, during site grading and subsurface operations, with the authority to assure reasonable measures to insure protection and recovery of significant archaeological resources.
33. All conditions imposed by this action, with the exception of Condition No. 9 relating to a project design plan and development guidelines, may be fulfilled incrementally as individual phases of the subject property are developed.

In accordance with provisions of Section 17.03 of the Los Angeles Municipal Code, the Advisory Agency approved Tentative Tract No. 34386 composed of 12 lots, located on the north side of the Los Angeles International Airport (LAX) extending generally between Sepulveda Boulevard and Pershing Drive, with varying depths, for a proposed development of approximately 358 acres of land with commercial, recreational and airport-related industrial land uses, subject to the following conditions:

1. That Lincoln Boulevard between Sepulveda Boulevard and proposed Westchester Parkway be dedicated to a width of 120 feet as shown on the tentative map, together with suitable transitions to the existing dedications at each end and on- and off-ramps to Westchester Parkway satisfactory to the City Engineer and the Department of Transportation.
2. That Westchester Parkway between Sepulveda Westway and Lincoln Boulevard be dedicated to a width of 120 feet and that Westchester Parkway between Lincoln Boulevard and Pershing Drive be dedicated to a width of 110 feet, together with on- and off-ramps for the overpass at Lincoln Boulevard to the satisfaction of the City Engineer and the Department of Transportation.
3. That the extension of La Tijera Boulevard west of Sepulveda Westway to Westchester Parkway be dedicated to a width of 100 feet substantially as shown on the tentative map.
4. That a variable width strip of land be dedicated along the west side of Sepulveda Boulevard between Lincoln Boulevard and 96th Street to complete a 74.5-foot half-street dedication (Reference Plan No. P-26953).
5. That a minimum turning area be dedicated at the westerly terminus of 88th Street westerly of McLean Avenue is satisfactory to the City Engineer.
6. That a variable-width strip of land be dedicated along the south side of 88th Street at its intersection with proposed La Tijera Boulevard to provide for the channelization in accordance with Figure 4.7a, Page IV-59, of the Final EIR and as shown on the tentative map.
7. That an 18 foot wide strip of land be dedicated along the west side of Sepulveda Westway between La Tijera Boulevard and Lincoln Boulevard to complete a 43-foot half-street, together with a realignment at the intersection with Lincoln Boulevard.
8. That the intersection of Sepulveda Westway, 92nd Street and Lincoln Boulevard be realigned to improve the angle, location and traffic movements satisfactory to the City Engineer and the Department of Transportation.
9. That three foot wide strips of land be dedicated along the east and west sides of Emerson Avenue between Westchester Parkway and 88th Place, in accordance with secondary highway standards, together with a suitable turning area at its terminus south of 88th Place.
10. That the realignment of Loyola Boulevard be dedicated 86 feet wide and its easterly extension to Lincoln Boulevard south of Westchester Parkway be dedicated 64 feet wide, substantially as shown on the tentative map.
11. That Falmouth Avenue adjoining the tract be dedicated in accordance with secondary highway standards as shown on the tentative map unless it is deleted as a secondary highway prior to recordation.
12. That 8 foot, 40 foot and variable width strips of land be dedicated along the east side of Pershing Drive adjoining the tract to complete a 92 to 100 foot street dedication satisfactory to the City Engineer.

13. That for all street intersections in or adjoining the tract, the radius for property line returns shall be 20 feet for major and secondary highways, including local streets adjoining "M" zoned land and 15 feet for all other streets.
14. That arrangements be made with the Los Angeles County Flood Control District prior to recordation of the final map for any necessary easements and permits with respect to existing and/or new facilities.
15. That a covenant and agreement be recorded stipulating that the "airport drainage ditch," as shown on the tentative map, shall be maintained by the Department of Airports.
16. That prior to the issuance of a building permit for each development, two copies of a parking area and driveway plan be submitted to the appropriate district office of the Bureau of Engineering, and two copies be submitted to the Department of Transportation for approval, or that a covenant and agreement be recorded agreeing to do the same.
17. That the streets and alleys within the boundary of the tentative map, not shown as being retained, be permitted to be merged with the remainder of the subdivision pursuant to Section 66499.20-1/2 of the State Government Code, and in addition, the following be done and be administered by the City Engineer:
- a. That consents to the streets and alleys being merged and waivers of any damages that may accrue as a result of such merger, be obtained from all property owners who might have certain rights in the areas being merged.
 - b. That satisfactory arrangements be made with all public utility agencies maintaining existing facilities within the area being merged.
18. That the Board of Airport Commissioners, by resolution, shall guarantee compliance with tract conditions in a manner satisfactory to the Advisory Agency and the City Engineer in lieu of posting bonds and recording covenant and agreements.
19. That a covenant and agreement be recorded stipulating that the Department of Airports will provide maintenance for the landscaped medians in Lincoln Boulevard, Westchester Parkway and Emerson Avenue, as required herein.
20. That the affected lots of the tract be restricted by the final map against vehicular access to and from:
- a. Cum Laude Avenue, 92nd Street, St. Bernard Street and 91st Street.
 - b. Lincoln Boulevard between Sepulveda Boulevard and the airport property line northwesterly of Westchester Parkway.
 - c. Eighty-Eighth Street, Liberator Avenue, 88th Place, Emerson Avenue between 88th Street and 88th Place, and the alley southeasterly of 91st Street between Loyola Boulevard and Rayford Drive.
 - d. Rayford Drive, Stanmoor Drive, Hastings Avenue and Pershing Drive.
21. That the following requirements in connection with grading and construction in and adjacent to public right-of-ways be complied with in a manner satisfactory to the City Engineer:
- a. Cut and fill slopes shall be no steeper than 2:1, steeper slopes may be allowed if calculations are submitted justifying these slopes. In no case shall slopes exceed 1-1/2:1.
 - b. The toes and crests of all slopes shall be located on private property and shall be set back 2 and 3 feet, respectively, from the property line.

- c. Where fill overlies a cut slope, the fill shall be keyed horizontally into bedrock a minimum width of 12 feet or the slope shall be over-excavated a minimum of one equipment width or 12 feet and replaced as a compacted fill slope.
- d. All streets shall be founded upon firm, natural materials or properly compacted fill. Any existing loose fill, loose colluvial or alluvial soils, organics or landslide material shall be removed prior to placement of engineered fill.
- e. Fill material shall be compacted to a minimum of 90 percent relative compaction, as defined in Bureau of Engineering Standard Plan S-610. Fill shall be benched into competent material.
- f. Slopes that are not immediately proposed to be planted or sprinkled before the October-March winter rain season shall be sprayed with a non-toxic, non-flammable, non-polluting material such as or equivalent to SOILBOND HP 401.
- g. All slopes shall be planted and a sprinkling system installed as soon as possible after grading to alleviate erosion.
- h. Slopes which daylight adversely dipping bedding shall be supported by either a retaining wall or designed buttress fill.
- i. A final as-graded geologic and soils report shall be submitted to the City Engineer's Office prior to acceptance of City streets.
- j. Prior to issuance of grading permits, a geologic and soils report covering the possibility of mud flows must be submitted to this office.
22. Prior to the recordation of the final map, the subdivider will prepare and execute four copies of a covenant and agreement (General Form) in a manner satisfactory to the Department of Building and Safety and the Planning Department, binding the subdivider and all successors to the following:
- a. Construct all exterior walls with double-pane glass and construct exterior walls and floor-ceiling assemblies in a manner to provide an airborne sound insulation system achieving an Ldn of 75, as defined by the Wyle Research Report WCR 74.3, June 1974, prepared for the City of Los Angeles (this condition applies to building areas used for offices, retail sales and restaurants).
- b. That a solar access report shall be submitted to the satisfaction of the Advisory Agency prior to obtaining a building permit.
- c. That a summary of the solar report will be provided to lessees of the proposed subdivision.
- d. That all construction activities be limited to weekdays during daylight hours.
- e. That the lease agreements shall encourage that lessees develop active recycling programs to reduce the volumes of solid waste.
23. That the subdivider file a covenant and agreement satisfactory to the City Planning Department to the effect that the tract area will never be developed for residential use unless the required dedication of land or payment of fees in lieu thereof for park and recreational purposes have been made. Four copies shall be filed for approval by the Planning Department prior to recordation of the final map.
24. Prior to clearing any conditions and thus recordation, a copy of the final zone change ordinance to (T) (Q) C2-1 and (T) (Q) M2-1 Zones shall be submitted to the Advisory Agency. If there are any differences in the adopted ordinance and the Advisory Agency or City Planning Commission action, a modification will be required. The applicant will be required to furnish copies of the final zone change ordinance and Council directions for review and comparison.

25. The Fire Department requires the submission and approval of a plot plan prior to the recordation of the final map or the recordation of an agreement satisfactory to the Fire Department to the effect that the said plan will be submitted prior to issuance of building permits for the tract. Forms may be obtained at the Bureau of Engineering counter.
- All access roads to be paved to City Engineer's requirements with a minimum width of 28 feet, or to the satisfaction of the Fire Department.
- A Fire Department permit is required on all private fire hydrant systems.
- The making of financial arrangements with the Department of Water and Power will indicate concurrence with the installation location of public fire hydrants.
- All hydrant installations and enlargements to be completed prior to any street paving required for this project.
26. That the tract be permitted to record with final map units in a number and sequence satisfactory to the Advisory Agency in consultation with the Department of Transportation, City Engineer and Council office to assure adequate infrastructure in accordance with the level of development.
27. That prior to recordation of any final map units, the subdivider shall specifically request the City Planning Department through its preparation of the Los Angeles International Airport Plan and the Westchester-Playa Del Rey District Plan to initiate proceedings for the following:
- a. Delete the extension of La Tijera Boulevard westerly through the Westchester Golf Course to its connection to the new Westchester Parkway west of Lincoln Boulevard.
 - b. The downgrading of 88th Street to a collector street between the proposed La Tijera connector and Emerson Avenue and to a local street west of Emerson Avenue.
 - c. The deletion of Stanmoor Drive between the north project boundary and the new Westchester Parkway.
 - d. The addition of the La Tijera Connector between 88th Street and the new Westchester Parkway as a major highway.
 - e. The upgrading of the new Westchester Parkway from Sepulveda Westway to Pershing Drive to a major highway.
 - f. The upgrading of Sepulveda Westway from the Westchester Parkway to La Tijera Boulevard to a secondary highway.
9. The upgrading of Loyola Boulevard to a secondary highway between the north project boundary and the new Westchester Parkway.
28. That prior to recordation, the Department of Airports shall submit evidence satisfactory to the Department of Transportation (DOT) and the Advisory Agency that they have contracted with Commuter Transportation Services (formerly Commuter Computer) or a like organization to prepare, develop and reasonably implement a Transportation System Management (TSM) Program. The implementation of the initial stages of the program will have to be in place no later than the first occupant of any portion of the project (the private contract agreement shall so provide and copies shall be given to the Advisory Agency and Department of Transportation for review).
- The TSM Program shall be an integral part of all lease agreements between the Department of Airports and lessees or tenants of the project and should consider those conditions listed in "Q" Condition No. 28, CPC 83-190 (ZC). An integral part of the TSM Program and the contract shall be the designation of a "rideshare coordinator." The rideshare coordinator required for this project, which may be shared by all lessees of the property and with other firms, shall be provided on an continuous

basis for the life of this project. The coordinator shall be an employee of Computer Transportation Services (formerly Commuter Computer) or a like organization. The rideshare coordinator shall submit a transportation system management report for review to the Department of Transportation and the Advisory Agency on an annual basis. The owner shall provide, with the annual report, a copy with a computation of gross floor area, net building area and a description of area use. This information shall also be reviewed to monitor the potential building limitations imposed under CPC 83-190, "Q" Condition No. 6.

In addition to the aforementioned annual report, prior to the recordation of succeeding units after the first tract map unit and three months after the date that any phase or tract unit project is fully constructed and 18 months after each phase project or tract unit is fully occupied, the rideshare coordinator shall report on the effectiveness of the TSM Program and the percentage of the project's employee population participating in the TSM Program. At such time as the entire project is constructed, this evaluation shall be subsumed within the aforementioned annual report.

29. That prior to recordation of each final map unit, the subdivider shall execute an rec-

ord against the property a covenant and agreement in form and substance satisfactory to the City Attorney pursuant to which the applicant shall agree that the owner (s) or lessee (s) or successor (s) in interest of the property involved in this tract map will participate in any benefit assessment district or any trust fund based on a formula or criteria which is applicable to all new developments within the Westchester-Venice-Palms-LAX Transportation Corridor Specific Plan area, had such ordinance, resolution or plan been in effect at the time of approval of this tract map.

The following are among those improvements that should be included in the Specific Plan. In the event the applicant does not participate in such a benefit assessment district or if the benefit assessment district does not provide for the following highway improvements, Items c, h, k and l shall be implemented by and at the expense of the applicant. Any expenses incurred by the applicant for such improvements will be credited to any assessment or fund contribution subsequently placed into effect:

a. Sepulveda Boulevard between Lincoln Boulevard and north of Manchester Avenue: Widen street and modify median to

provide eight through lanes, plus dual left turn lanes. (Existing right-of-way width is 126 feet south of and 120 feet north of Manchester.)

b. Manchester Avenue at La Tijera Boulevard: As a TSM measure, modify median to provide six through lanes and dual westbound left-turn lanes.

c. Manchester Avenue at Lincoln Boulevard: Modify median islands and widen to provide six through lanes and dual left turn lanes on all approaches.

d. La Tijera Boulevard at Sepulveda Boulevard: Widen roadway to 80 feet to provide six through lanes and dual left turn lanes.

e. La Tijera Boulevard at Airport Boulevard: Modify median island to provide six through lanes and dual westbound left turn lanes.

f. Vista Del Mar at Imperial Highway: Add a northbound right-turn lane, if this is not accomplished as part of the reconstruction of the Hyperion Treatment Plant.

g. Vista Del Mar at Grand Avenue: Widen to provide left turn lanes.

- h. Lincoln Boulevard between Westchester Parkway and Manchester:
Remove median island and widen street to provide six through lanes and dual left turn lanes at all signalized intersections in coordination with the Council Office.
- i. Arbor Vitae Street between Airport Boulevard and San Diego Freeway:
Contribute to the cost of constructing the Arbor Vitae interchange with the San Diego Freeway and associated improvement of Arbor Vitae Street between the San Diego Freeway and Airport Boulevard.
- j. La Tijera Boulevard at San Diego Freeway:
As an alternative, if the Arbor Vitae interchange is not built, widen La Tijera Boulevard (including the freeway over-crossing) to provide six through lanes and a three lane width median to provide dual left turns onto the freeway (see example LADOT Drawing No. 011,139, Santa Monica Boulevard at San Diego Freeway).
- k. Manchester Avenue at Sepulveda Boulevard:
As an interim TSM measure, remove median islands and re-stripe all approaches for dual left turn lanes with appropriate traffic signal phasing.
- l. Imperial Highway at Pershing Drive:
Provide dual westbound right turn lanes.
30. That satisfactory arrangements shall be made between the Department of Airports, the Southern California Rapid Transit District and the Department of Transportation to include provisions for bus stops in conjunction with the design of the roadway improvements.
31. That the owner reserved a bikeway easement along the extension of Stanmoor Drive and Emerson Avenue or along alternate routes through the tract property to Westchester Parkway to the satisfaction of the Department of Transportation and the Council office.
32. That, if determined to be warranted by the Department of Transportation, new traffic signals shall be installed at the intersections of:
- Pershing Drive and Westchester Parkway.
 - Falmouth Avenue and Westchester Parkway.
 - Loyola Boulevard and Westchester Parkway.
 - La Tijera and Lincoln Boulevards.
- e. Lincoln Boulevard northbound on/offramps and Westchester Parkway.
- f. Emerson Avenue and Westchester Parkway.
- g. La Tijera Boulevard and Westchester Parkway.
- h. La Tijera Boulevard and Sepulveda Westway.
- i. Sepulveda Westway, Westchester Parkway and Will Rogers Street.
- j. Westchester Parkway and any major development access driveways.
33. That satisfactory arrangements be made with the cable television franchise holder for this area in accordance with policies adopted by the Department of Transportation to assure that cable television facilities will be installed in the same manner as other required improvements. Refer to the Los Angeles Municipal Code, Section 17.05-N. Evidence of the arrangements must be submitted to the Department of Transportation before the condition can be cleared by the Department.
- The current cable television holder for this area is:

Jack Barry Cable TV
6382 Arizona Circle
Westchester, California 90045
Telephone: 644-5844

factory to the City Engineer and located within the California Coordinate System prior to recordation of the final map. Any alternative measure approved by the City Engineer would require prior submission of complete field notes in support of the boundary survey.

g. That any required slope easements be dedicated by the final map.

h. That each lot in the tract comply with the width and area requirements of the Zoning Ordinance.

34. That in conjunction with the recordation of any final map units over Lots 3, 5, 6, 8 or 12, the subdivider shall dedicate, as a future street, the land within the tract boundaries necessary to provide for the extension of Lincoln Boulevard underneath the runways to the satisfaction of the Department of Transportation and the City Engineer or that the extension of Lincoln Boulevard be deleted from the Los Angeles International Airport Plan by amendment. The Plan Amendment should, in its preparation, include a study of the feasibility of providing the indicated extension and the impacts to traffic and circulation of its deletion from the Plan.

c. That satisfactory arrangements be made with both the Water System and the Power System of the Department of Water and Power with respect to water mains, fire hydrants, service connections and public utility easements.

i. That one foot future street and/or alleys be shown along the outside of incomplete public dedications and across the termini of all dedications abutting un-subdivided property. The one foot dedications on the map shall include a restriction against their use for purposes until such time as they are accepted for public use.

d. That any necessary sewer, street, drainage and street lighting easements be dedicated. In the event it is necessary to obtain off-site easements by separate instruments, records of the Bureau of Right-of-Way and Land shall verify that such easements have been obtained. The above requirements do not apply to easements of off-site sewers to be provided by the City.

j. That any one foot future street and/or alley adjoining the tract be dedicated for public use by the tract, or that a suitable resolution of acceptancy be transmitted to the City Council with the final map.

35. That sufficient land to accommodate a transit station be provided in Lot 11 to the satisfaction of the Department of Transportation.

k. That no public street grade exceed 15%.

S-2 That the following provisions be accomplished in conformity with the improvements constructed herein:

e. That drainage matters be taken care of satisfactory to the City Engineer.

S-1 a. That the sewerage facilities charge be deposited prior to recordation of the final map over all the tract in conformance with Section 64.11.2 of the Municipal Code.

f. That satisfactory street, sewer and drainage plans and profiles as required, together with a lot grading plan of the tract and any necessary topography of adjoining areas be submitted to the City Engineer.

a. Survey monuments shall be placed and permanently referenced to the satisfaction of the City Engineer. A set of ap-

b. That survey boundary monuments be established in the field in a manner satis-

- proved field notes shall be furnished, or such work shall be suitably guaranteed, except where the setting of boundary monuments requires that other procedures be followed.
- b. Make satisfactory arrangements with the Department of Traffic with respect to street name, warning, regulatory and guide signs.
 - c. All grading done on private property outside the tract boundaries in connection with public improvements shall be performed within dedicated slope easements or by grants of satisfactory rights of entry by the affected property owners.
 - d. All improvements within public streets, private street, alleys and easements shall be constructed under permit in conformity with plans and specifications approved by the Bureau of Engineering.
 - e. Any required bonded sewer fees shall be paid prior to recordation of the final map.
- S-3 That the following improvements be either constructed prior to the recording of the map or that such construction be suitably guaranteed:
- a. Construct on-site sewers to serve the tract as determined by the City Engineer.
 - b. Construct any necessary drainage facilities.
 - c. Install street lighting facilities to serve the tract as required by the Bureau of Street Lighting.
 - d. Plant street trees and remove any existing trees within dedicated streets or proposed dedicated streets as required by the Street Tree Division of the Bureau of Street Maintenance. When the City has previously been paid for tree planting, the subdivider or contractor shall notify the Street Tree Division (485-5675) upon completion of construction to expedite tree planting.
 - e. Repair or replace any off-grade or broken curb, gutter and sidewalk satisfactory to the City Engineer.
 - f. Construct access ramps for the handicapped as required by the City Engineer.
 - g. Close any unused driveways satisfactory to the City Engineer.
 - h. Abandon sewers in a manner satisfactory to the City Engineer.
 - i. Drainage facilities required under Condition No. S-3(b) will include, but not be limited to, the construction of the following:
 - (1.) Catch basins to the existing storm drains and any new underground drains will be required to provide 50 year protection at the following locations:
 - (a.) The proposed intersection of La Tijera Boulevard and Westchester Parkway.
 - (b.) Liberator Avenue and 88th Place.
 - (c.) Eighty-Eighth Street and McConnell Avenue.
 - (d.) The new terminus of Rayford Avenue south of 91st Street.
 - (e.) The new terminus of Stanmoor Avenue south of 91st Street.
 - (2.) Construct storm drains to provide 50 year storm protection at the following locations:
 - (a.) Loyola Boulevard and La Tijera Boulevard.
 - (b.) The sump in 91st Street at the former Hastings Avenue.
 - (3.) Provide sufficient storm water collection on Westchester Parkway to keep the storm flow from the traveled lanes for a 10 year storm.

j. Grade the streets being dedicated and adjoining the tract, as required.

k. Improve Lincoln Boulevard being dedicated and adjoining the tract by the construction of the following:

(1.) Concrete curbs, concrete gutters and 10 foot concrete sidewalks with tree wells.

(2.) Suitable surfacing to provide a 100 foot roadway which includes a landscaped median island with mountable curbs.

(3.) On- and off-ramps satisfactory to the City Engineer and the Department of Transportation.

(4.) Any necessary removal and re-construction of existing improvements.

(5.) The necessary transitions to join the existing improvements.

l. Improve Westchester Parkway being dedicated by the construction of the following:

(1.) Concrete curbs, concrete gutters and 10 foot concrete side walks with tree wells.

(2.) Suitable surfacing to provide a 100 foot

roadway between Sepulveda Westway and Lincoln Boulevard and a 90 foot roadway between Lincoln Boulevard and Pershing Drive. The roadway widths include landscaped median islands with mountable curbs and bicycle lanes.

(3.) On- and off-ramps at the Lincoln Boulevard overpass satisfactory to the City Engineer and the Department of Transportation.

(4.) Any necessary removal and re-construction of existing improvements.

(5.) The necessary transitions to join the existing improvements.

m. Improve Sepulveda Boulevard by the construction of the following:

(1.) A concrete curb, a concrete gutter and a 10 foot concrete sidewalk with tree wells on the west side between Lincoln Boulevard and 96th Street.

(2.) Place additional concrete to complete a full-width sidewalk on the east side between Sepulveda Eastway and Interceptor Street.

(3.) Suitable surfacing on the west side between Lincoln Boulevard and northerly of 96th Street to provide a 64.5 foot roadway westerly of the center line substantially, as shown on City Engineer's Plan P-26953.

(4.) Remodel the intersection at Lincoln Boulevard to provide a grade separation to accommodate the left turn lanes from northbound Sepulveda Boulevard to westbound Lincoln Boulevard satisfactory to the City Engineer and the Department of Transportation.

(5.) Remodel the median between Sepulveda Eastway and Interceptor Street satisfactory to the City Engineer.

(6.) Any necessary removal and/or re-construction of existing improvements.

(7.) The necessary transitions to join the existing improvements.

n. Improve La Tijera Boulevard between Sepulveda Westway and Westchester Parkway by construction of the following:

(1.) Concrete curbs, concrete gutters and 10 foot concrete sidewalks with tree wells.

(2.) Suitable surfacing to provide an 80 foot roadway.

(3.) Any necessary removal and reconstruction of existing improvements.

(4.) The necessary transitions to join the existing improvements.

- o. Improve 88th Street by the construction of the following:
- (1.) Suitable improvements of the turning area at the westerly terminus of 88th Street.
 - (2.) Suitable improvements at the intersection with the proposed extension of La Tijera Boulevard to provide for the channelization as shown on the tentative map.
- p. Improve Sepulveda Westway by the construction of the following:
- (1.) A concrete curb, a concrete gutter and a full-width concrete sidewalk with tree wells on the west side between La Tijera Boulevard and Lincoln Boulevard.
 - (2.) Suitable surfacing to join the existing pavement and to complete a 33 foot half roadway.
 - (3.) Remodel the intersection at Lincoln Boulevard to the satisfaction of the City Engineer and the Department of Transportation.
- q. Improve Emerson Avenue from 88th Place to proposed Westchester Parkway by the construction of the following:
- (1.) Concrete curbs, concrete gutters and 5 foot concrete sidewalks and landscaping of the parkways.
 - (2.) Suitable surfacing to provide 20 foot roadways on both sides of a 26 foot landscaped median island with mountable curbs.
 - (3.) Suitable improvements of the turning area south of 88th Place.
 - (4.) Close Emerson Avenue south of 88th Place to through traffic satisfactory to the City Engineer.
- r. Unless it is deleted as a secondary highway prior to recordation, improve Falmouth Avenue by the construction of the following:
- (1.) Concrete curbs, concrete gutters and 10 foot concrete sidewalks with tree wells.
 - (2.) Suitable surfacing to provide a 33 foot half roadway northerly of 92nd Street and a 66 foot roadway southerly of 92nd Street. If a plan amendment is approved downgrading the secondary highway designation, a lessor roadway will be required.
 - (3.) Any necessary removal and reconstruction of existing improvements.
 - (4.) The necessary transitions to join the existing improvements satisfactory to the City Engineer.
- s. Improve Loyola Boulevard by the construction of the following:
- (1.) Concrete curbs, concrete gutters and 10 foot concrete sidewalks with tree wells.
 - (2.) Suitable surfacing to provide a 66 foot roadway and a 44 foot roadway at its easterly extension to Lincoln Boulevard.
 - (3.) Any necessary removal and reconstruction of existing improvements.
 - (4.) The necessary transitions to join the existing improvements.
- t. Improve the east side of Pershing Drive by the construction of the following:
- (1.) A concrete curb, a concrete gutter and a 10 foot concrete sidewalk with tree wells.
 - (2.) Suitable surfacing to join the existing pavement to complete a 40 foot half roadway.
 - (3.) Any necessary removal and reconstruction of existing improvements.
 - (4.) The necessary transitions to join the existing improvements.

- u. Improve 92nd Street and Cum Laude Avenue by the construction of an elbow curve within the existing right-of-way, including concrete curbs, gutters and sidewalks satisfactory to the City Engineer.
- v. Improve the terminus of Rayford Drive by the construction of a turning area satisfactory to the City Engineer.
- w. Improve the on- and off-lamps and connector roads for Westchester Parkway and Lincoln Boulevard satisfactory to the City Engineer and the Department of Transportation.

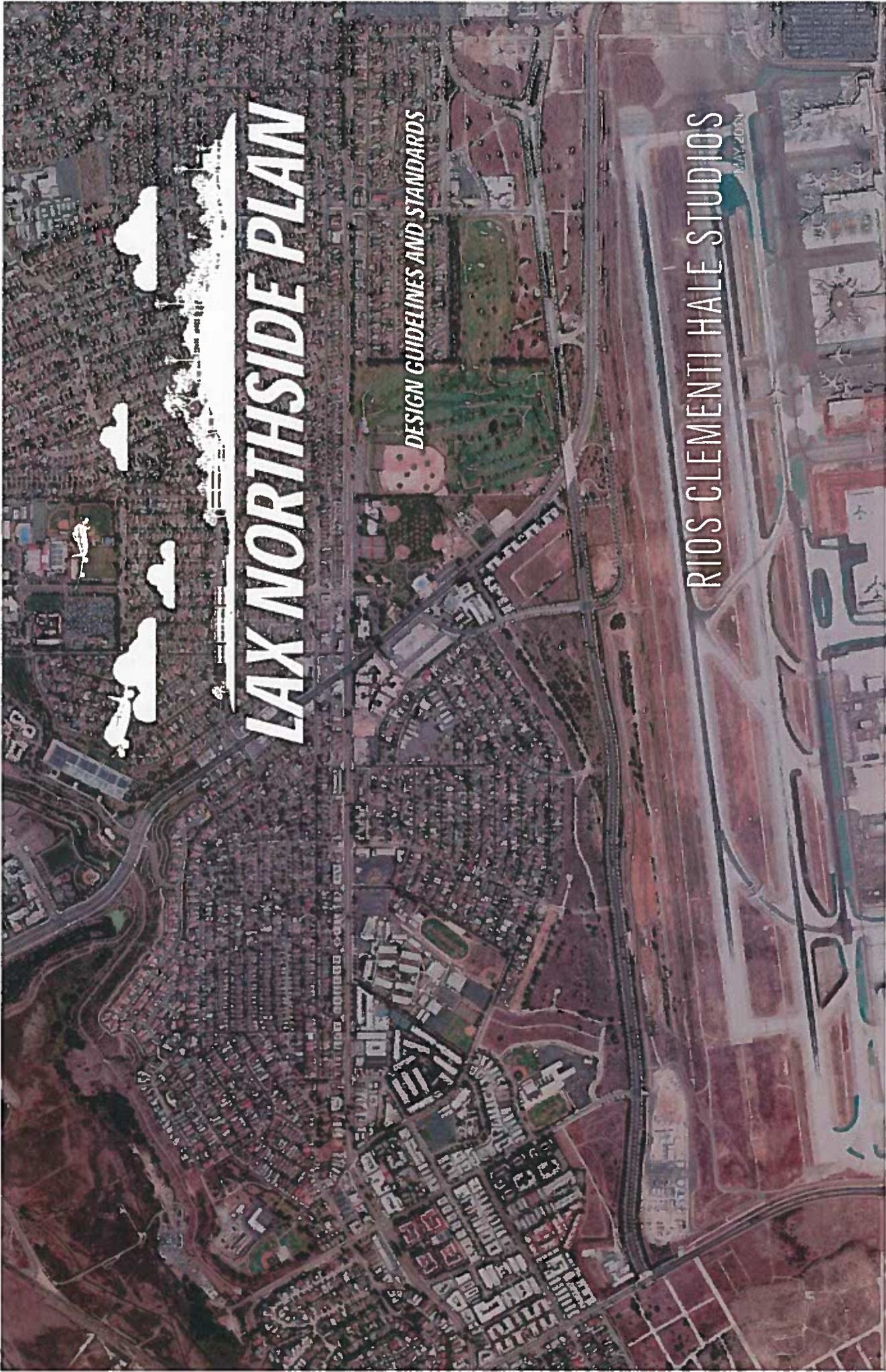
Attachment C:

LAX Northside Design Guidelines and Standards

4. Introduction

The purpose of this report is to provide a comprehensive overview of the current state of the field and to identify key areas for future research.

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LAX NORTHSIDE PLAN

DESIGN GUIDELINES AND STANDARDS

RIOS CLEMENTI HALE STUDIOS

MAY 2021

May 2014

Vision and Design provided by :

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Edited and prepared with :



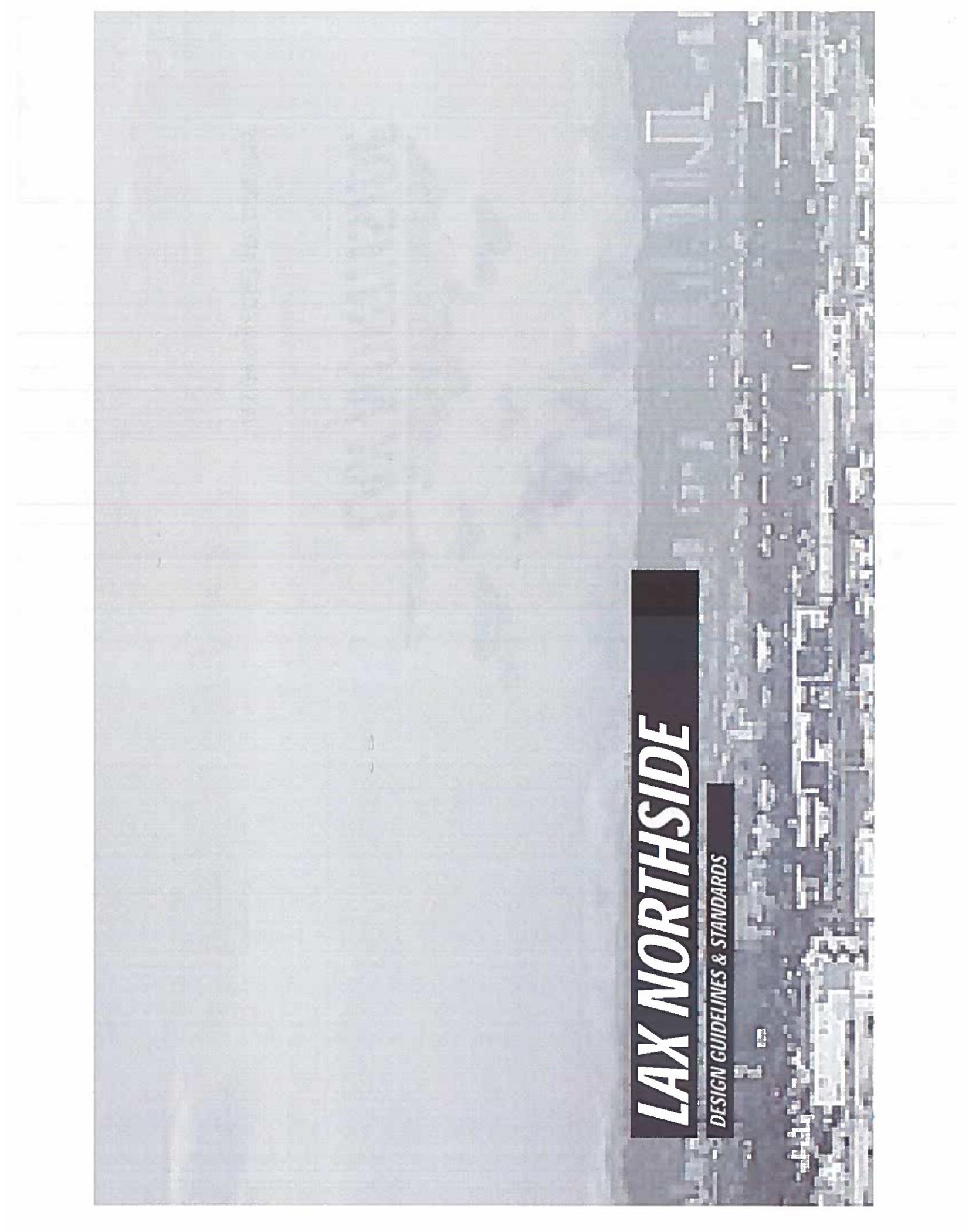
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DESIGN GUIDELINES AND STANDARDS

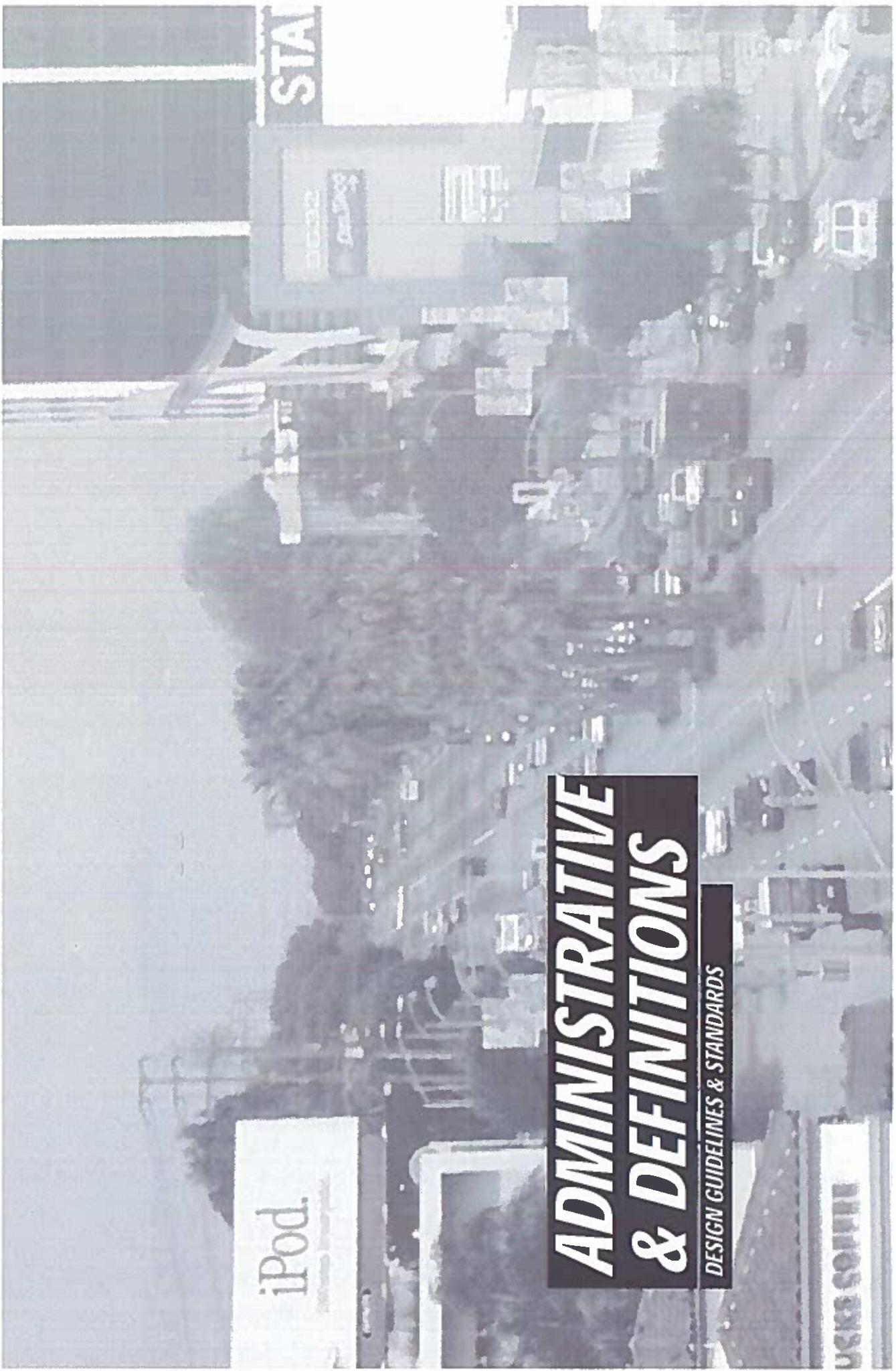
An aerial photograph of a city skyline, likely Los Angeles, viewed from across a large body of water. The city buildings are visible in the distance, and the water occupies the foreground. The text is overlaid on the image.

LAX NORTHSIDE

DESIGN GUIDELINES & STANDARDS

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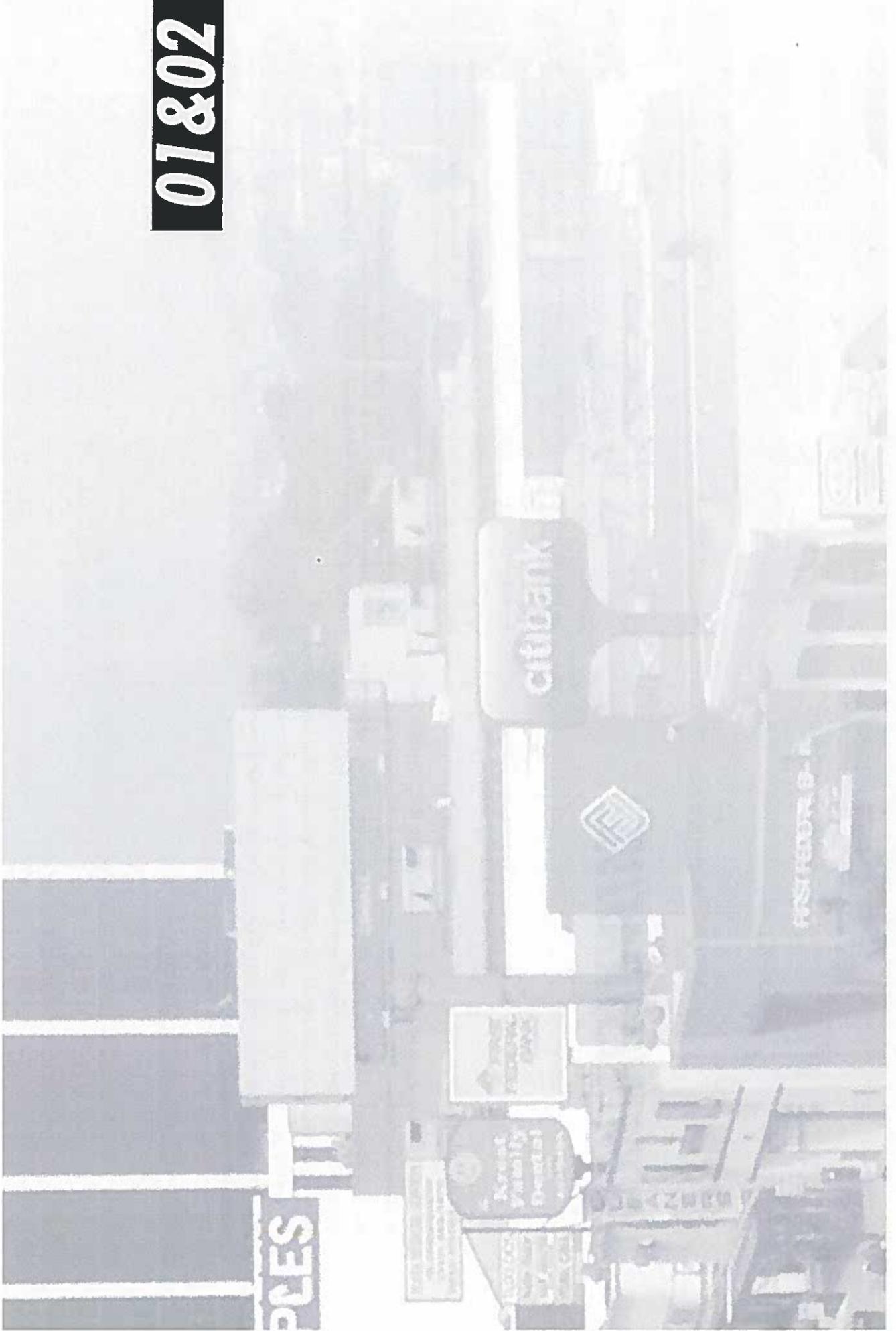
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ADMINISTRATIVE & DEFINITIONS

DESIGN GUIDELINES & STANDARDS

01&02



01 ADMINISTRATIVE

OVERVIEW

The LAX Northside Design Guidelines and Standards are intended to supplement Section 11 LAX Northside Sub-Area of the LAX Specific Plan with detailed development guidelines and standards for the LAX Northside. The design guidelines and standards are organized to address three geographic districts: the LAX Northside Center District, the LAX Northside Campus District and the LAX Northside Airport Support District. These districts are outlined in Figure 1.1.

The context for the LAX Northside project area is described in Chapter 3, providing insight into both the regional and local influences on the LAX Northside project area. The vision and overall design intent for the project area is articulated in Chapter 4, which includes graphics and illustrations intended to help facilitate the understanding and implementation of the guidelines and standards.

Chapters 5 through 9 contain guidelines and standards as follows: Urban Design Guidelines, Architectural Design Guidelines, Landscape Design Guidelines, Paseo and Signage and Graphics. The guidelines and standards are formatted in a checklist manner to help facilitate use, provide clarity, and standardize the review process.

The overall purpose of the Los Angeles International Airport (LAX) Northside Design Guidelines and Standards is to provide a framework for appropriately scaled development that is consistent with airport needs and neighborhood conditions. These guidelines and standards have taken into consideration Federal Aviation Administration (FAA) regulations, market conditions, sustainability, and the context of the LAX Northside, which includes an active airfield and residential neighborhoods. These design guidelines and standards will direct land uses and address issues of urban design, architecture, landscape materials and design, pedestrian infrastructures, and signage.

FIGURE 01.1
DISTRICT MAP



01.2 RELATIONSHIP to OTHER DOCUMENTS

The LAX Northside is located in the City of Los Angeles, and development of the site is governed by the City's land use policy and related ordinances. The City of Los Angeles Citywide General Plan Framework defines the City's long-range comprehensive growth strategy, and sets forth policies, goals, and objectives to guide land use regulations for Community Plans. The City of Los Angeles General Plan Land Use Element consists of 35 local Community Plans that set forth land use regulations and zoning for specific areas. The LAX Plan is the City of Los Angeles General Plan Land Use Element for LAX, including the LAX Northside. The LAX Plan is intended to promote an arrangement of airport uses that encourages and contributes to the modernization of the Airport in an orderly and flexible manner within the context of the City and region. Finally, the LAX Specific Plan implements the goals and objectives of the LAX Plan through zoning and development standards, and contains specific provisions for the LAX Northside. This document supplements the LAX Specific Plan with design guidelines and standards for the LAX Northside.

Relevant documents and ordinances that have established the development pattern for the the LAX Northside are summarized here:

- 1984 Zoning Ordinances (159, 526, 169, 254, and 169, 768) and Final Tract Map No. 34836; Permit up to 4.5 million square feet of commercial development. These entitlements were incorporated into the 2004 LAX Specific Plan.
- 2004 LAX Plan. Provides the long-range land use policy framework and serves as the land use element for Los Angeles' General Plan for LAX, including the LAX Northside. The currently adopted LAX Plan land use designation for the Project site is LAX Northside. This land use designation provides for the development of a variety of uses that are consistent with airport needs and neighborhood conditions. The primary allowable uses within the LAX Northside include: commercial development; office; light industrial; research and development; hotel and conference facilities; retail and restaurant uses; schools and community facilities; open space; bicycle paths; and greenway buffers.
- 2004 LAX Master Plan: Sets forth the comprehensive development program for LAX properties, including runway and taxiway system modernization, redevelopment of terminal areas, airport access improvement, and passenger safety, security, and convenience enhancements.

- 2004 LAX Specific Plan: Implements the goals and objectives of the LAX Plan through zoning and development standards, and contains specific provisions for the Project site. The currently adopted LAX Specific Plan zoning for the Project site is Los Angeles International Airport Northside Zone (LAX-N Zone). The purpose of the LAX-N Zone is to provide for development that is consistent with airport needs and neighborhood conditions.
- Los Angeles World Airports (LAWA) Sustainability Airport Planning, Design, and Construction Guidelines. Provides a comprehensive set of performance standards applicable to airports that can be utilized to integrate sustainable practices into airport planning, design, and construction for both on-airport and off-airport facilities, including the LAX Northside.
- Los Angeles Municipal Code: Includes regulatory provisions for development within the City of Los Angeles, including building regulations, noise standards, specific plans, and zoning.
- Coastal Transportation Corridor Specific Plan: Regulates phased development of land uses to ensure that transportation infrastructure can accommodate uses, and establishes programs and fees to reduce trips, encourage public transportation, and fund transportation improvements.
- Coastal Bluffs Specific Plan: Intended to protect, maintain, enhance and where feasible, restore the overall quality of the coastal environment and its natural and cultural resources through specific development and environmental regulations tailored to the particular conditions and circumstances of the Playa del Rey Bluffs, located northwest of the LAX Northside.
- Federal Aviation Administration Guidelines: Several Federal regulations intended to guide and control aviation noise, building heights, and uses adjacent to airports apply to the LAX Northside. These include, but are not limited to, Federal Aviation Regulation (FAR) Part 150, Airport Noise Compatibility Planning and FAR Part 77, Obstructions to Navigation.

- Los Angeles County Airport Land Use Plan; Intended to protect the public health, safety, and welfare by ensuring orderly expansion of airports and the adoption of land use measures that minimize the public's exposure to excessive noise and safety hazards within areas around public airports.

01.3 DESIGN REVIEW and IMPLEMENTATION

AUTHORITY

The Executive Director shall have the authority to review each Project for compliance with the requirements of this Specific Plan, and if in compliance, to issue a Project Plan Compliance.

Rights Granted Under Project Plan Compliance. The issuance of a Project Plan Compliance indicates compliance with the LAMC Section 12.16.2, but does not in any way indicate compliance with other applicable provisions of the LAMC Chapter 1 (Planning and Zoning Code), nor with Chapter IX (Building Code.)

PROCEDURES

Applications for Project Plan Compliance shall be filed with the Executive Director, upon such forms and accompanied by the applicable fees, a site plan drawn to scale and other information prescribed by the Executive Director. The property owner shall verify the application and submit a title report indicating the record owner at the time of submission of the application and that the Applicant is either the property owner of the agent of the property owner.

02 DEFINITIONS

Ancillary Building

A building or structure that is incidental and related to the primary permitted use building.

Area

A geographic division of the LAX Northside Districts as depicted in Figure 01.1 of these design guidelines and standards, which includes Areas 1, 2 (A, B, C, D, and E), 3, 4, 5, 6, 7, 8, 9, 10, 11, 12A East, 12A West, 12B, and 13.

Area Designation Sign

Signs that are ancillary to Tenant Identification Signs that provide wayfinding, or ascribe use for both vehicular and pedestrian convenience.

Bioswales

Bioswales are landscape elements designed to remove silt and pollution from surface runoff water, consisting of a swaled drainage course with gently sloping sides of less than six percent and filled with vegetation.

Boilard

A vertical free-standing short post used as a barrier to traffic.

Building Frontage

The projection of the building walls upon the street level for street frontage.

Building Setback

The distance from a fixed point, such as a property line or existing street right-of-way, that prohibits the placement of buildings but allows such activities as landscapes and parking.

Channel Letters

Individually cut letters, numbers or figures, illuminated or non-illuminated, affixed to a building or structure.

The following define terminology and words associated with the design guidelines and standards.

Address Sign

Any sign that is used to communicate the numerical or alphanumerical identification of a given business or development's address on a street.

Airport Support District

The southernmost district in the LAX Northside that is defined by the following boundary: Westchester Parkway to the North and the active airfield to the South. This district includes Areas 4, 5, 6, 7, 8, 9 and 10.

Airport Surveillance Radar (ASR)

Radar system used to detect and display aircraft positions in the terminal area. The current ASR locations are in Parcel 9 in the southwest corner along McConnell Avenue between Westchester Parkway and Lincoln Boulevard, and in parcel 12B in the southwest corner along Westchester Parkway.

District

A geographic division of the LAX Northside as depicted in Figure 01.1 of these design guidelines and standards, which includes the LAX Northside Airport Support, Campus, and Center Districts.

Floor Area

The area in square feet confined within the exterior walls of a building, but not including the area of the following: exterior walls, stairways, shafts, rooms housing building-operating equipment or machinery, parking areas with associated driveways and ramps, space for the landing and storage of helicopters, and basement storage areas.

Identification Sign (Permanent Identification Sign)

A wall sign that is limited to a company logo, generic type of business, or the name of a business or building.

Landscape Buffer

A secured landscaped area that is defined by a certain dimension taken from a fixed point, such as a property line, that prohibits the placement of buildings, parking or the entry of the general public.

Landscape Setback

A landscaped area that is defined by a certain dimension from a fixed point, such as the edge of a building or a property line, that requires the introduction of plant materials to help differentiate boundaries within the development. A Landscape Setback does not prohibit the placement of parking or the entry of the general public.

LAX Northside

The LAX Northside Sub-Area as depicted on Map 2 of the LAX Specific and Figure 01.1 of these design guidelines and standards.

LAX Northside Campus District

The northwesternmost district in the LAX Northside that is defined by the following boundary: Lincoln Boulevard on the East, 91st Street to the north, Westchester Parkway to the South and Pershing Drive on the West. This district includes Parcels 1, 2 and 3.

LAX Northside Center District

The northeasternmost district in the LAX Northside that is defined by the following boundary: the intersections of

Saputovac Wayway, La Tijera Boulevard and Westchester Parkway on the east, and Lincoln Boulevard on the West. This district includes Parcels 11, 12A East, 12A West and 13.

Low Impact Design (LID)

Environmentally friendly, sustainable practices that reduce our impact on the environment and conserve natural resources through the focus on a site's hydrological capacity to ensure that post-development runoff does not exceed pre-development runoff.

Major Intersection / Entrance

Characterized by a signalized intersection that allows for both right and left turn entry.

Minor Intersection / Entrance

Characterized as a right turn only entry way into developments that does not require a signalized entrance way

Passo

Permits an open space pedestrian pathway that is open to the sky, publicly accessible, and connects an open space or right of way. Vehicular traffic shall be prohibited within any passo.

Project

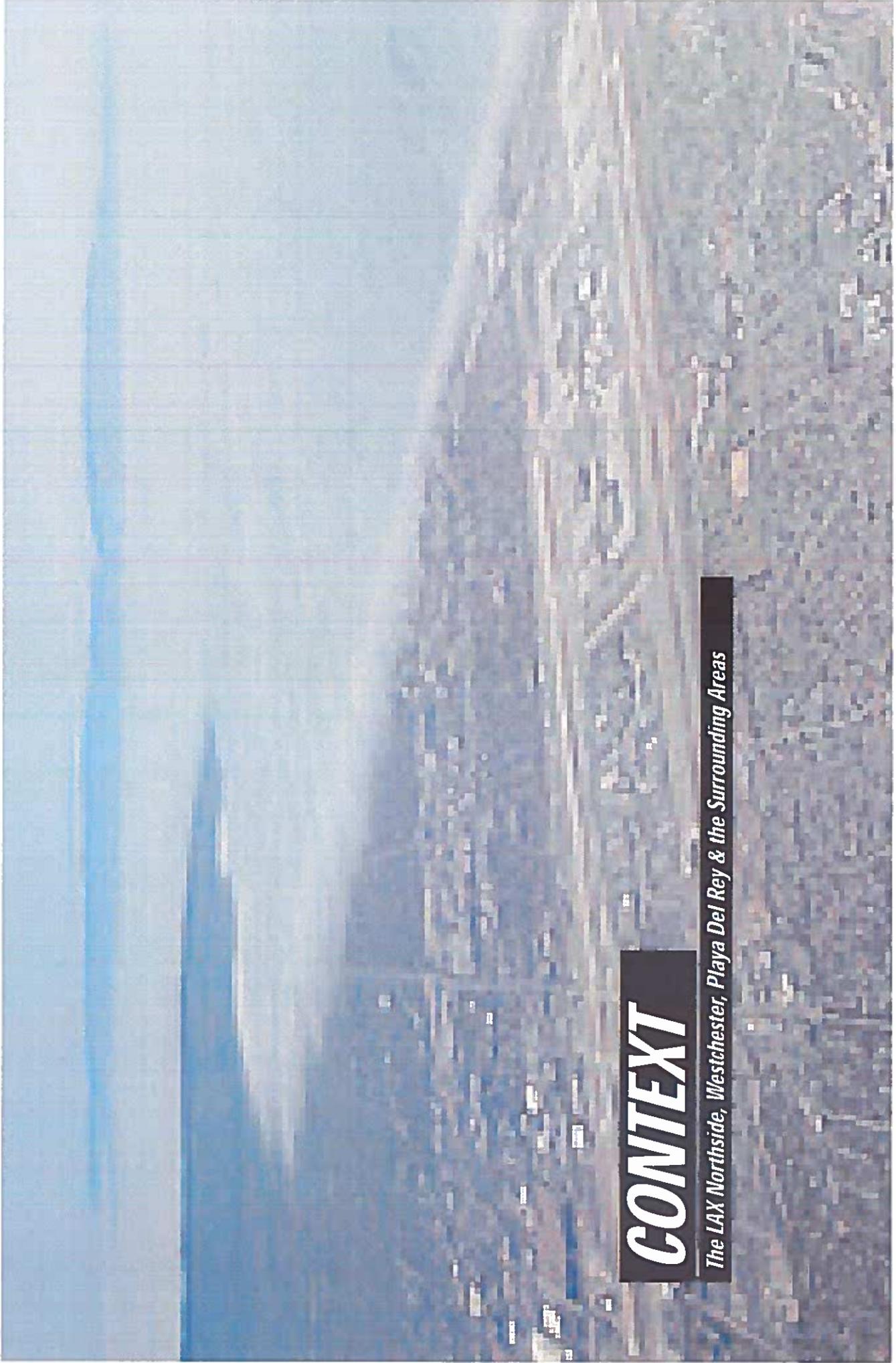
The construction, erection, addition to, or structural alteration of any building or structure, or use of building or land, or change of use of building or land, subject to the exclusions in Section 5 of the LAX Specific Plan

Sub-Area

The primary geographic division of the LAX Specific Plan, which includes the Airport Airside Sub-Area, Airport Landside Sub-Area, and LAX Airport Northside Sub-Area, as depicted on Map 2 of the LAX Specific Plan.

Temporary Sign

Any sign that is to be maintained for no longer than 180 days, including paper signs and other signs that are not permanently affixed to the ground or building. Temporary signs include, but are not limited to construction related signage, lease signs, flags, banners and pennants.



CONTEXT

The LAX Northside, Westchester, Playa Del Rey & the Surrounding Areas



03 CONTEXT

The LAX Northside is comprised of approximately 340 acres within Los Angeles, located approximately 15 miles southwest of downtown Los Angeles. The LAX Northside vicinity includes the Westchester community of Los Angeles to the immediate north, the City of El Segundo and unincorporated community of Del Aire to the south of LAX, the City of Inglewood and unincorporated community of Lennox to the east of LAX, the Los Angeles community of Playa del Rey to the immediate west, and the Pacific Ocean further west. Major surrounding regional landmarks include Loyola Marymount University to the north, Dockweiler Beach State Park to the west, LAX to the south, and Interstate 405 to the east.

The LAX Northside is generally bounded by Sepulveda Westway and Sepulveda Boulevard to the east, LAX to the south, South Pershing Drive to the west, and generally 91st Street, Manchester Avenue, and 88th Street to the north. Westchester Parkway runs from east to west through the LAX Northside. Westchester Parkway was completed

in 1993 in anticipation of up to 4.5 million square feet of development and as a requirement of the original entitlements for the LAX Northside.

The LAX Northside vicinity includes a diverse mix of low- to medium-density commercial, residential, and industrial development. To the north of the LAX Northside are single- and multi-family residences in Westchester and Playa del Rey. Further northeast are the Playa del Rey Bluffs. Directly to the south are airfields, terminals, and LAX airport support uses. Retail and commercial uses are located to the east, primarily along Sepulveda Boulevard. The residential community of Playa del Rey is located to the west, and further west are beaches and the Pacific Ocean.

Open space, educational, public, and community-serving uses are also located near the LAX Northside and include Otis College of Art and Design, Westchester Recreation Center, St. Bernard High School, Westchester Senior High School, Paseo del Rey Elementary School, St. Anastasia School, Loyola Village Elementary, Visitation School, Playa del Rey Care and Rehabilitation Center, several churches, and Carl E. Nielsen Park. The Westchester Golf Course exists within the LAX Northside and Westchester Parkway is used for biking, walking, and jogging.



FIGURE 6L.1 // LOS ANGELES INTERNATIONAL AIRPORT (LAX)
The distinctly LAX Gateway Pylons located at Century Boulevard and Sepulveda Boulevard.



FIGURE 6L.2 // WESTCHESTER NEIGHBORHOOD
Typical single family houses and tree-lined streets throughout the Westchester neighborhoods.

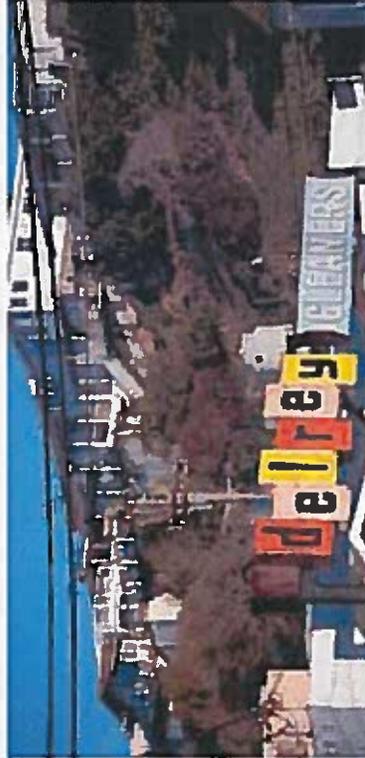
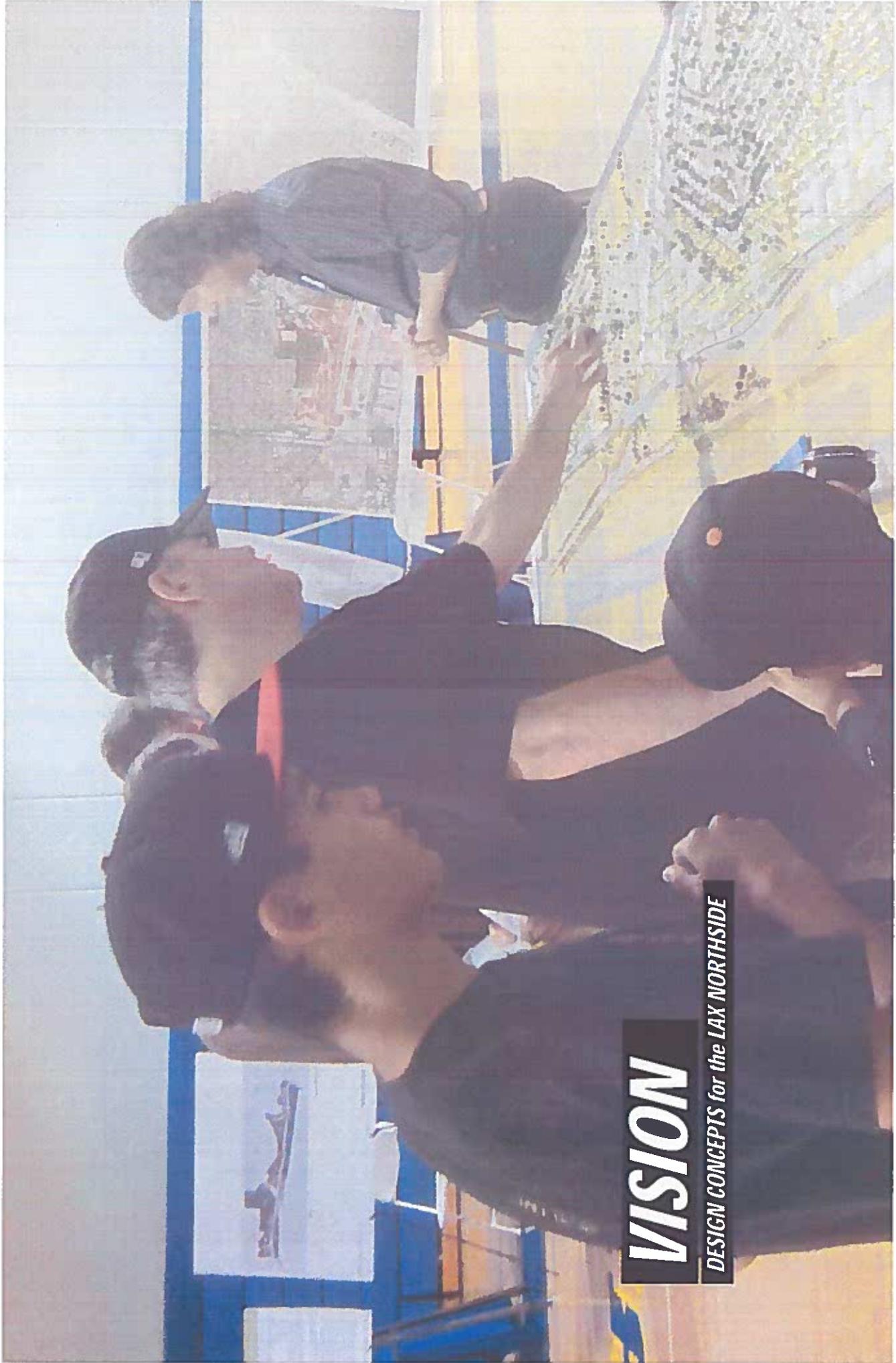


FIGURE 6L.3 // PLAYA DEL REY
View of the bluffs and residences that characterize Playa Del Rey.



FIGURE 6L.4 // WESTCHESTER BUSINESS DISTRICT
Aerial view of Sepulveda Boulevard looking north at Westchester Parkway.



VISION

DESIGN CONCEPTS for the LAX NORTHSIDE



04 VISION

to preserve and enhance an already vibrant recreation culture, and (3) implementing an appropriate landscape palette that promotes sustainability and livability in overt and specific ways.

04.1 RESHAPING THE LAND AS A BUFFER

The LAX Northside Design Guidelines and Standards seek to ensure that all future development is compatible with the safe operation of aircraft at LAX today and into the future. Building heights and grading strategies accomplish two primary goals. The first goal is to ensure that building heights comply with applicable FAA restrictions for the safety of individuals adjacent to an active airfield. The second goal is to better connect the LAX Northside with Westchester Parkway, while buffering and creating compatible transitions with existing residences to the north.

The LAX Northside Design Guidelines and Standards provide a grading strategy that will better align future development along Westchester Parkway, provide marketable building footprints, and lower the grade of development on the LAX Northside relative to existing residential neighborhoods to the north. Grading the LAX Northside achieves the vision of the area as a buffer between LAX and adjacent uses and ensures that future development is sensitive to the surrounding context and compatible with aircraft operations.

04.2 THE LAX NORTHSIDE PASEO

Westchester Parkway currently provides a substantial pedestrian infrastructure that is capable of accommodating both active and passive forms of recreation. The main unifying design feature of the LAX Northside will be a pedestrian accessible paseo that connects all areas of the LAX Northside Center District and Campus District from east to west along Westchester Parkway. Starting in Area 11, adjacent to the existing Sepulveda Business District and downtown Westchester, the paseo will continue west along Westchester Parkway until it reaches Pinney Drive, where it will connect with an existing recreation path to the beach. This nearly three (3) mile stretch will

The LAX Northside Design Guidelines and Standards were developed through an extensive stakeholder engagement process that resulted in a comprehensive vision for the future of the LAX Northside. These guidelines and standards seek to address concerns about preserving sight lines and neighborhood privacy, controlling light spillage, creating better buffers between functioning active airfields and adjacent neighborhoods, controlling wildlife near airfields, and improving aesthetics and landscape in the LAX Northside.

Input was sought from a variety of community groups, resident organizations, business associations, environmental leaders, recreation advocates, the development community, and regulatory agencies, including the FAA and other stakeholders. The overall vision is composed of three main components: (1) reshaping the topography of the LAX Northside and introducing low-scale development to reinforce the LAX Northside as a buffer area between LAX and surrounding uses, (2) introducing a consolidated pedestrian infrastructure for passive and active recreation

04.3 CREATING AN APPROPRIATE AND SUSTAINABLE LANDSCAPE

accommodate active and passive forms of recreation using a variety of appropriate materials. The paseo will be composed of the existing ten (10) feet of concrete sidewalk intended for walking and a twelve (12) foot wide path of stabilized decomposed granite intended for various forms of recreation.

Continuity of the pedestrian experience will be maintained by minimizing vehicular entries and breaks in the paseo. Pedestrian safety will be promoted with appropriate pedestrian crosswalk signage and specific finish materials to reinforce crossing areas. Streetscape lighting will enhance pedestrian awareness and safety at all hours of the day.

Activity along the paseo will be promoted with the introduction of entry plazas and shared common spaces that complement adjacent land uses. These could include outdoor restaurant patios, additional features such as fountains and green space, potential for a community farmer's market, or plaza spaces that connect buildings to the pedestrian realm along Westchester Parkway. Landscape materials shall be introduced along the paseo that foster a dynamic sense of color and annual bloom, while being resilient enough for the high pedestrian traffic in the area.

Aside from supporting an existing active recreation culture throughout the Westchester Community, the introduction of the paseo will consolidate pedestrian traffic and activity in the LAX Northside away from residential neighborhoods. This critical aspect to the design and location of the paseo reinforces the overall concept of the LAX Northside serving as a buffer between LAX and adjacent neighbors. By focusing pedestrian activity along Westchester Parkway and restricting access from the north and into adjacent neighborhoods, a safe environment will be maintained that does not infringe on the society of the airfield to the south, or the comfort and privacy of the communities to the north.

A primary goal for landscaping at the LAX Northside is to create a sustainable and functional urban landscape that prevents any unnecessary impact on adjacent uses. The LAX Northside Design Guidelines and Standards allow landscaping that unifies the site, is compatible with aircraft operation per FAA guidelines, is sustainable, and responds to the local plant palette. Landscaping at the LAX Northside must not promote the proliferation of wildlife that may disrupt or endanger the functioning of the airfield. As such per consultation with the US Fish and Wildlife Service, plant materials are restricted to those that: 1) have a sparse to moderately dense foliage growth, 2) do not produce fruits or seeds, 3) and do not require extensive maintenance to maintain an appropriate foliage. Additional requirements, such as tree spacing and the prohibition of casting and spraying of seed for sod installation will further reduce the possibility of attracting flocking birds. To help implement these approaches consistently across the LAX Northside, landscape zones have been established to help isolate materials to their appropriate locations.

Landscaping is also designed to advance sustainability. Drought-tolerant plant materials are allowed to preserve water resources. Storm water will be managed in all surface parking lots through the integration of permeable materials in parking spaces and in the medians that separate them. The LAX Northside Design Guidelines and Standards also require the use of blowables to remove silt and pollution from surface runoff water.

Finally, the planting palettes presented as a part of these guidelines seek to respond to native plant communities. Planting materials that are locally native to the surrounding coastal area (in comparison to native to the Southern California region) have been considered, and where appropriate, integrated into the required plant palettes. Additionally, native and non-native species are allowed to provide increased flexibility for project development.

The following drawings, illustrations, and images depict the overall vision for the LAX Northside.



LEGEND

- BOUNDARIES
- PROJECT BOUNDARY
- AIRPORT PROPERTY BOUNDARY
- SECURITY FENCE

PROPOSED LAND USES

- PARK and RECREATION USE
- PARK and RECREATION USE
- LANDSCAPE BUFFER and SECURITY FENCE
- OFFICE, and RESEARCH and DEVELOPMENT USE
- EDUCATION USE
- COMMUNITY and CIVIC USE
- MIXED USE
- AIRPORT USE
- FUTURE PUBLIC TRANSIT LOCATION





FIGURE 04.1.1
ILLUSTRATIVE SITE PLAN

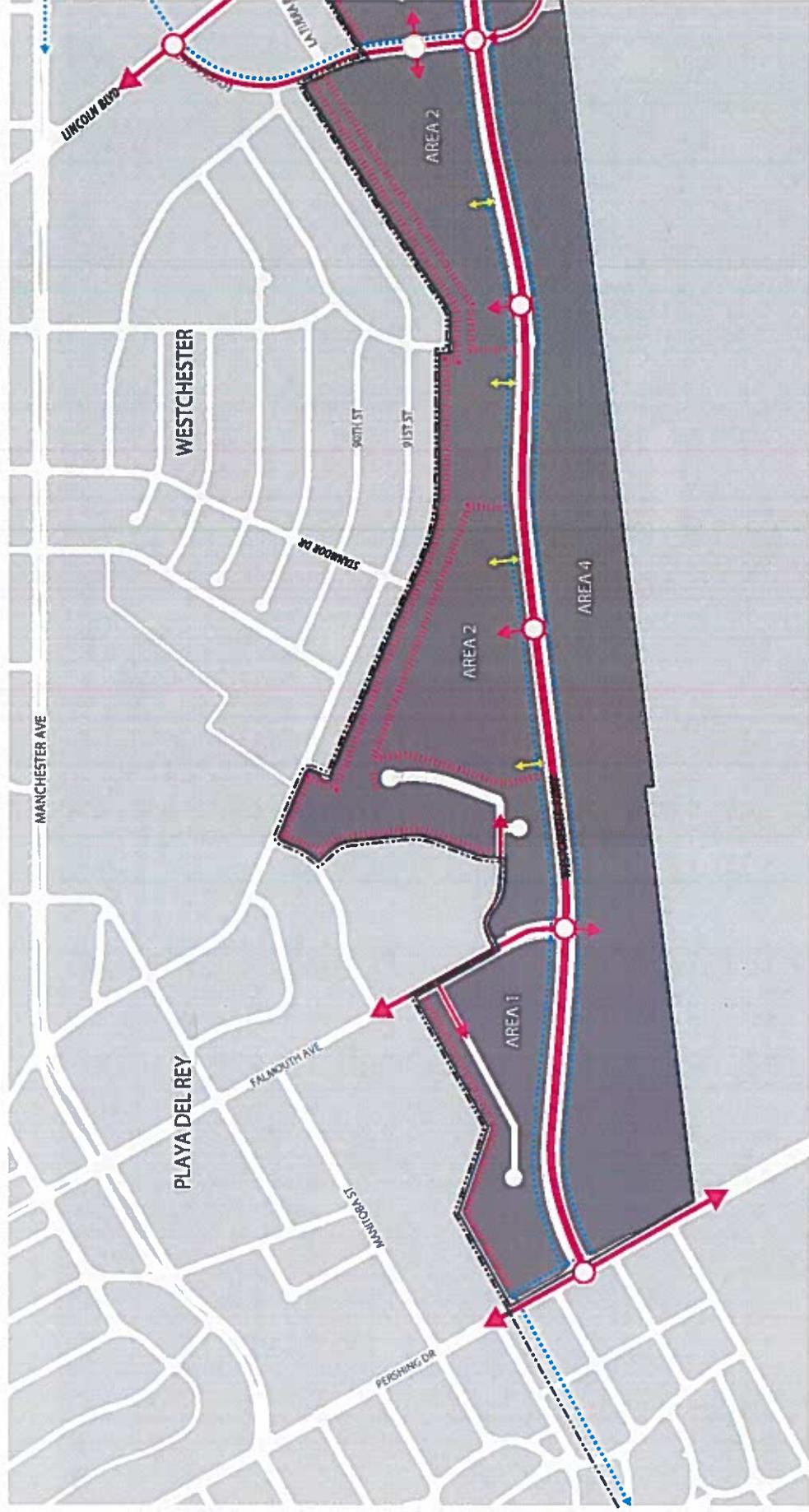
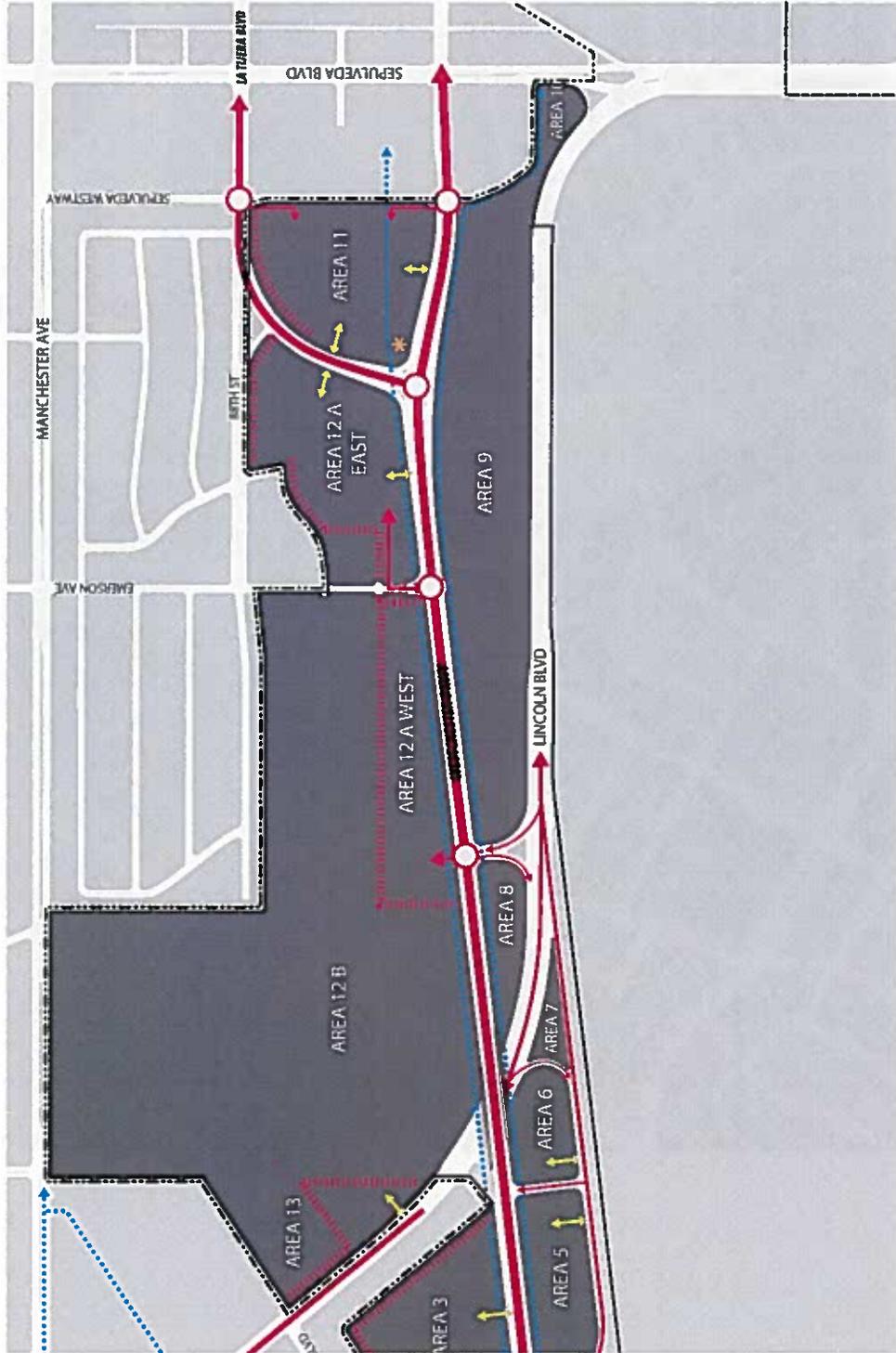


FIGURE 04.12
CIRCULATION & ACCESS

This Figure provides depicts vehicular entrances, access points, and circulation paths.



LEGEND

- BOUNDARIES
 - PROJECT BOUNDARY
 - AIRPORT PROPERTY BOUNDARY
- VEHICULAR ACCESS
 - MAJOR ACCESS
 - MINOR ACCESS
 - PROHIBITED ACCESS
- BIKE and ALTERNATIVE MOVEMENT
 - FUTURE PUBLIC TRANSIT LOCATION
- INTERSECTION TYPES
 - SIGNALIZED INTERSECTION
 - STOP-CONTROLLED INTERSECTION

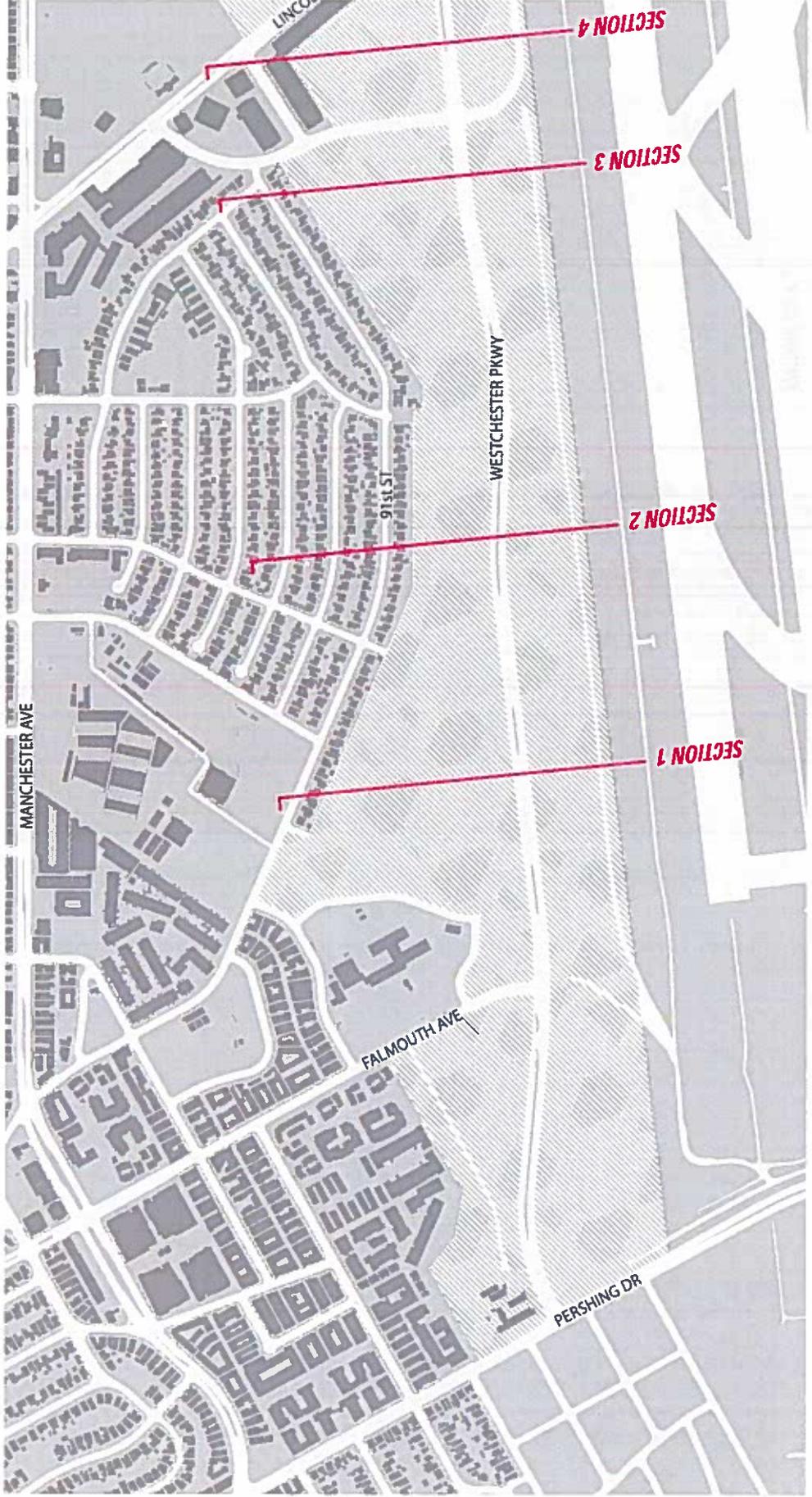


FIGURE 04.1.3
KEY PLAN

This Figure provides information specific to the location of section cuts in the LAX Northside Design Guidelines and Standards.

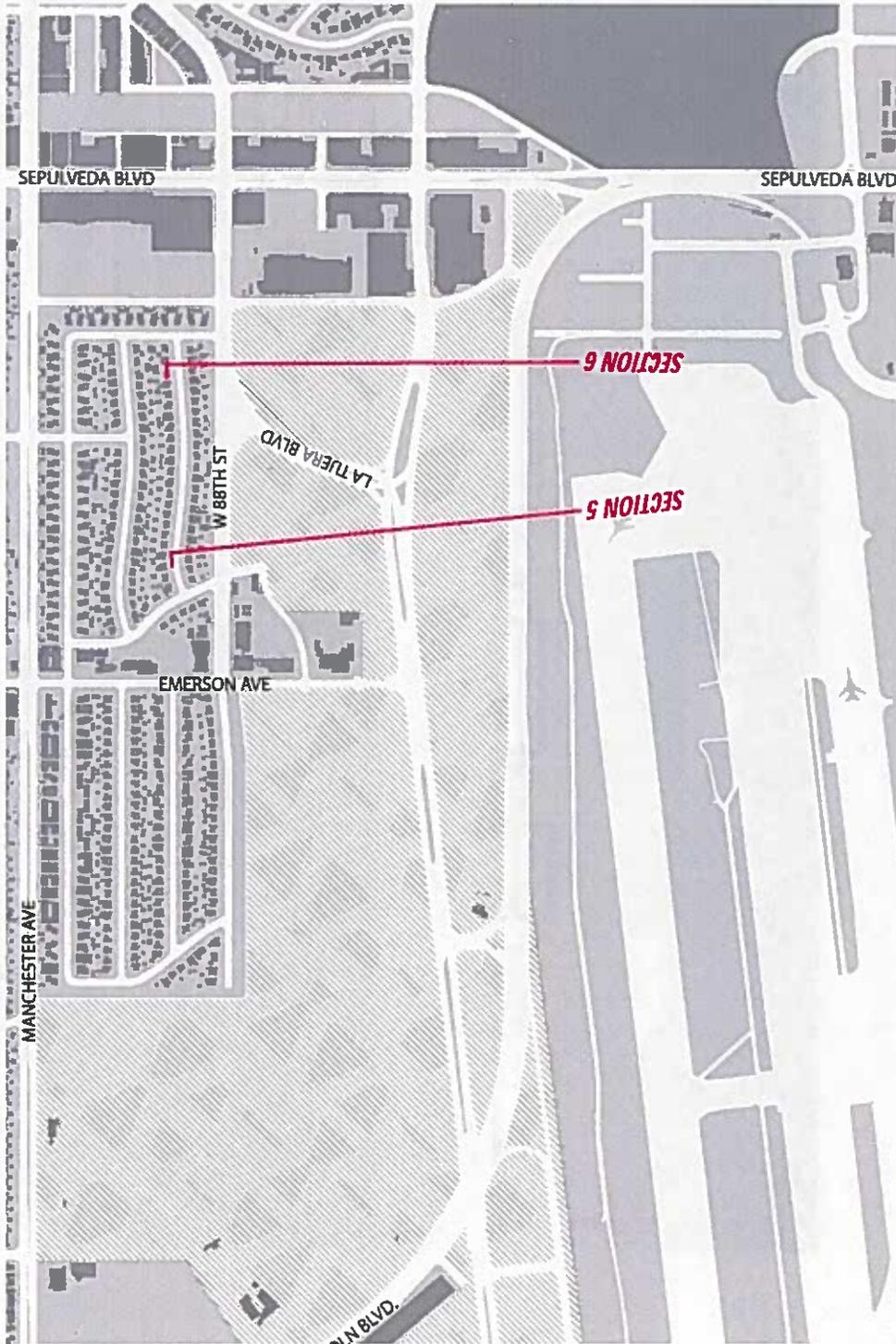


FIGURE 04.1.3-2

SECTION 02 / Along Stanmoor Drive

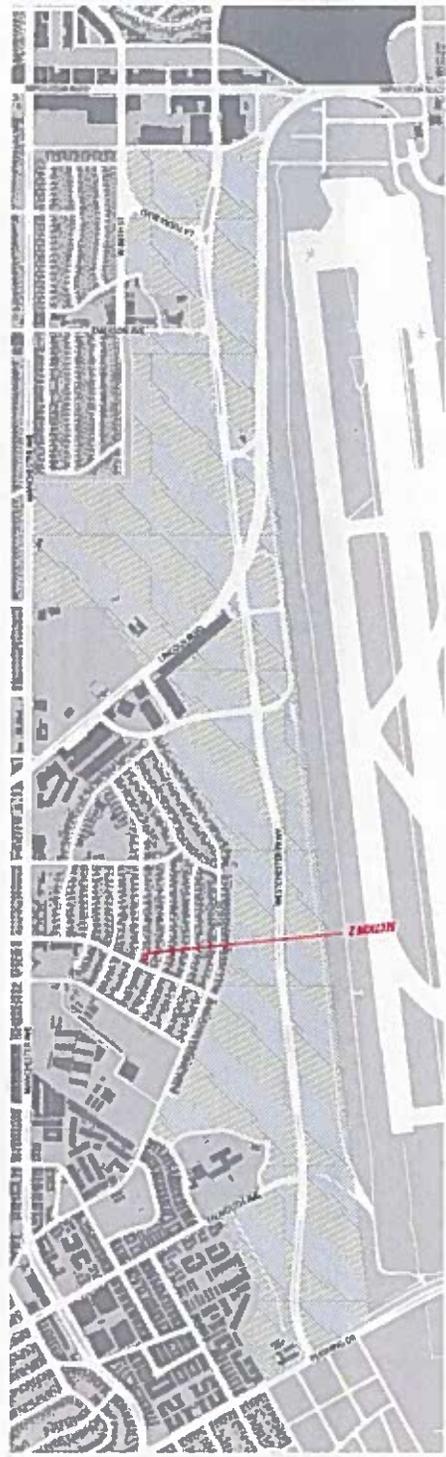
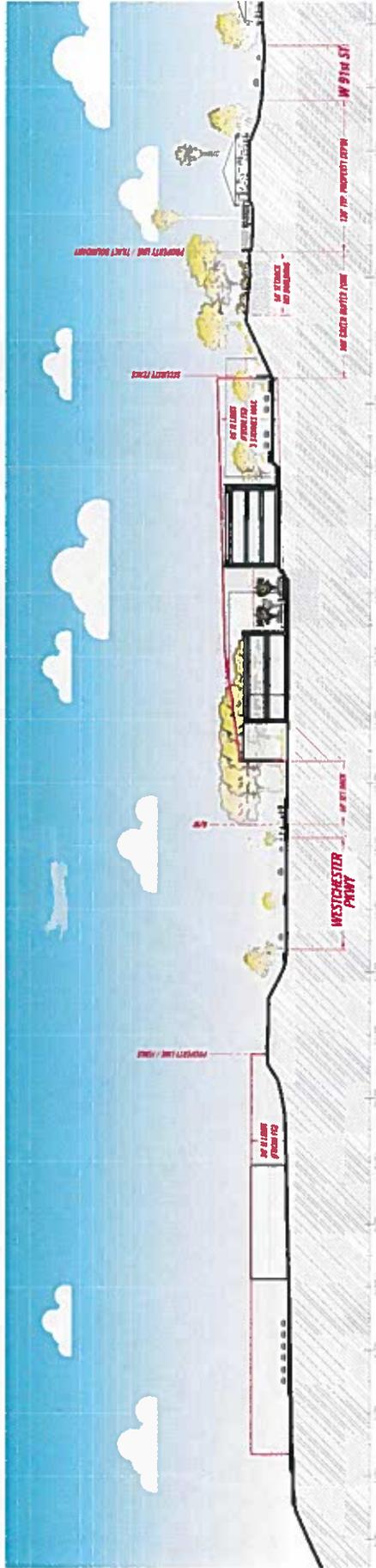


FIGURE 04.1.3-4

SECTION 04 / East of Loyola Boulevard

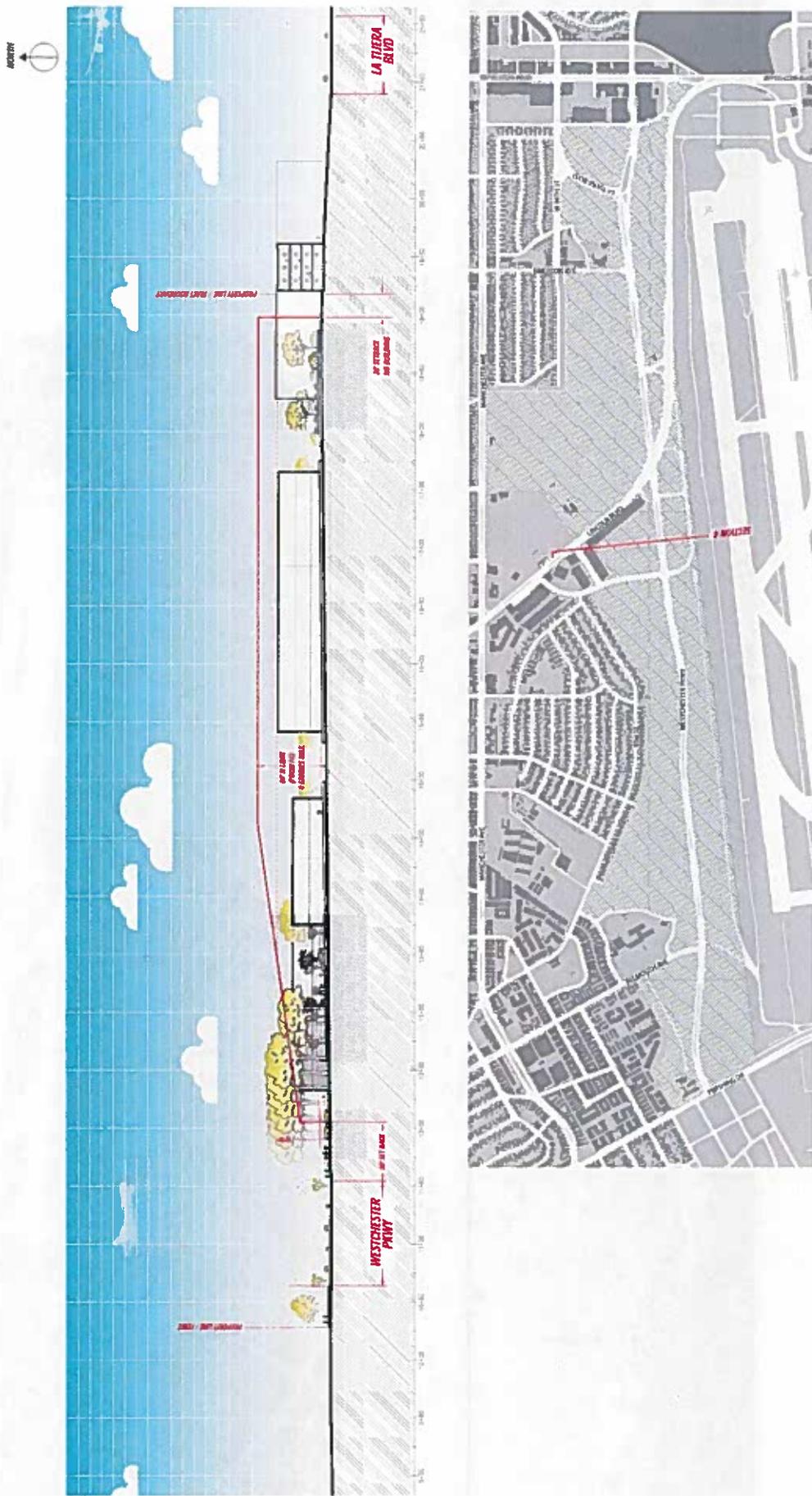


FIGURE 04.1.3-5
SECTION 05 / La Tijera West

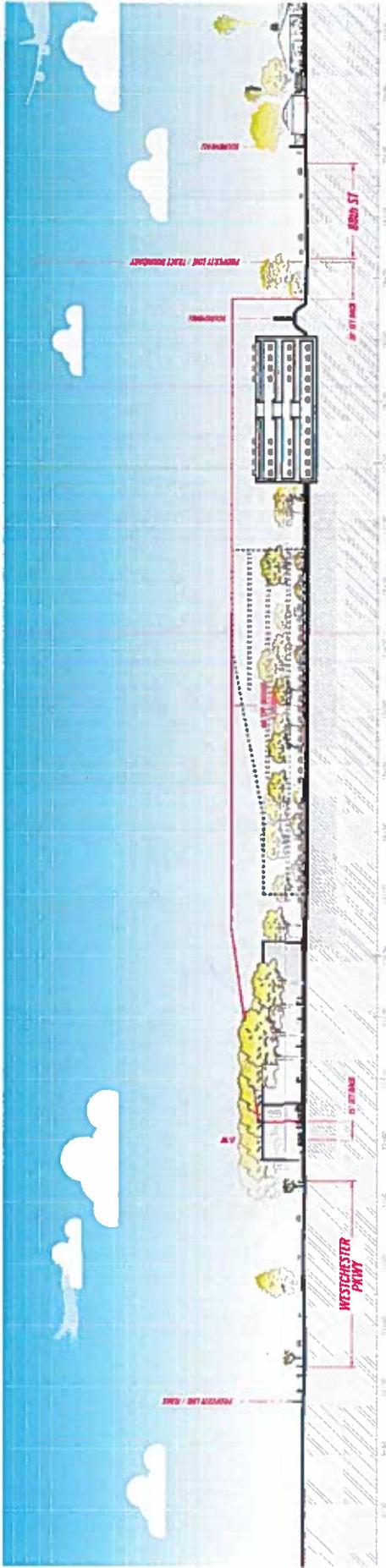
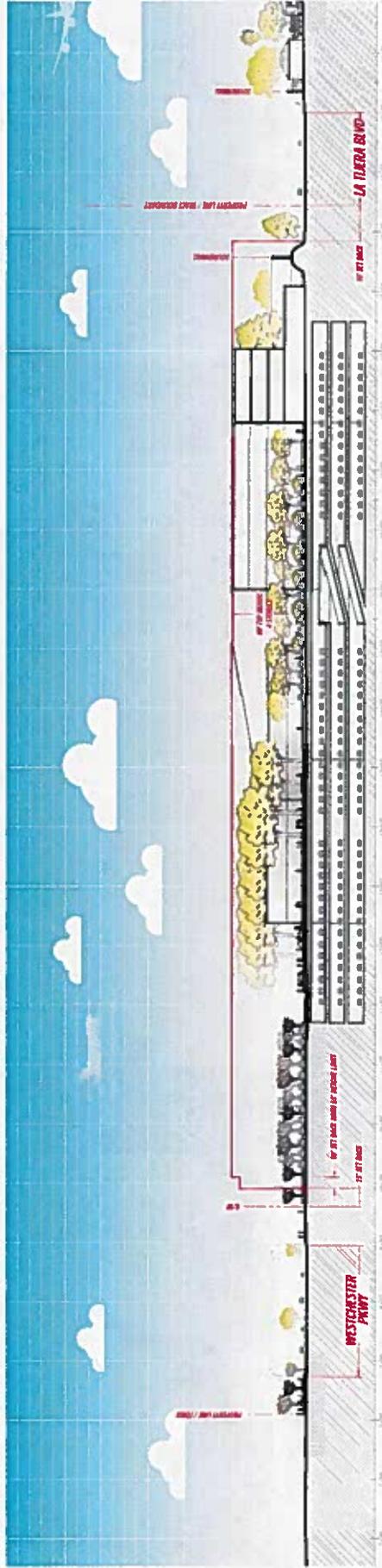


FIGURE 04.1.3-6

SECTION 06 / Sepulveda Westway



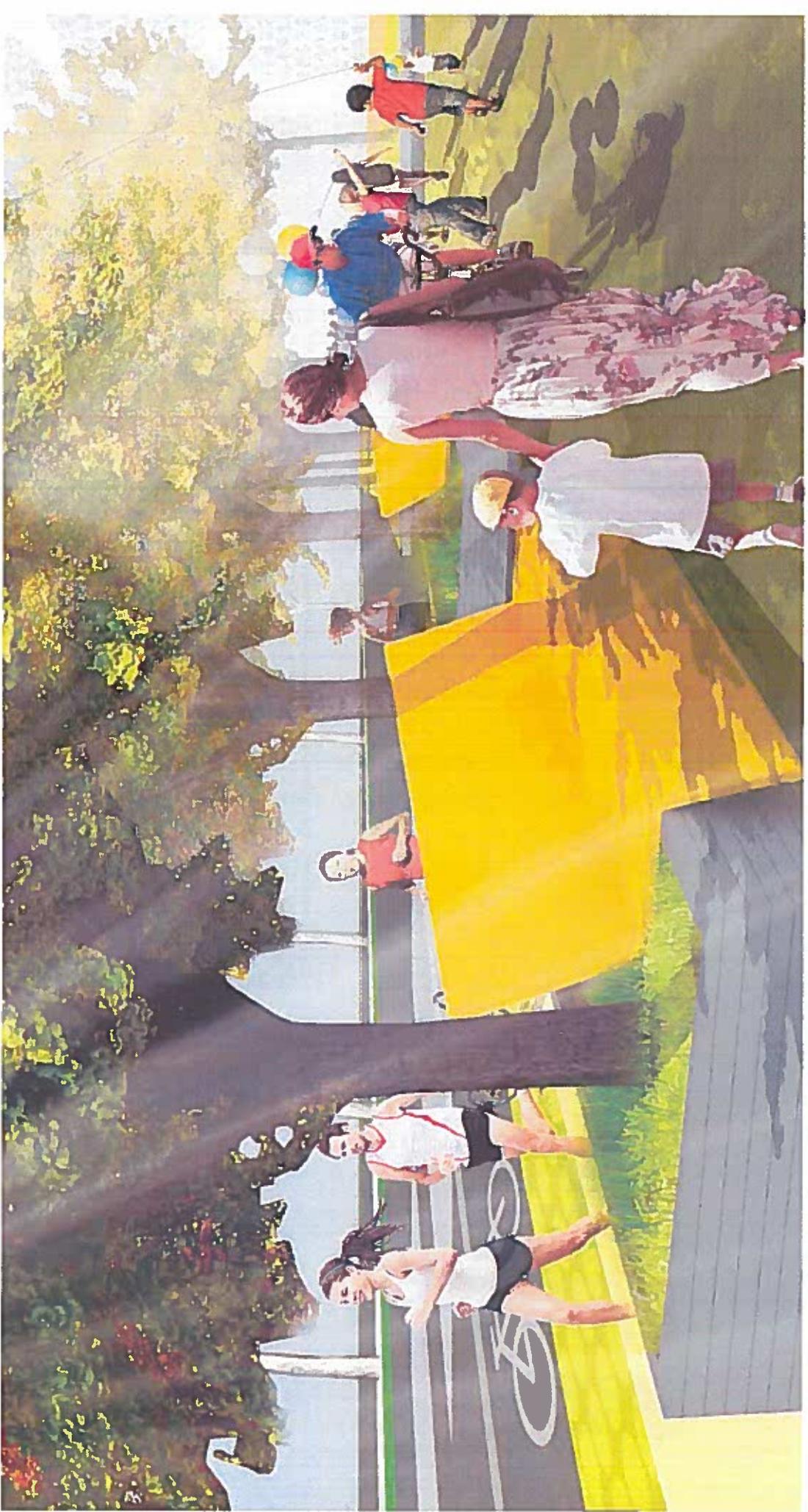


FIGURE 04.2.1

LAX NORTHSIDE CENTER - PASEO

This Figure presents a conceptual rendering of the LAX Northside Center Paseo experience, including appropriate planting materials for the specific location, in addition to pedestrian amenities such as benches, trash receptacles and pathway lighting.

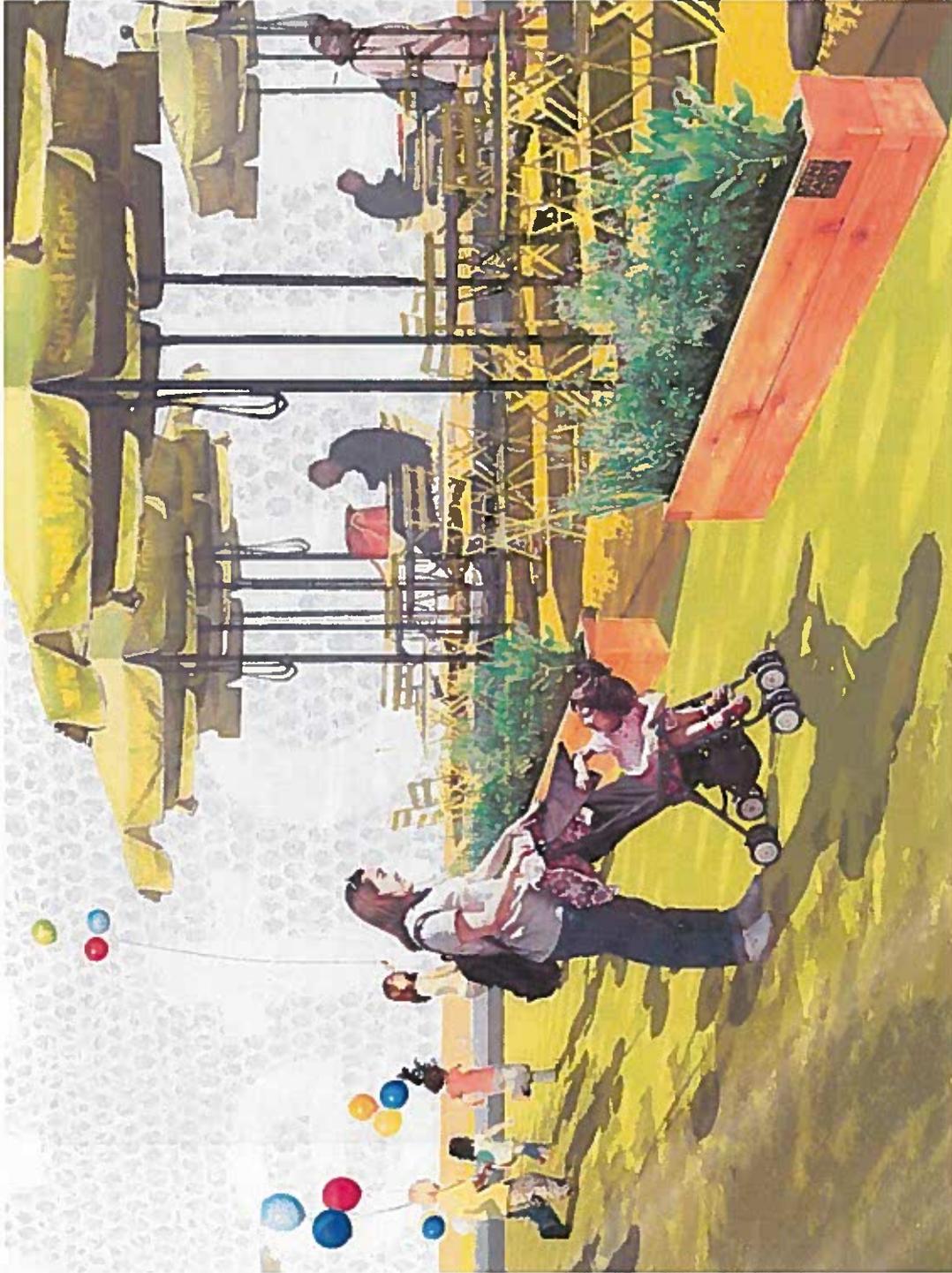
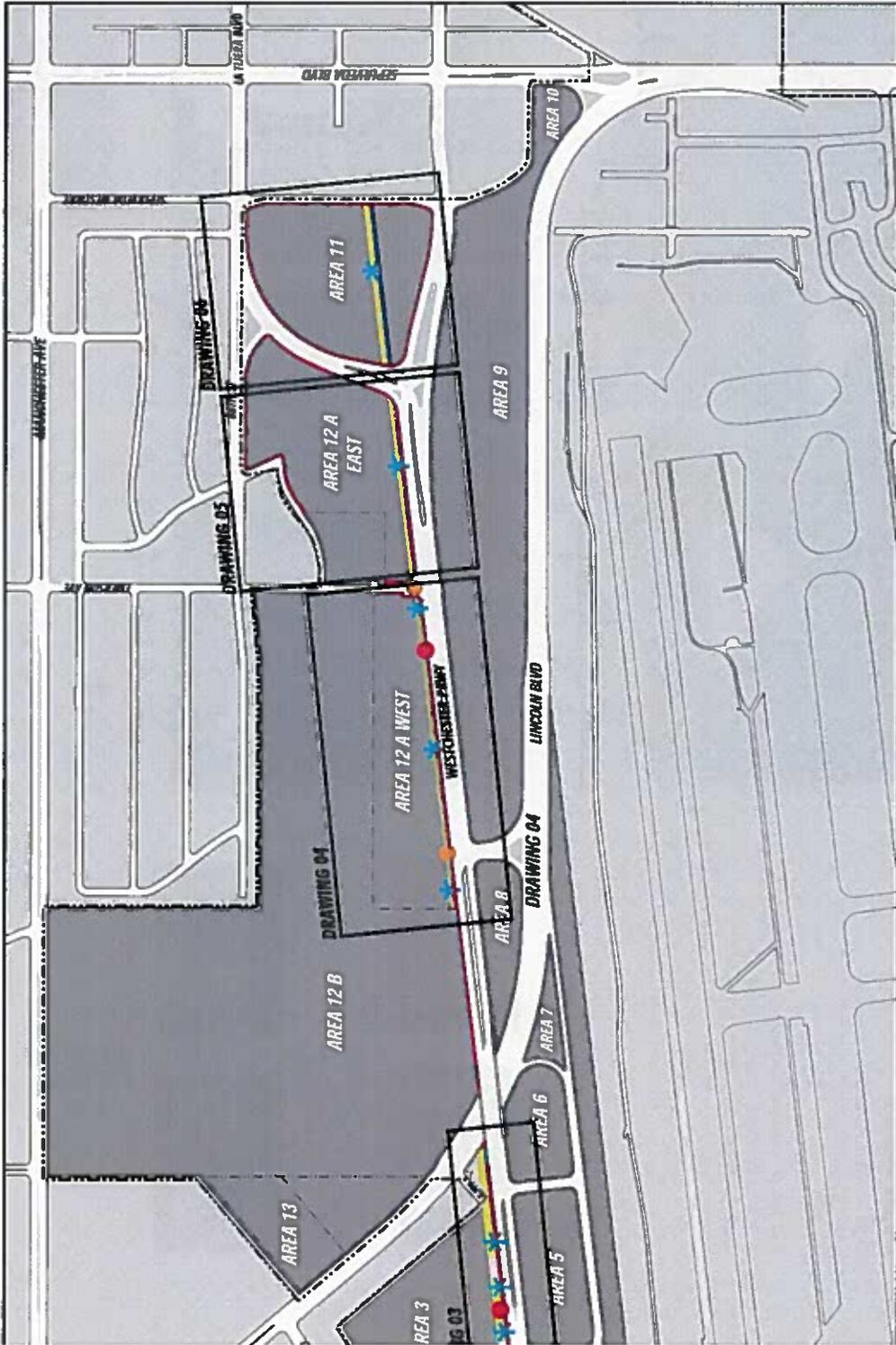


FIGURE 04.2.2
PASEO CONDITIONS & LOCATIONS

The following figure provides information specific to the location and condition types that define the pedestrian accessible paseo in the LAX Northside Plan.



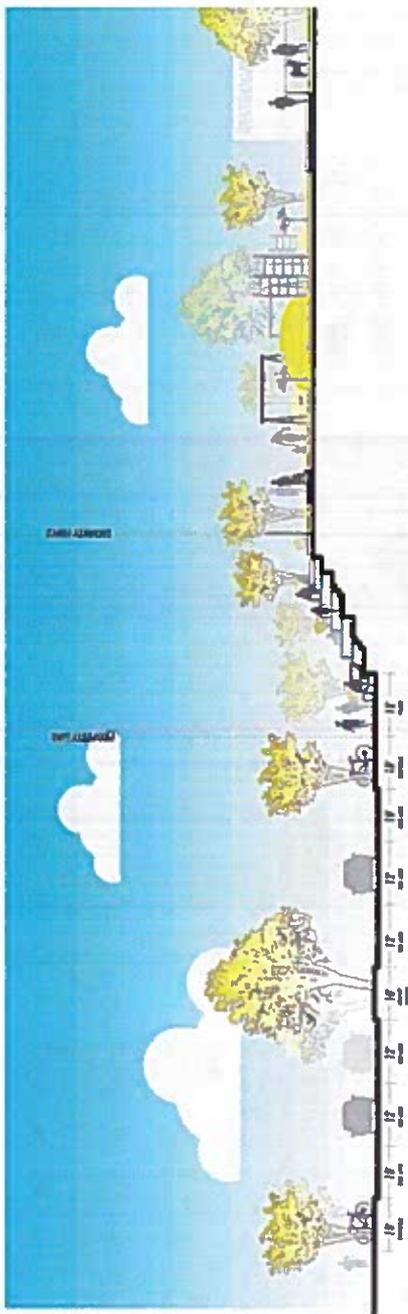
LEGEND

- EXISTING Sidewalk
- ADDITIONAL Sidewalk
TOTAL AREA: 7,720 sq. ft.
- ADDITIONAL Decomposed Granite
TOTAL AREA: 145,350 sq. ft.
- ADDITIONAL Space Remaining in Setback
Landscaping
Parking (Double Row)
- * PLAZA Entry / RESPITE Area
Additional Finish Material & Scape Amenity
- MAJOR Development Entry
Signified
- MINOR Development Entry
Right Turn only

FIGURE 04.2.2-1

PASEO / AREA 1 and 2A - Recreation
Drawing 01

This Figure provides additional context for the requirements associated with the design and implementation of the paseo.



EXERCISE



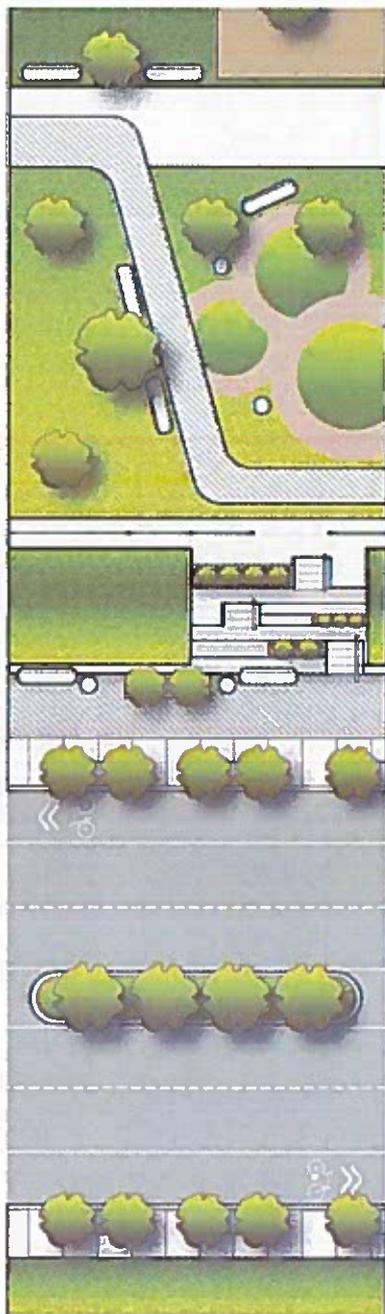
STROLLING



ALTERNATIVE MOVEMENT



OUTDOOR SEATING



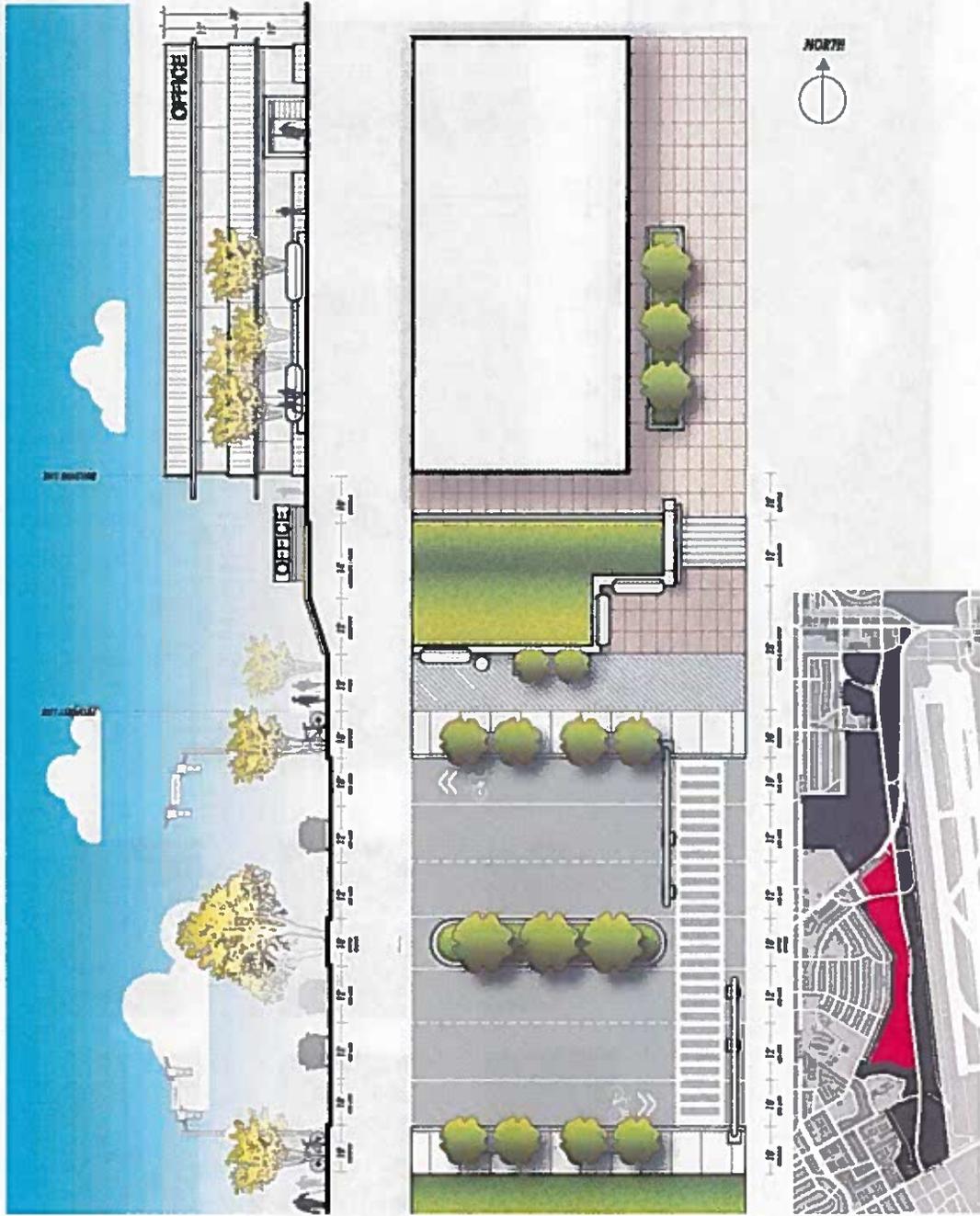
NORTH



FIGURE 04.2.2-2

PASEO / AREAS 2C, 2E & 3 - Office
Drawing 02

This Figure provides additional context for the requirements associated with the design and implementation of the paseo.



PROPOSED ACTIVITIES



STROLLING



EXERCISE



OUTDOOR SEATING



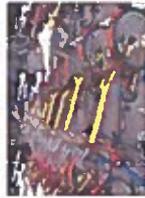
ALTERNATIVE MOVEMENT

FIGURE 04.2.2-3

PASEO / AREAS 2C, 2E & 3 - Office
Drawing 02 - Plaza Entry

This figure provides additional context for the requirements associated with the design and implementation of the paseo.

PROPOSED ACTIVITIES



BIKE RENTAL



SMALL PARKS



LARGER GROUPS



WATER FEATURES



AIRPORT VIEWING



PUBLIC EVENTS

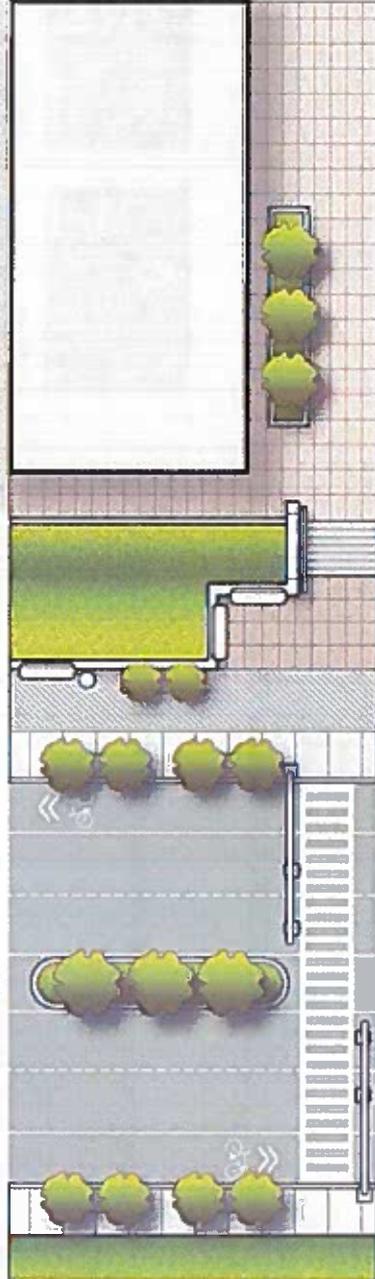
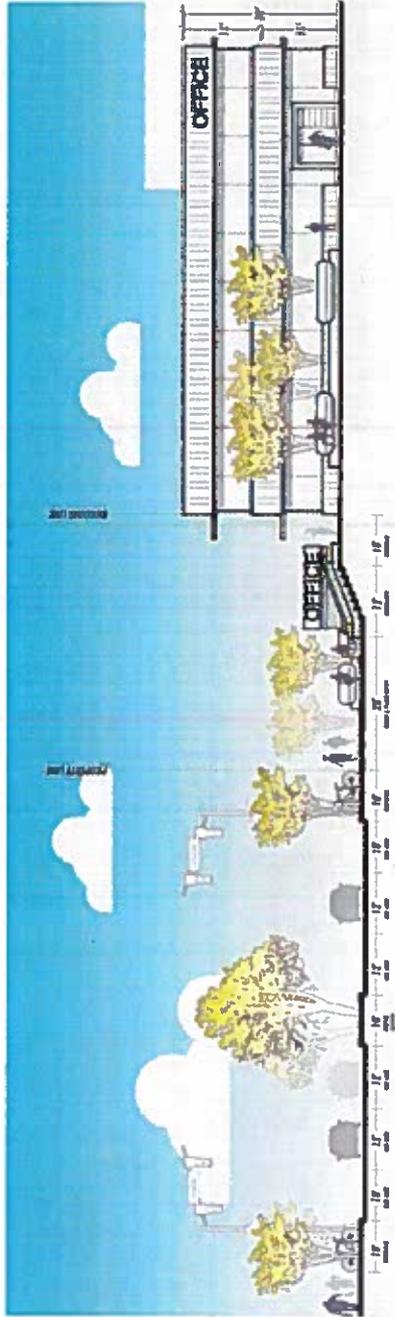


FIGURE 04.2.2-4

PASEO / AREAS 2C, 2E & 3 - Office
Drawing 03 - Double Row of Parking

This Figure provides additional context for the requirements associated with the design and implementation of the paseo.



FIGURE 04.2.2-5

PASEO / AREAS 2C, 2E & 3 - Office
 Drawing 03 - Single Row Parking and
 Plaza Entry

This Figure provides additional context
 for the requirements associated with the
 design and implementation of the paseo.

PROPOSED ACTIVITIES



BIKE RENTAL



SMALL PARKS



LARGER GROUPS



WATER FEATURES



AIRPORT VIEWING



PUBLIC EVENTS

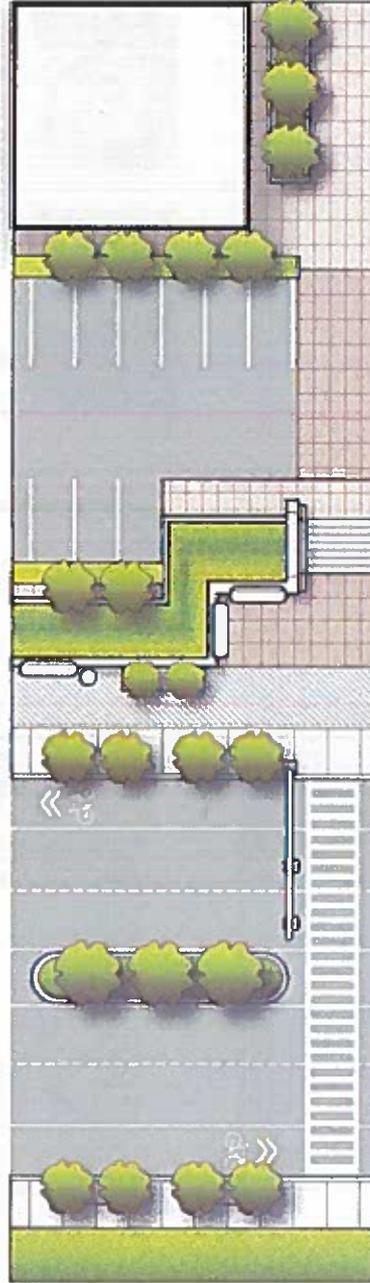
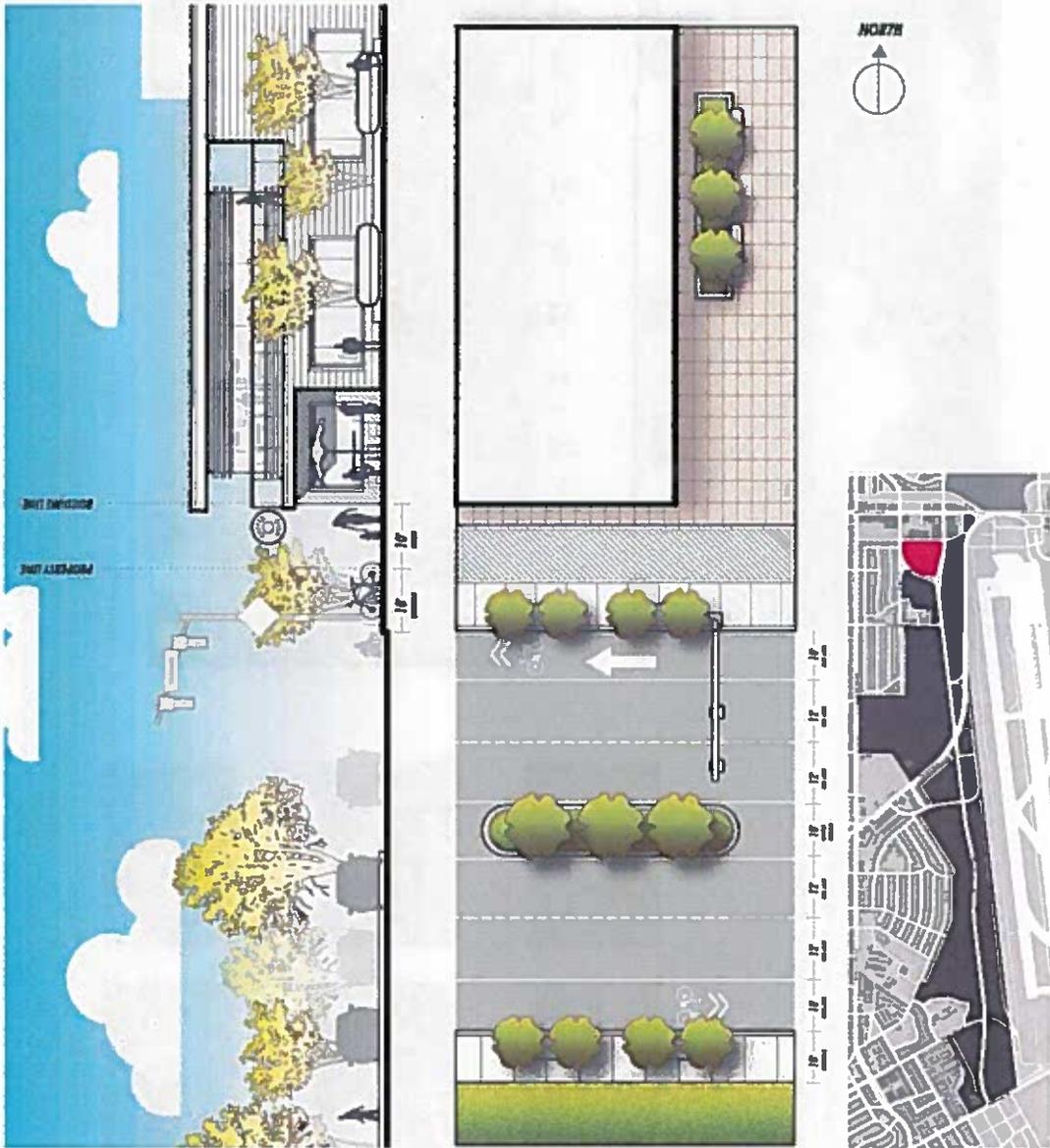


FIGURE 04.2.2-6

PASEO / AREA 12A West - Center
 Drawing 04 - Civic Use / Open Space

This Figure provides additional context for the requirements associated with the design and implementation of the paseo.



PROPOSED ACTIVITIES



STROLLING



EXERCISE



OUTDOOR SEATING



ALTERNATIVE MOVEMENT



FIGURE 04.2.2-7

PASEO / AREA 12A East - Center
Drawing 05 - Office and Retail

This Figure provides additional context for the requirements associated with the design and implementation of the paseo.

PROPOSED ACTIVITIES



CAFE SPACE



SIDEWALK SALES



SMALL MARKETS



ADDITIONAL PLANTINGS



AIRPORT VIEWING



BICYCLISTS

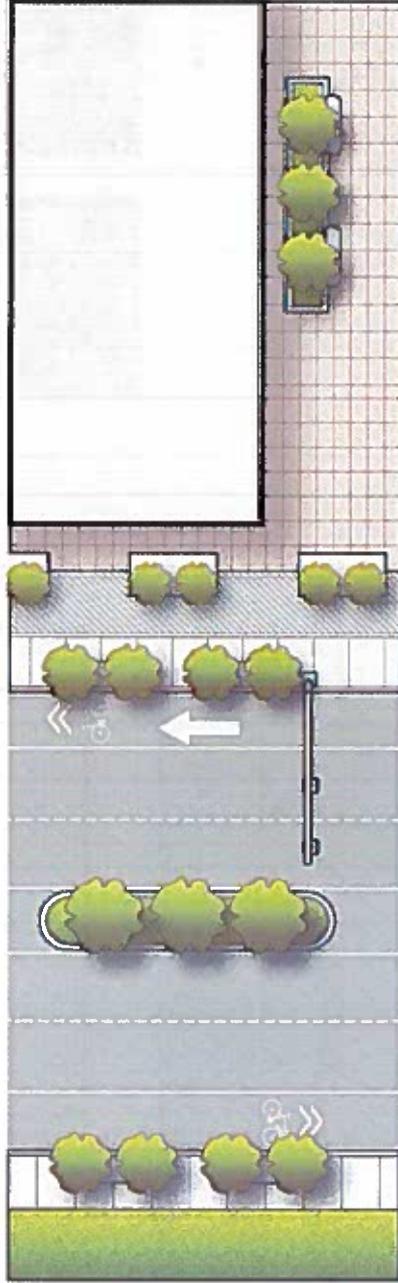
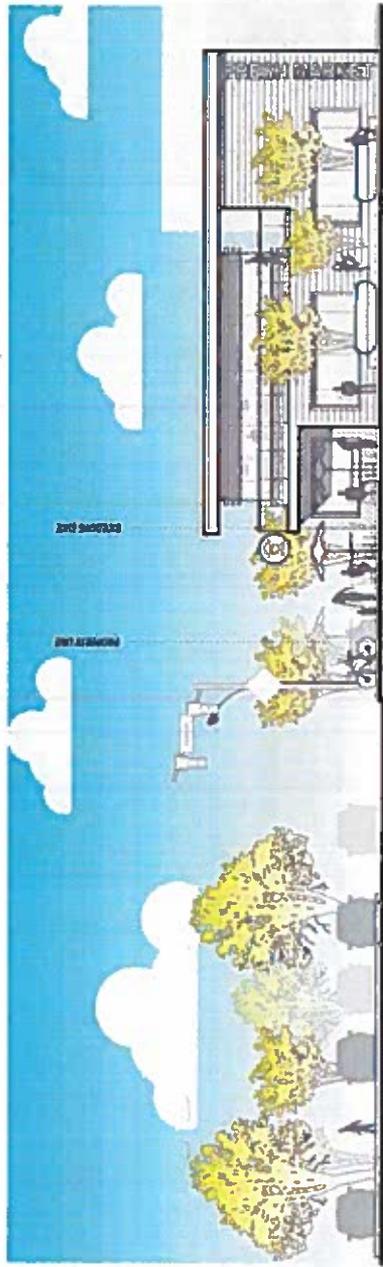
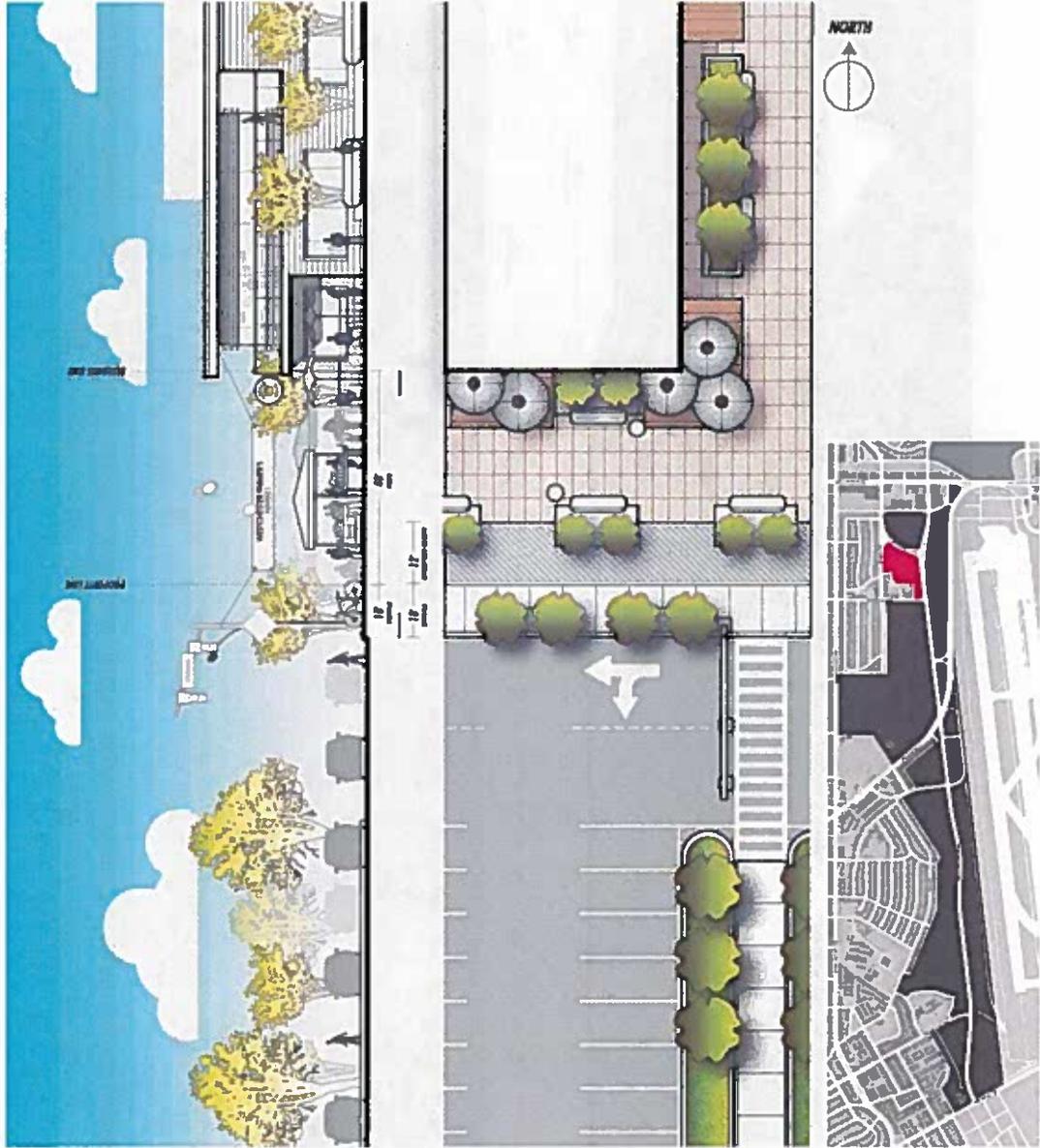


FIGURE 04.2.2-8
 PASEO / AREA 11 - Center
 Drawing 06 - Retail

This Figure provides additional context for the requirements associated with the design and implementation of the paseo.



PROPOSED ACTIVITIES



KIOSKS



BIKE RENTAL



SMALL PARKS



LARGER GROUPS



WATER FEATURES



FOOD TRUCKS

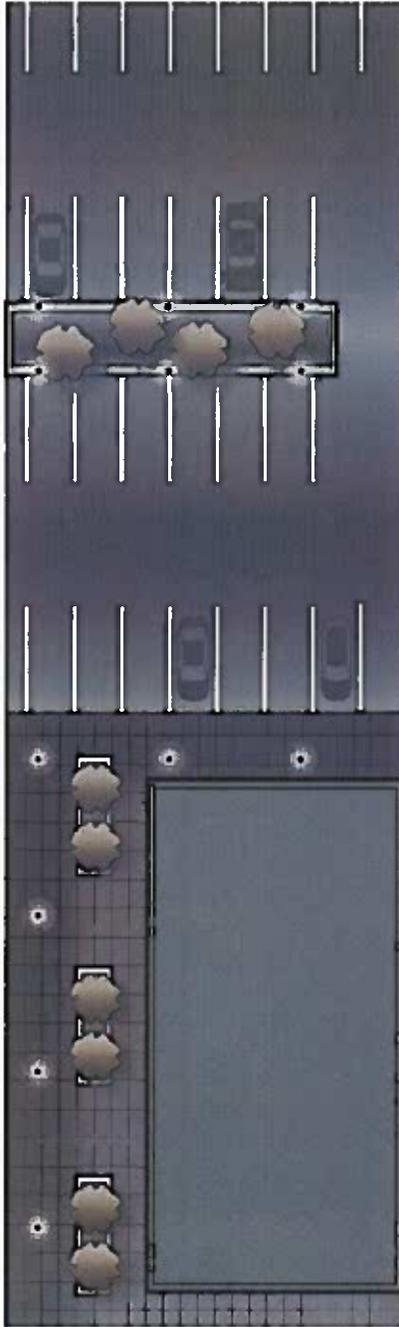
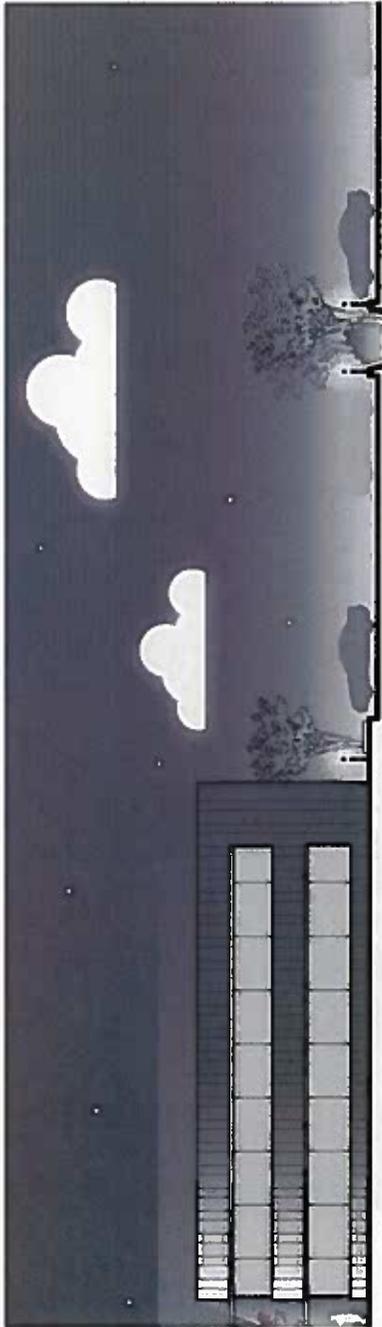


PUBLIC EVENTS

FIGURE 04.2.3

SITE LIGHTING - CAMPUS EXAMPLE

This Figure provides an example of site lighting in the LAX Northside Campus District.



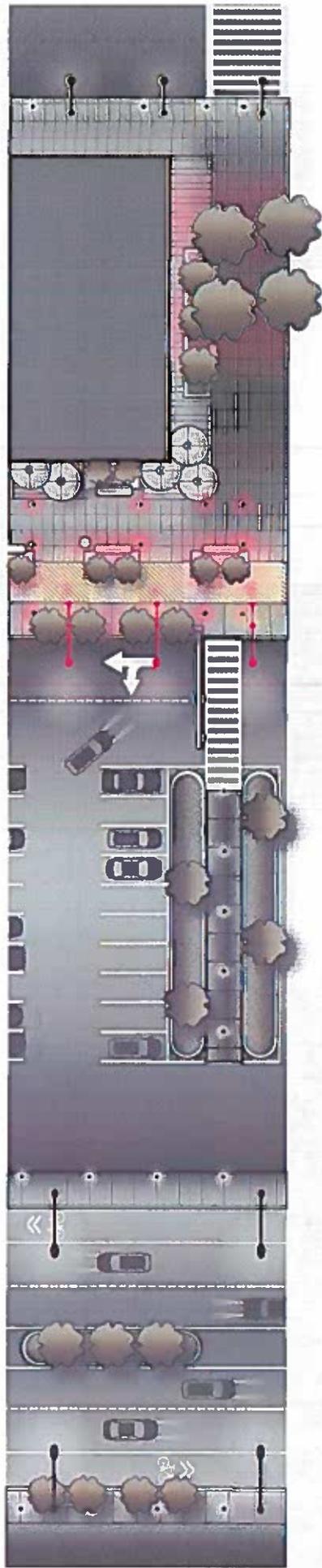
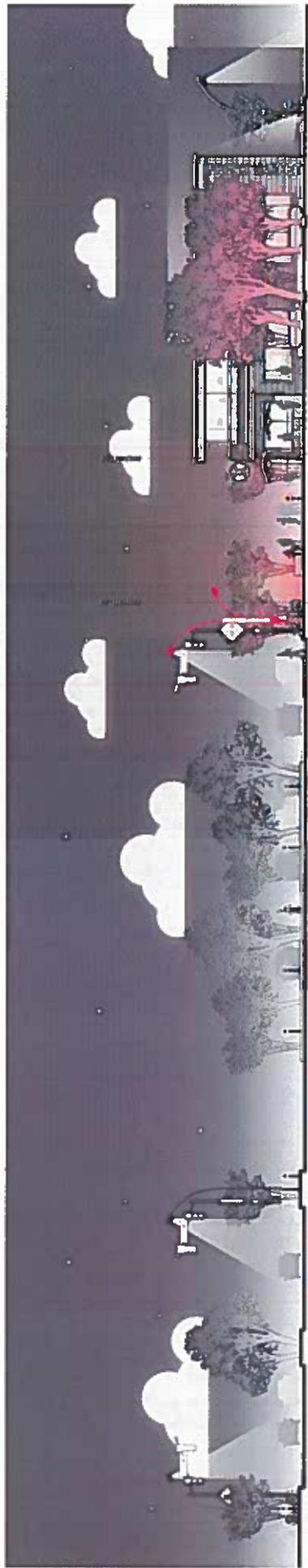
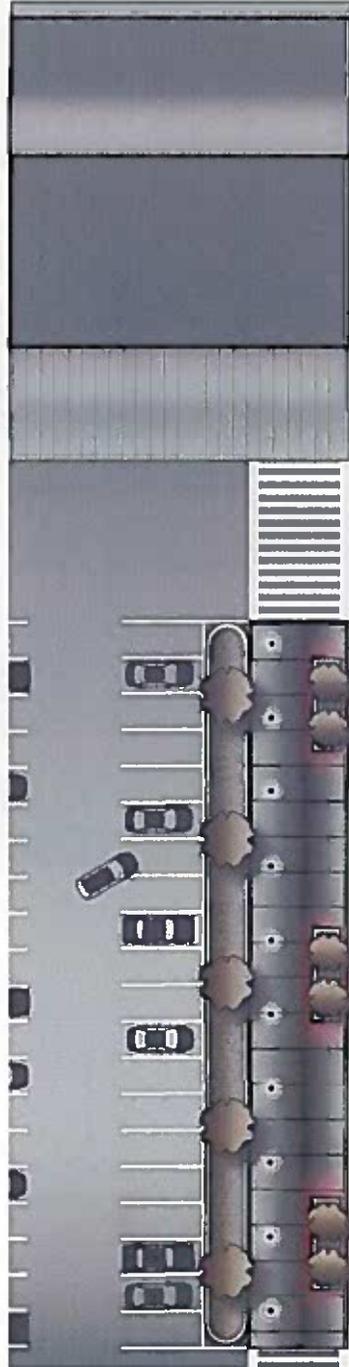
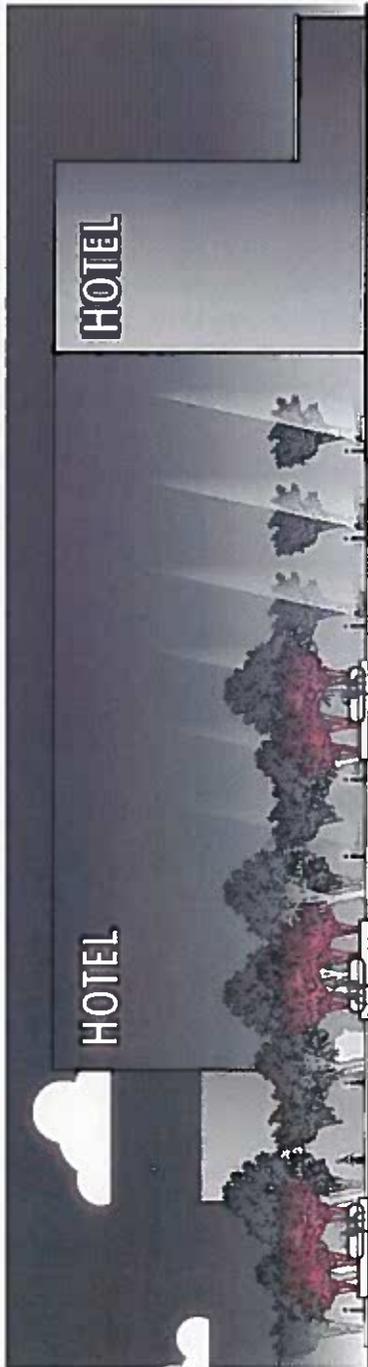


FIGURE 04.2.3
SITE LIGHTING - CENTER EXAMPLE

This Figure provides an example of site lighting in the LAX Northside Center District.



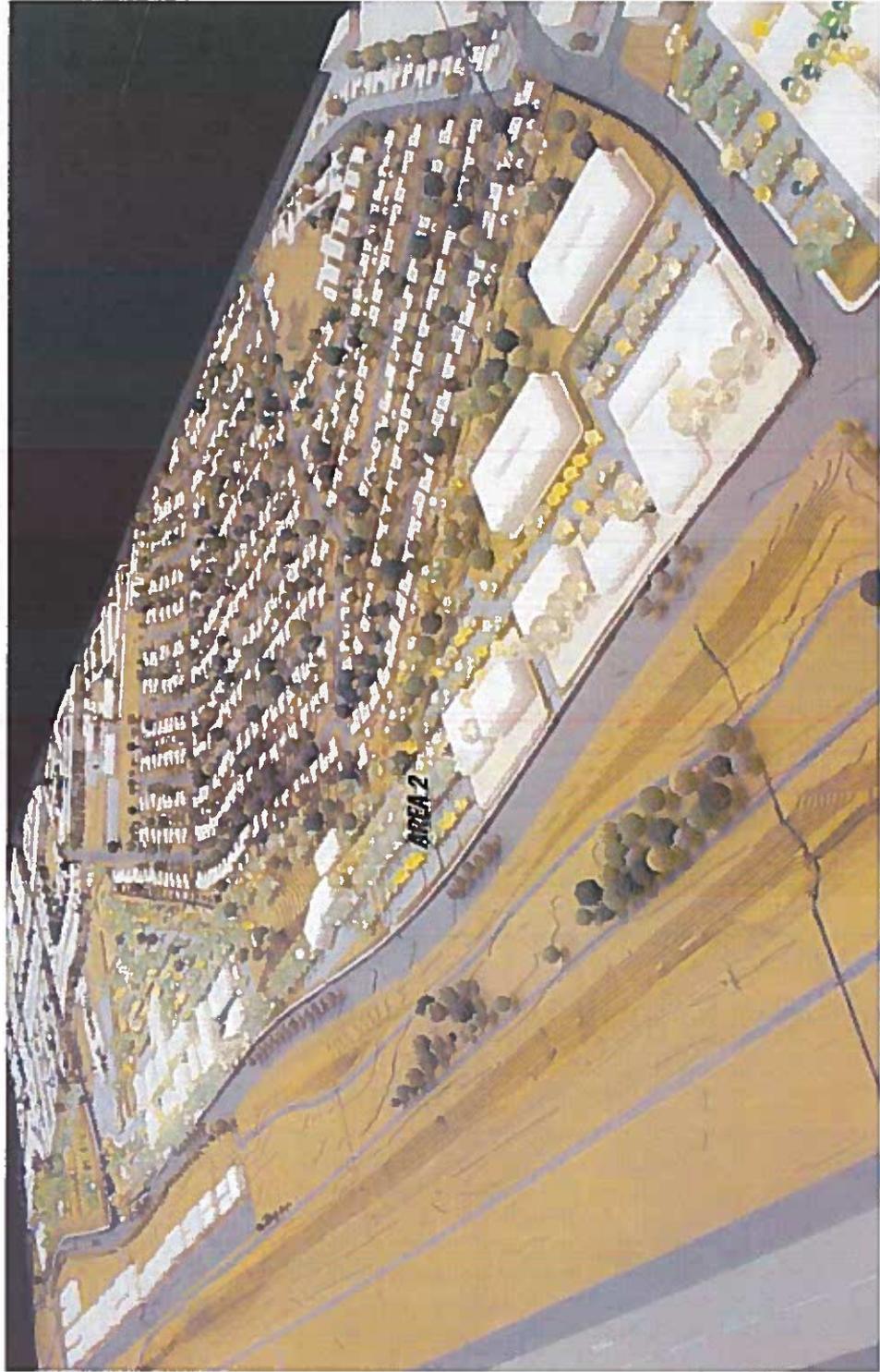


FIGURE 04.3-3
LAX NORTHSIDE CAMPUS - AREA 2

This Figure presents an image for the LAX Northside Campus District, Area 2 illustrating in the form of a physical model the conceptual direction for all design aspects for the LAX Northside.



FIGURE 04.3-4
LAX NORTHSIDE CAMPUS - AREA 3

This Figure presents an image for the LAX Northside Campus District, Area 3 illustrating in the form of a physical model the conceptual direction for all design aspects for the LAX Northside.



FIGURE 04.3-5
LAX NORTHSIDE CENTER - AREA 13&12

This Figure presents an image for the LAX Northside Center District, Area 13 and 12 illustrating in the form of a physical model the conceptual direction for all design aspects for the LAX Northside



FIGURE 04.3-6

LAX NORTHSIDE CENTER - AREA 12 & 11

This Figure presents an image for the LAX Northside Center District, Area 11 and 12 illustrating in the form of a physical model the conceptual direction for all design aspects for the LAX Northside.

FIGURE 04.3-7
LAX NORTHSIDE CAMPUS - AREA 1

This Figure presents an image for the LAX Northside Campus District, Area 1 Open Space illustrating in the form of a physical model the conceptual direction for all design aspects for the LAX Northside.

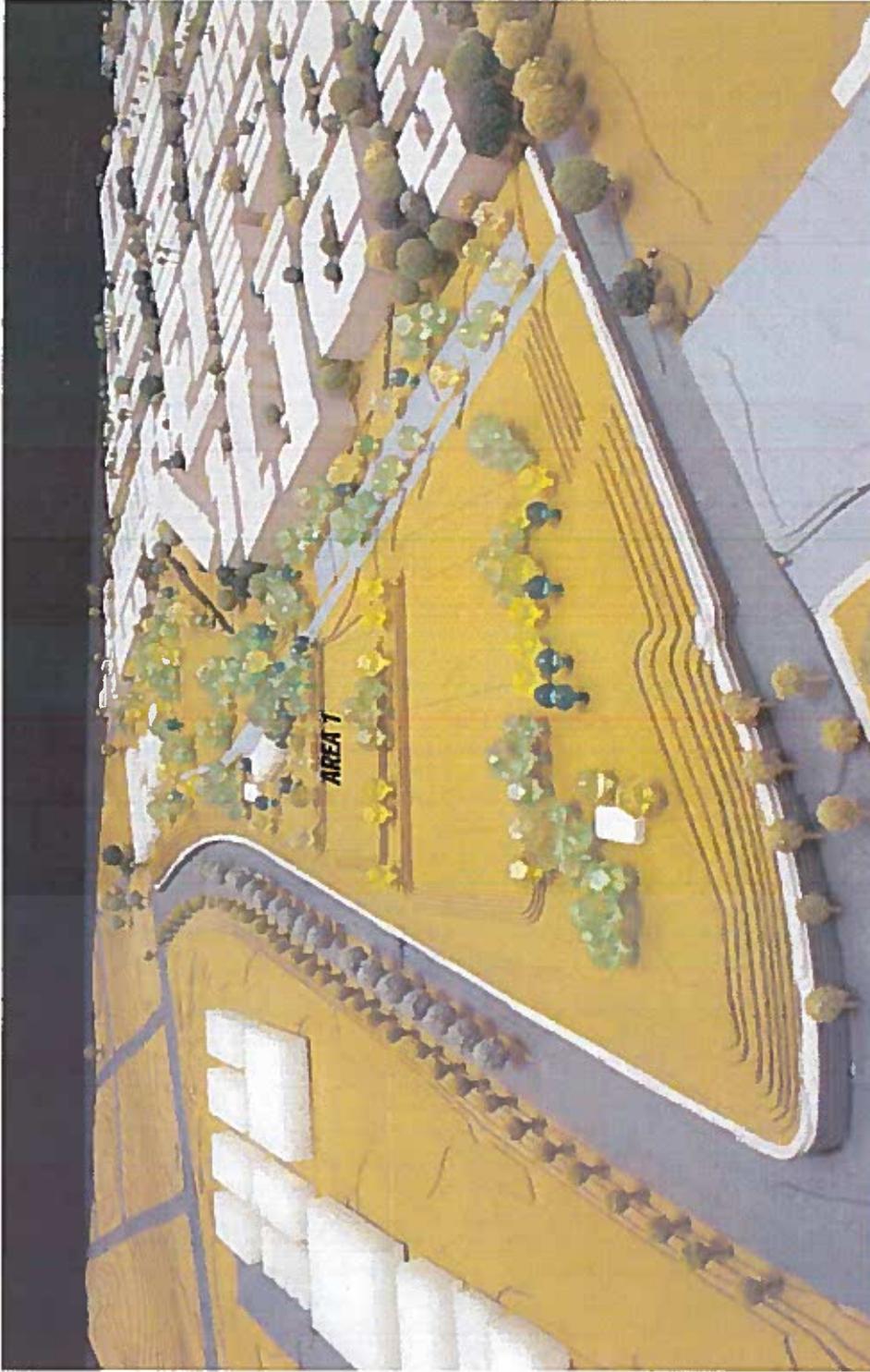
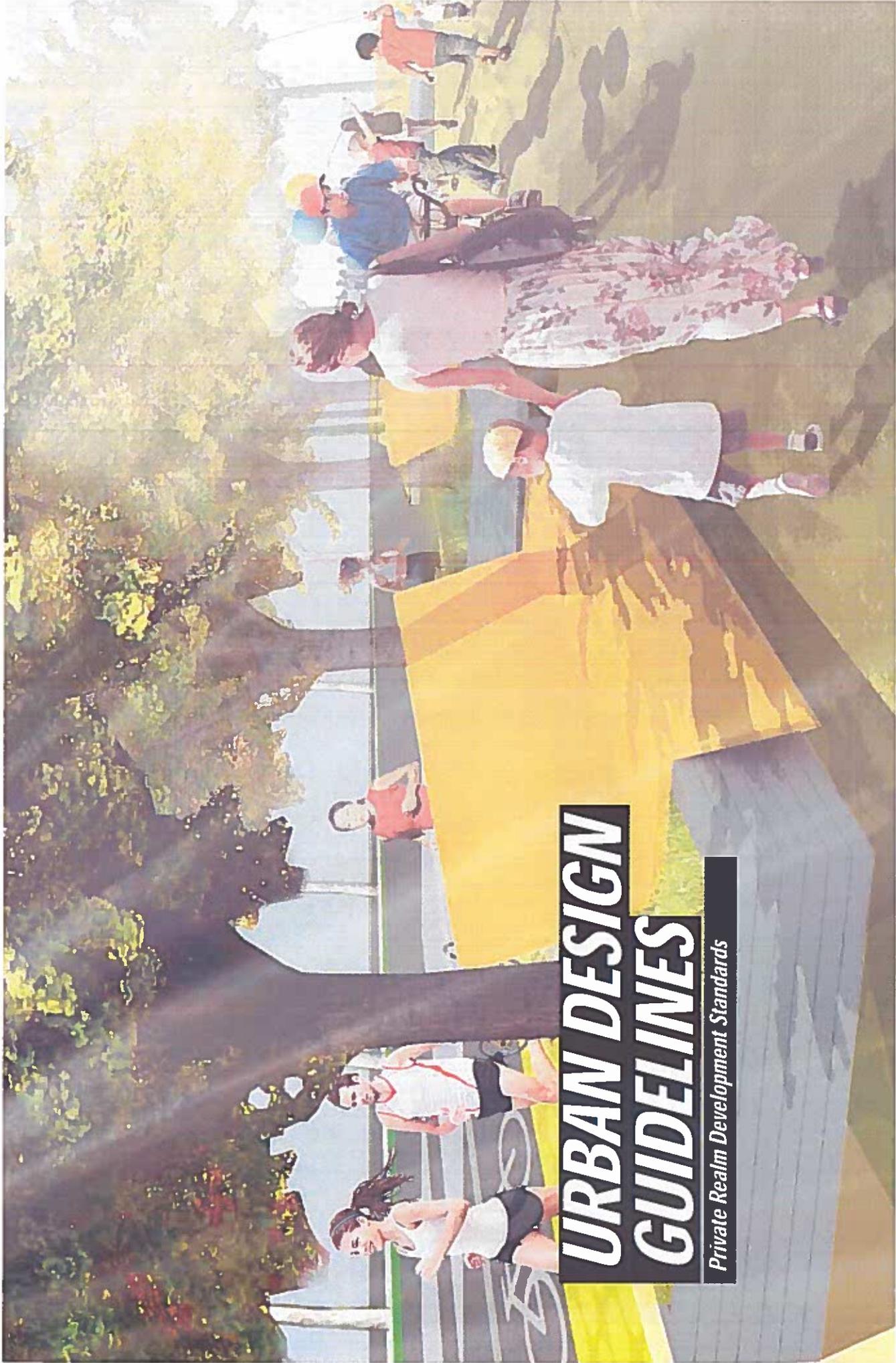




FIGURE 04.3-8
LAX NORTHSIDE CENTER - AREA 11

This Figure presents an image for the LAX Northside Center District, Area 11 illustrating in the form of a physical model the conceptual direction for all design aspects for the LAX Northside.



URBAN DESIGN GUIDELINES

Private Realm Development Standards



05

URBAN DESIGN

GUIDELINES

that will provide a seamless transition and buffer adjacent to the Westchester and Playa Del Rey neighborhoods. The LAX Northside Design Guidelines and Standards address the sensitivity of neighboring uses by focusing activity and development along Westchester Parkway. This strategy will provide consistency in the design of the urban environment and will establish secure boundaries between future developments and existing neighborhoods.

In addition to defining urban design concepts, these guidelines and standards include sustainable practices as an integral aspect of the overall design direction. These design strategies focus on supplying active lifestyles and existing recreational activities. By nurturing active living through design, and providing pedestrian-oriented infrastructure throughout all parcels developed in the LAX Northside, the LAX Northside will be a leading example of sustainable, healthy design. The primary design elements that will unify the project include:

1. Concentrating building density closest to Westchester Parkway to create a buffer from the adjacent neighbors and to establish a vibrant and active street frontage.
2. Preserving and enhancing existing recreational activities in the community through the creation of a pedestrian paseo along Westchester Parkway.
3. Creating a comfortable and active pedestrian experience along the length of the paseo, spacing development entrances at walkable distances, providing variable depths of space to accommodate a variety of active and passive activities, and providing pedestrian amenities throughout the project area.
4. Complementing the established linear character of the adjacent runways by planting a double row of trees across the LAX Northside to buffer future development.
5. Using missing strategies that address the pedestrian scene.

Within the LAX Northside, three primary districts have been defined to help differentiate various design strategies.

"Urban design" refers to all spaces and infrastructures that provide character, identity and form to the overall built urban environment in the LAX Northside. The purpose of the urban design guidelines is to establish a framework for the development of the built environment within the LAX Northside, and to shape the design of future projects. These guidelines and standards are intended to achieve compatibility with adjacent communities, while maintaining the flexibility needed to respond to market conditions and reflecting the latest best practices in sustainability.

Urban design concepts will differentiate the LAX Northside as a unique area within the Los Angeles region. By taking advantage of the LAX Northside's location between Los Angeles International Airport and the existing business district in Westchester, the LAX Northside Design Guidelines and Standards supports new development



LAX NORTHSIDE CENTER DISTRICT

The LAX Northside Center District will be a low to mid rise, retail and office environment extending from Sepulveda Westway to Lincoln Boulevard, incorporating Areas 11, 12A East and West, 12B (existing Westchester Golf Course) and 13. Vehicular access will be allowed primarily off of Westchester Parkway, with secondary access allowed along La Tijera Boulevard and Sepulveda Westway. Building setbacks and setbacks along Westchester Parkway and La Tijera Boulevard will accommodate a pedestrian environment that is consistent with the proposed paseo and promotes pedestrian activity along primary building frontages.

Pedestrians will access the development through plazas, connecting existing site conditions along Sepulveda Boulevard into the new developments. Transportation stations (for either bus or future light rail) are also accommodated in this mixed office and commercial use environment. In addition, future developments will provide bike racks and carpool parking to help promote alternative forms of transportation and trip reduction.

In Areas 11 and 12A East, buildings will frame a series of small, internal landscaped open spaces. In Area 11, building massing will be allocated to the periphery of the uniquely shaped parcel, providing massing along the edges of Westchester Parkway and La Tijera Boulevard. Along the La Tijera edge, the existing southeast will be preserved and building frontages will be required to step back as height increases in order to provide privacy for adjacent neighbors. By locating building massing around the area's edges, the internal space of the parcel will be available for surface parking and pedestrian connections in necessary locations. Similar massing strategies will be used in Area 12A East as a means to reinforce activity along Westchester Parkway. In the northern portion of the area, a parking garage is provided for that will maintain privacy for adjacent neighbors through the location and articulation of the parking garage's facade. In Areas 12A West and 13, civic and community buildings along Westchester Parkway and Lincoln Boulevard will maintain pedestrian scaling through setbacks and height limits (20 to 60 feet).

Abundant annual blooms will provide color and attraction to the pedestrian environments of the Northside Center District. Landscape setbacks and general landscape materials in the LAX Northside Center District will be comprised of a fifty-fifty split between native and non-native species. Where landscape setbacks are required, privacy between adjacent neighbors will be preserved through the articulation of a planted edge. With the majority of space being designated for surface parking, planting materials will be designed to be capable of managing storm water and runoff, including through use of bioswales. These landscaping requirements in parking areas will help provide an additional buffering screen to adjacent neighbors and will help to define a sense of boulevards throughout the district.



LAX NORTHSIDE CAMPUS DISTRICT

The LAX Northside Campus will be a low-rise, low density office and research and development park extending from Lincoln Boulevard to Falmouth Avenue. Site access will be controlled and project entry points will become major design features along Westchester Parkway incorporating signage and landscape elements. Along the north side of Westchester Parkway, buildings will maintain a relationship to the street, but will be diverse in terms of siting, design, and building clustering. Internal to the parcels, wider setbacks are required at major access points, while minimal building setbacks are required everywhere else creating a campus-like environments (see Figure 05.2 for specific setback requirements). Additionally, 65% of building square footage is required to be adjacent to Westchester Parkway. These design strategies reinforce a pedestrian scale within each development, while providing the opportunity for integration with the paseo.

Grading strategies, building height limits (ranging from 45-60 feet), and density allocation requirements will limit the visual impact of the new developments on neighboring residences where possible. In Areas 2 and 3, grading

strategies will lower elevations for building frontages, providing a more accessible relationship with Westchester Parkway. In Area 1, existing grading will be preserved to separate planned open spaces from the busy nature of Westchester Parkway. In all areas, landscaping specific to the streetscape, setbacks, and campus environments will combine native and non-native materials that create a dynamic experience with annual blooms and vibrant colors articulated through a series of trees, shrubs and groundcover.

Landscape Buffers are required in two separate locations in the LAX Northside Campus District. Buildings, parking, and pedestrian access are prohibited in these areas. The Landscape Buffers will be secured on all of their sides to prevent access. A 100-foot Landscape Buffer is located on the northern property line in Parcel 2, and a 20-foot Landscape Buffer is located along the northwest property line in Parcel 1. These buffers will be planted primarily with locally-native trees, shrubs and ground cover, and when needed, will provide appropriate ground cover to control erosion. Existing trees will be preserved if they are compatible with the landscape material palette in these guidelines and standards. The Landscape Buffer will function as a visual screen that physically separates the proposed land uses from the adjacent neighborhoods.

Recreation areas, which are open spaces designed to accommodate active and passive forms of recreation, including, but not limited to soccer, tennis, basketball, and dog parks shall be primarily allocated to Areas 1 and ten acres in the western portion of Area 2. All recreation spaces will be secured with a 10-foot tall perimeter fence and will operate with established hours of operation, preventing the need for lighted fields and the possibility of light trespass. Parking will be provided for all recreation spaces consistent with Los Angeles City Code requirements. Recreation areas will provide additional amenities such as ancillary buildings for storage, recreation centers, pedestrian pathways and compliant access for handicapped individuals. Recreation areas will be planted with a required palette that is eighty (80) percent locally-native and twenty (20) percent non-native creating a landscape that is specific to the LAX Northside's location with the occasional pop of vibrant color and dynamic annual change.

locally native and twenty (20) percent non-native species, creating a compassia, low-lying landscapes. Along existing security fence boundaries, replacement trees or shrubs may be introduced in the event of an existing tree dying or becoming damaged. These replacement trees have been selected to prevent illegal access into the airfield by having minimal branch strength and density.



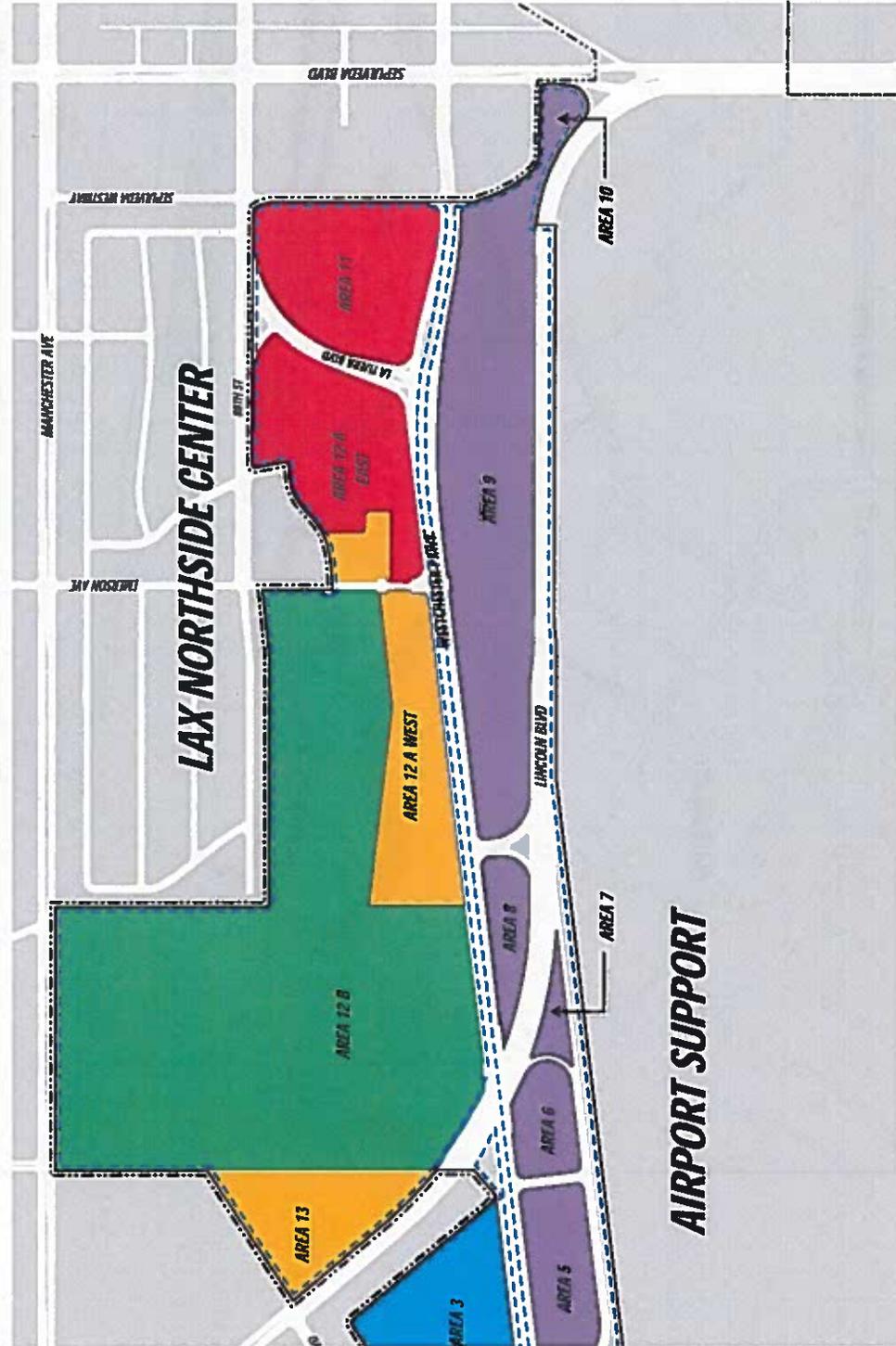
LAX NORTHSIDE AIRPORT SUPPORT DISTRICT

The areas south of Westchester Parkway will be comprised of low-rise (30 foot height limit), light industrial structures, with the majority of building density located in Area 4. The existing site entrance and security checkpoint at the intersection of Falmouth Avenue and Westchester Parkway will be maintained, allowing a secured access point for employees. Grading strategies and landscape berms will be preserved as they exist today, limiting the visual presence of this area from the point of view of neighbors north of Westchester Parkway. Where applicable, additional grading and landscape berms may be introduced to further enhance these design ideas.

In addition to the existing landscape materials found within these areas, new materials will be introduced that are locally-native, drought tolerant, and require little to no maintenance. Landscape materials will be limited to shrubs and ground cover, and when applicable, existing materials that are compatible with these guidelines and standards will be preserved. In addition, newly introduced plant materials will be composed of eighty (80) percent

FIGURE 05.1
LAND USE MAP

This figure provides information specific to the location and distribution of land uses in the LAX Northside.



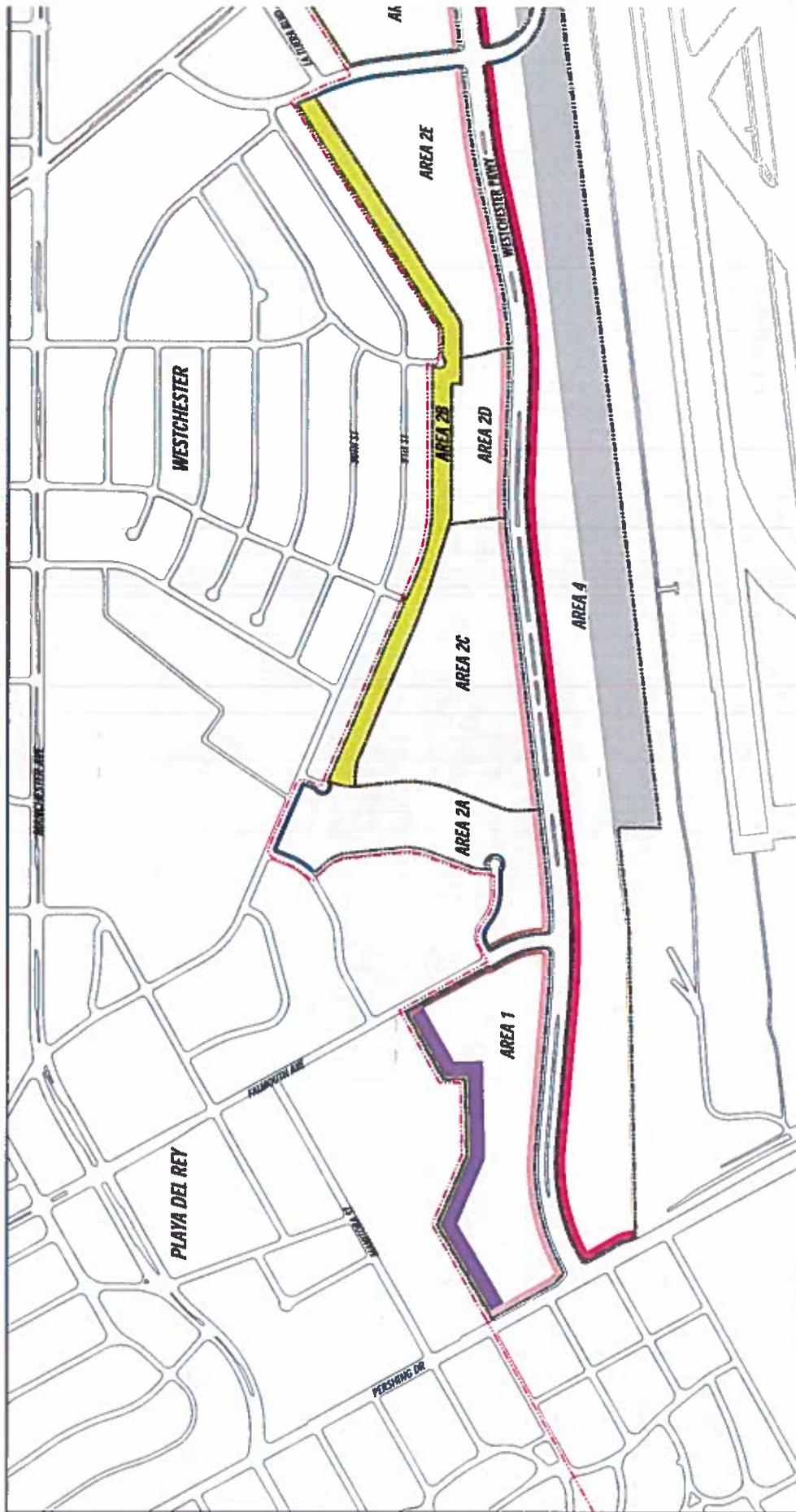


FIGURE 05.2
BUILDING SETBACKS

This Figure provides information specific to the location of all regulated building setbacks in the LAX Northside.

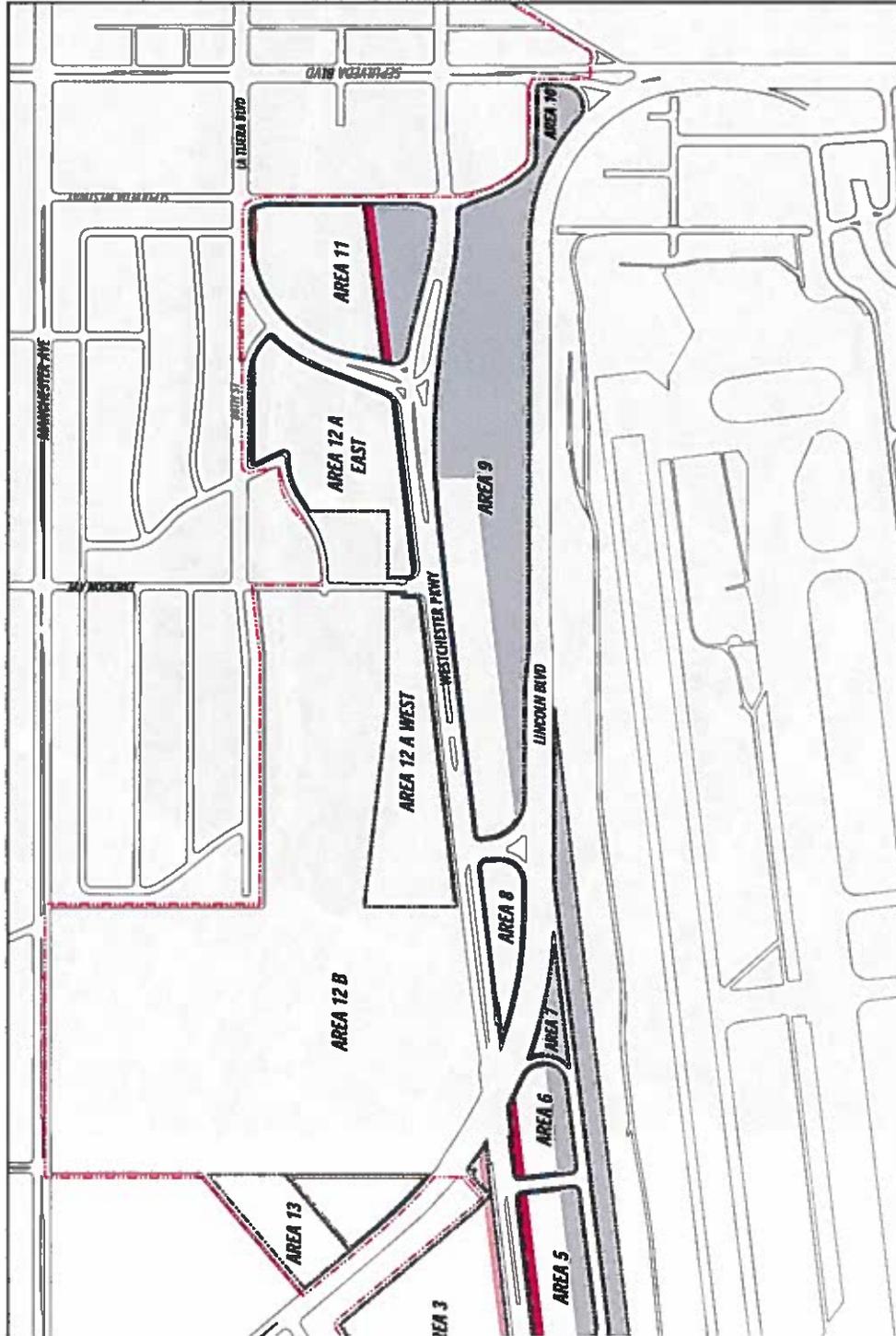
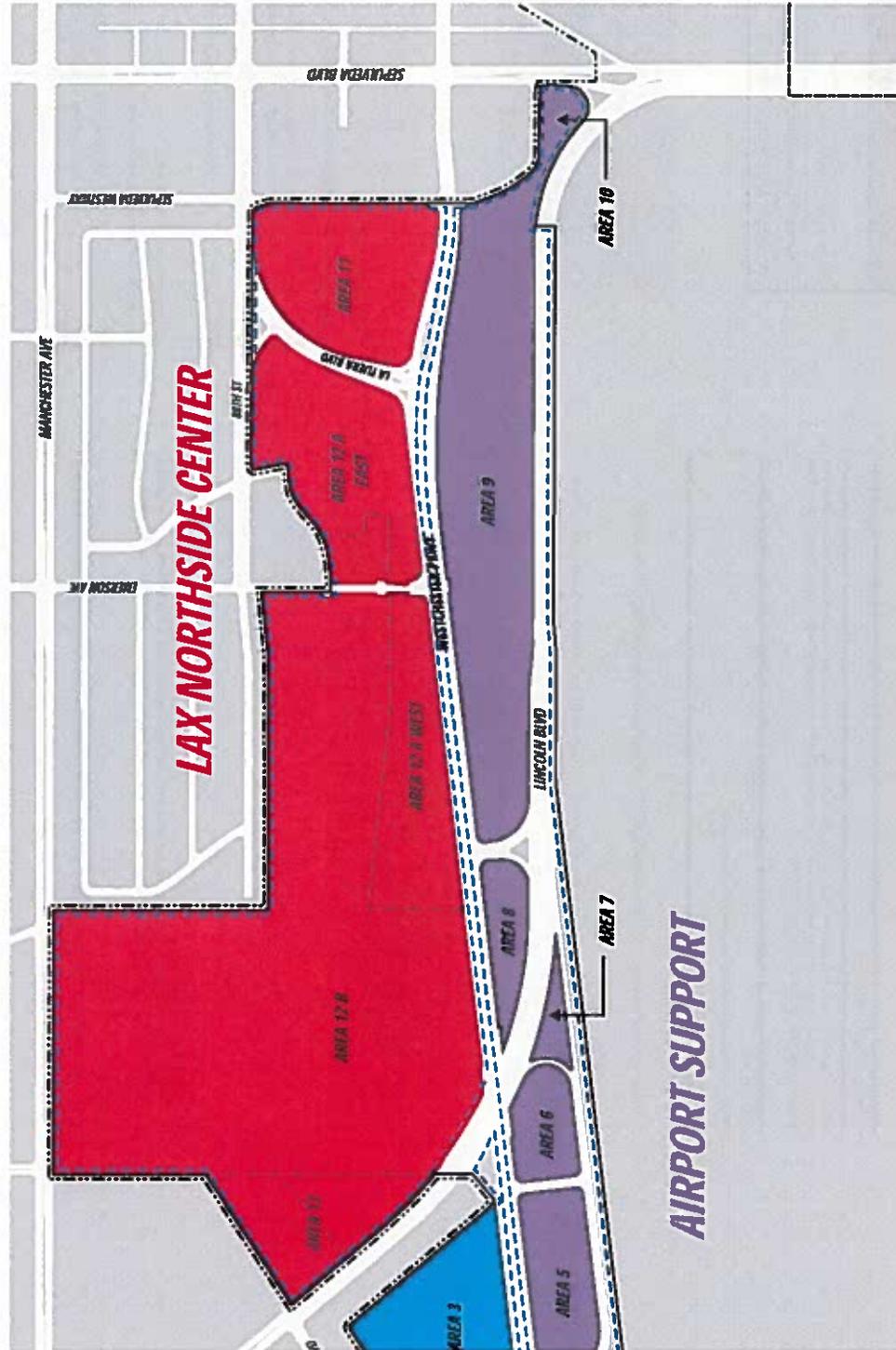




FIGURE 05.3
DISTRICT MAP

This Figure provides information specific to the location of the three primary development districts in the LAX Northside.



LAX Northside Design Guidelines Checklist

05 Urban Design Guidelines

All projects within the LAX Northside Area shall comply with the LAX Specific Plan and the standards and guidelines contained within this document. Additional information that illustrates and informs the overall concepts associated with the urban design approach can be found in Chapter 4: Vision.

05.1 Land Use

A Project shall comply with the following land use standards.

Standards

A. Maximum Permitted Floor Area

The maximum total permitted Floor Area within the LAX Northside shall not exceed 2,320,000 square feet.

B. Project Land Use

The LAX Northside shall be developed with the following land uses as defined by Section D and E below. These land uses shall be developed in those locations, as shown on Figure 05.1, the Land Use Map.



Figure 5.1 // Land Use Map

C. Designation of Sub-Areas

The LAX Northside contains three Districts, as shown on the Districts Map, Figure 05.3. The Districts are designated as: LAX Northside Center District, LAX Northside Campus District, and LAX Northside Airport Support District.

D. Floor Area

The LAX Northside shall be developed with the development capacity for each district as defined in the following table:

MAXIMUM FLOOR AREA PER DISTRICT	
District	Building Square Footage
LAX Northside Center District	645,000
LAX Northside Campus District	1,075,000
LAX Northside Airport Support District	600,000

Table 05.1.1



Figure 5.3 // District Map

D.1 Floor Area Calculations

Every application for Compliance Review shall include a table that identifies the following:

- Proposed project building floor area.
- New total of developed building floor area within the Northside District.
- Total remaining allowed building floor area within the Northside District.
- New total developed building floor area for all areas within the LAX Northside.
- Total remaining allowed building floor area within the LAX Northside.

D.2 Trip Generation Calculations

Every application for Compliance Review shall include a table that identifies the following:

- Proposed project trip generation.
- New total trip generation within the LAX Northside.
- Total remaining allowed trip generation within the LAX Northside.

E. Permitted Uses
Permitted uses are indicated with "X" for each Area in the table below.

PERMITTED LAND USE TABLE				
LAND USE	AREAS			
	1-3	4-10	11-13	
Buffer Uses (B)				
a. Underground utilities, with limited related surface structures for ventilation or access	X			
Community or Civic Uses (CC)				
a. Non-profit businesses or institutions that serve the local community	X			X
b. Outside recreation related to non-profit institutions such as swimming pools and athletic courts/fields	X			X
c. City, county or state government buildings including senior center, fire stations and libraries	X			X
d. Cultural institutions such as performing arts or museums	X			X
e. For-profit businesses or enterprises that serve the local community	X			X
f. Non-profit club or lodge	X			X
Office and Research Uses (OR)				
a. Community and civic zone uses				X
b. Office, general business or professional				X
c. Medical or dental office, including surgery center, outpatient services, primary care clinic and pharmacy				X
d. Research and development including office, engineering, showroom, laboratory. Limited test and assembly of not-for-sale prototypes is permitted				X
e. Business college, professional or scientific school or college, not including trade schools involving shop work, or the repair or maintenance of machinery or equipment				X
f. Media, post-production or broadcast studio, without transmission towers				X
g. Animal care or boarding facility, with conditions				X
Recreation and Open Space Uses (OS)				
a. Golf Course	X			X
b. Athletic fields, outdoor athletic courts	X			X

Table 05.1.2

PERMITTED LAND USE TABLE				
LAND USE	AREAS			
	1-3	4-10	11-13	
Recreation and Open Space Uses (OS)				
c. Public shade structures, picnic areas and rest rooms	X			X
d. Dog park, with conditions	X			X
e. Below grade storm water treatment facilities, with conditions	X			X
Mixed Use - Commercial (MU)				
a. Banks or financial institutions				X
b. Retail, including merchandise sales and local services such as bakery, barber shop, beauty shop, book store, stationary store, software or computer store, toy store, clothes cleaners, tailors, florist or gift shop, real estate, hardware or appliance store or jewelry store				X
c. Professional Office				X
d. Restaurants, including sit down, quick serve, and drive through				X
e. Hotel, including related restaurants, services and parking				X
f. Transit station	X	X		X
g. Medical or dental office, including surgery center, outpatient services, primary care clinic and pharmacy				X
h. Animal medical clinic				X
i. Surface and subterranean parking	X			X
j. Wholesale stores, with conditions				X
k. Auto repair or service, with conditions				X
l. Auto fueling stations, with conditions				X
Airport Support Uses (AS)				
a. Maintenance and repair shops		X		
b. Indoor storage and warehouses		X		
c. Exterior storage		X		
d. Administrative offices		X		
e. Radar and surveillance facilities		X		X
f. Utilities and utility-related structures		X		X
g. Construction material temporary storage		X		X
h. Recycling sorting and storage		X		X
i. Parking		X		X

Table 05.1.2

F. Prohibited Uses

The following uses shall be prohibited in the LAX Northside.

- a. Residential, or dwelling units of any kind
- b. K-12 education
- c. Retail stores over 100,000 gross square feet
- d. Auto dealerships
- e. Adult entertainment as defined in LAMC 12.70
- f. Massage parlors as defined in LAMC 12.70
- g. Parking as a primary use, except in Airport Support and Mixed Use designated areas

05.2 Development Regulations

A Project shall comply with the following development standards.

Standards

05.2A Building Heights

Buildings shall be developed in compliance with the height standards identified below and contained within Table 05.2A.1.

- 1. Heights shall be measured from mean sea level (MSL).

BUILDING HEIGHT	
Area	Maximum Height
LAX Northside-Center	
Area 11 and 12A East	60'
Area 12A West	20'
Area 13	45'
LAX Northside-Campus	
Area 3	60'
Areas 1, 2A, 2C, 2D, 2E	45'
LAX Northside Airport Support	
All Areas	30'

Table 05.2A.1

05.2B Building Setbacks

- In Area 11, buildings located adjacent to the 88th Street and La Tijera property line shall be stepped back by one foot for each additional foot of height above 15'.

05.2C Building Setbacks

Buildings shall be developed in compliance with the following setback standards.

- 1. Buildings shall be developed in compliance with the Building Setback standards as shown the Building Setbacks Map (Figure 05.2).
- 2. No building or portion of a building is permitted within the Building Setback.
- 3. No parking is permitted within the Building Setbacks in the LAX Northside Center District.
- 4. No walls or fences are permitted within the Building Setback along Westchester Parkway, La Tijera Boulevard, Loyola Boulevard, Falmouth Avenue or Pershing Drive.
- 5. Plaza spaces, outdoor eating areas, and enhanced pedestrian connections are permitted within the Building Setback.
- 6. Landscaped areas within Building Setbacks shall be landscaped in accordance with the Landscape Zone Map and Palettes established in Chapter 7.



Figure 5.2 // Building Setbacks

05.2D Landscape Buffers

Landscape Buffers have been identified as Area 2B and the 20-foot Landscape Buffer on the northern boundary of Area 1. Landscape Buffers shall be developed in compliance with the following standards.

- 1. Pedestrian access is prohibited, except for maintenance.
- 2. Landscaped Buffers shall be landscaped in accordance with the Landscape Zones and Required Palettes established in Chapter 6.
- 3. Plantings shall be dispersed evenly throughout Landscape Buffers and shall not be limited to the perimeter.

05.2D Landscape Buffers, Continued

4. A 10-foot high fence shall secure the perimeter of a Landscape Buffer identified in the Land Use Map, Figure 05.2. Fence color shall complement proposed landscaping. Examples of appropriate fencing are presented at the end of this Section.

5. Trees planted within the Area 1 Buffer shall be planted to minimize obstruction of views from adjacent residences.

05.2E Building Location

Buildings or structures shall be developed in compliance with the following standards.

1. Buildings within Areas 2, 11, and 12A shall front Westchester Parkway, La Tijera, and Sepulveda Westway setbacks.
2. Buildings within Areas 2C and 2E shall be located with a minimum of 65 percent of the proposed project ground floor area located within 250 feet of the Westchester Parkway property line.
3. Ancillary Buildings shall not front Westchester Parkway, Sepulveda Westway, La Tijera Boulevard, Loyola Boulevard, Falmouth Avenue or Pershing Drive.
4. Parking structures shall not front Westchester Parkway.

05.2F Pedestrian Orientation

All Areas fronting Westchester Parkway, La Tijera, and Sepulveda Westway are designated as "pedestrian oriented." The pedestrian circulation system shall connect buildings, streets, parking areas, and public transit stops to create an environment that supports public transportation, carpool, biking, and other forms of transportation. The following development standards, in addition to the development standards set forth above, shall apply to all primary buildings. These standards shall not apply to Ancillary Buildings.

1. A direct pedestrian connection designated by distinct landscaping and paving materials shall be provided between parking areas and the buildings they serve.
2. Retail or restaurant users shall provide bicycle parking within a minimum of one hundred (100) feet of each primary building entrance.
3. Office or research buildings shall provide bicycle parking within a minimum of two hundred (200) feet of an employee entrance.
4. A clearly-marked pedestrian connection designated by distinct landscaping and paving materials shall be provided between the primary building entrance of buildings and the paseo. The maximum distance between such pedestrian connections and the paseo shall be no more than one hundred (100) feet within the LAX Northside Center District and three hundred (300) feet within the LAX Northside Campus District.

05.2G Site Access

Vehicular access location and design shall conform with the following standards. Additional content is illustrated in Figure 04.1.2.

1. Vehicular access is prohibited from Lincoln Boulevard, Pershing Drive, and all the local streets along the north edge of the Northside area, including locations at Rayford and Stanmore Drives, excluding the existing golf course on Manchester Avenue.
2. Primary access drives, allowing left turns, along Westchester Parkway shall be limited to enhance traffic flow and to reduce the disruption of the landscaping, pedestrian recreation paths, and Westchester Parkway medians. Reciprocal ingress and egress access shall be provided for all adjacent properties within the LAX Northside. This requirement may be waived by due to extreme site constraints or unforeseen conditions.

05.2H Parking

Required parking spaces shall conform to standards set forth in the provisions of LAMC Section 12.21.A.4 and surface parking lots shall be landscaped in accordance with the following standards.

1. A minimum of one tree for every four parking spaces shall be provided. Trees should be sized at 24-inch box or larger at the time of installation and remaining landscaped area shall contain understory planting.
2. Landscaping islands and landscaping fingers containing trees shall be a minimum of six feet in width.
3. Any portion of the parking area not used for parking, loading drive aisles, or pedestrian connectivity shall be landscaped.
4. Parking stalls shall be paved with permeable pavers or porous paving materials. Drive aisles and primary and secondary entrance roadways are excluded from this requirement.
5. Parking areas shall be designed to mitigate stormwater.
6. Landscaping within parking areas shall be protected from encroaching vehicles by concrete curbing or raised planting areas. Curb cuts shall be provided to allow stormwater drainage into landscaping islands and fingers.

05.2I Utilities and Service Areas

Utilitarian elements and loading/service areas shall conform to the following standards, with the exception of the LAX Northside Airport Support District which is excluded from these requirements.

1. All utility service equipment, including but not limited to meters, vaults, sprinkler risers, vacuum breakers, and all service and trash areas shall be screened from neighboring properties and public right-of-way and shall be located away from major pedestrian routes and outdoor seating areas. These areas shall be screened by landscape materials including trees, shrubs, and ground cover and/or fences or walls designed to conform to the standards outlined within this document.

05.21 Utilities and Service Areas, continued

<input type="checkbox"/>	2. No materials, supplies or equipment, including trucks or other motor vehicles (excluding company vehicles for passenger use) shall be stored on-site unless located inside a closed building or screened from public view.
<input type="checkbox"/>	3. Service areas shall be designed to minimize automobile/pedestrian conflicts.
<input type="checkbox"/>	4. Roof mounted equipment shall be screened at a minimum equal to the height of the equipment, using similar materials and colors as the primary building.
<input type="checkbox"/>	5. Walls designed to screen utilitarian equipment shall be a maximum of six (6) feet in height, measured from finish grade.
<input type="checkbox"/>	6. Loading areas shall be accommodated entirely on-site.
<input type="checkbox"/>	7. Areas dedicated to loading shall not be visible from a public street.
<input type="checkbox"/>	8. Ancillary buildings shall be built with permanent materials that relate in style and finish to the primary buildings with which they are associated.
<input type="checkbox"/>	9. Trash and recycling storage areas shall be located to the rear or sides of a building and shall be screened from public view with walls, berms, or landscaping.
<input type="checkbox"/>	10. Trash enclosures and loading areas shall be designed using similar materials and colors as the primary buildings with which they are associated.
<input type="checkbox"/>	11. Recycling bins shall be screened.
<input type="checkbox"/>	12. Functional building elements, such as roof scuppers and vents shall not be visible from a public street.
<input type="checkbox"/>	13. Sheet metal vents, pipe stacks, and flashing shall be similar in finish and color to the adjacent roof or wall material.

05.21 Walls and Fences

Fences and walls shall conform to the following standards.

<input type="checkbox"/>	1. Walls and fences are discouraged along interior lot lines, except for where Landscape Buffers or demonstrated security needs are required.
<input type="checkbox"/>	2. Recreation Areas shall be secured with an eight (8) foot tall fence and provide limited and controlled access to the general public.
<input type="checkbox"/>	3. Fences and walls not associated to Recreation or Buffer areas shall have a maximum height of eight (8) feet measured from the finished grade. A six (6) foot planting strip shall be located adjacent to walls and fences and shall include shrubs, vines and ground cover identified in Chapter 7.
<input type="checkbox"/>	4. Solid fences or walls shall be designed with both sides articulated with similar or complementary materials and colors as the primary building with which they are associated.
<input type="checkbox"/>	5. Chain link fencing (with or without slats), corrugated metal, and barbed/razor wire is prohibited within the Northside Center and Campus Districts.

05.21 Walls and Fences, continued

<input type="checkbox"/>	6. Long expanses of walls (50 feet or greater) shall be broken up with projections or recessed elements, landscape pockets or changes in materials.
<input type="checkbox"/>	7. Where a wall or fence is located adjacent to a public right-of-way, a minimum six (6) feet landscaped setback shall be provided.

05.2X Site Lighting

The following lighting standards apply:

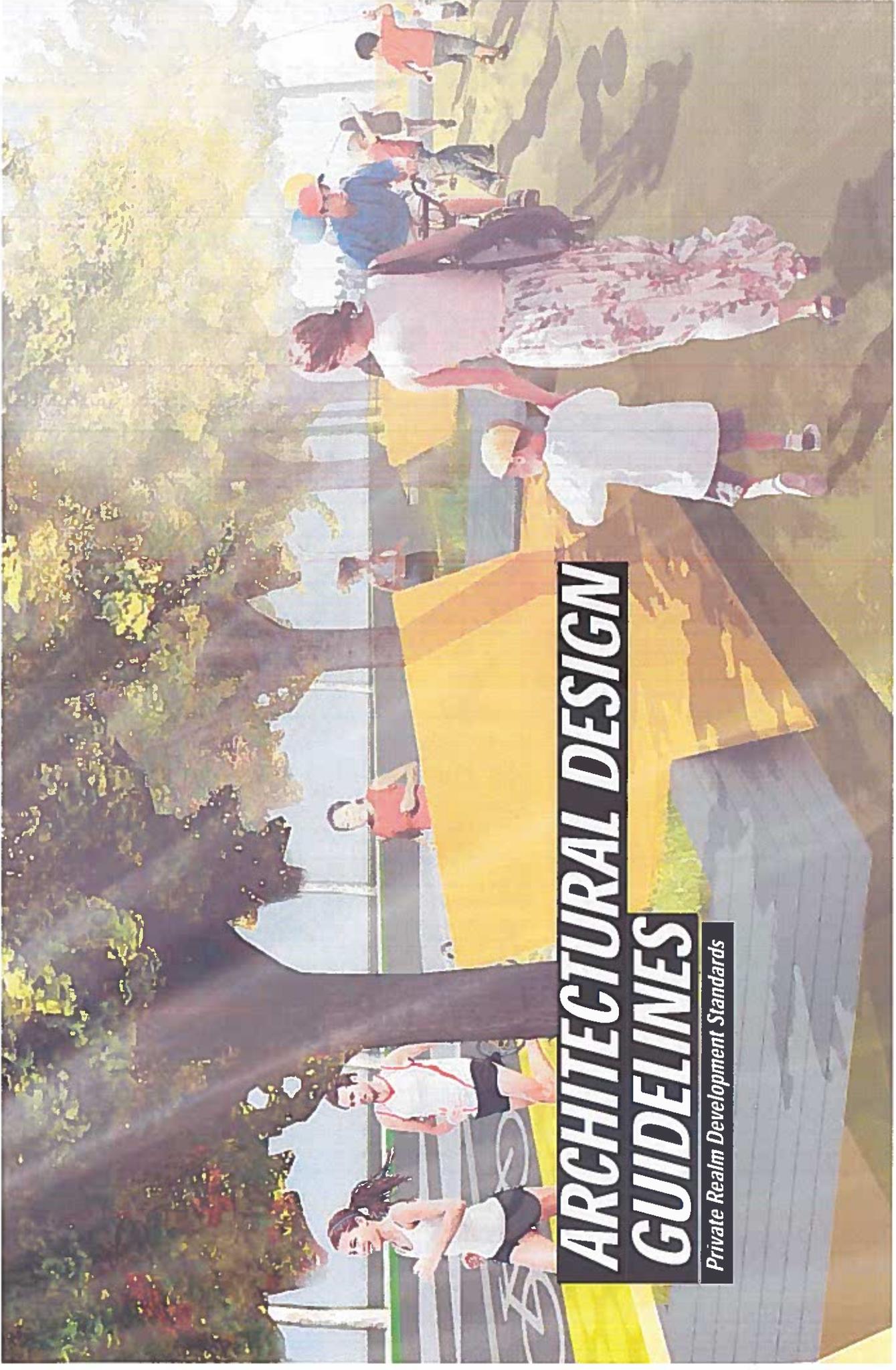
<input type="checkbox"/>	1. Glare or light trespass is prohibited on any adjacent streets, or within any adjacent properties.
<input type="checkbox"/>	2. Lighting mounted above ten (10) feet from finish grade shall incorporate a full cut-off shield fixture.
<input type="checkbox"/>	3. Lighting shall be provided in parking areas, near access drives, pedestrian pathways or crosswalks, and internal vehicular circulation areas.
<input type="checkbox"/>	4. The parking lot illumination level shall achieve a uniformity ratio of 3 to 1 (average to minimum) with a maintained average of 1 foot candle and minimum of .3 foot candle (minimum).
<input type="checkbox"/>	5. Service area lighting shall be contained within the service yard boundaries and enclosure walls. No light spillover shall occur outside the service area.
<input type="checkbox"/>	6. Pedestrian area lighting, including outdoor plazas, entry ways or other common areas shall achieve a uniformity ratio of 3 to 1 average to minimum, with an average illumination of .60 foot candles and minimum of .18 foot candles.
<input type="checkbox"/>	7. Pedestrian walking areas, such as the paseo or parking lot walkways, shall require point to point lighting at a minimum of twenty (20) feet between each point with no specific illumination levels required. Lighting shall clearly identify the pedestrian walking zone and direction of travel.

Landscape Buffer Fencing

LAX Northside Center District
Area 1 and Area 2b

These images depict the appropriate details and functional requirement specified for the fences that secure Buffer areas.





ARCHITECTURAL DESIGN GUIDELINES

Private Realm Development Standards



06 ARCHITECTURAL DESIGN GUIDELINES

LAX Northside Design Guidelines Checklist

06 Architecture Design Guidelines

The guidelines and standards provided within this chapter articulate the design expectations for the LAX Northside Campus District and LAX Northside Center District and are intended to be used in tandem with the Urban Design Guidelines located in Chapter 5. Along with the vision and direction provided within the rest of this document, the guidelines and standards address the minimum requirement for creating quality development.

06.1 Building Form

One of the key aspects to manage development within the LAX Northside is the reduction of impacts on adjacent uses, such as the working airfield and surrounding residential and commercial communities.

Standards

1. Building facades within 150 feet of neighboring residences shall be located to maximize privacy associated with abutting homes and shall incorporate two or more of the following strategies:

- Buildings shall be oriented to limit direct views into neighborhood homes or sensitive use spaces, such as the golf course, day care or existing condominium facilities.
- Off-set windows on walls adjacent to a neighboring residences to prevent direct views into neighboring windows.
- Utilize clerestory windows, translucent glass, and/or vision glass beginning in elevation for the second story or higher at a minimum of four (4) feet from finish floor to prevent direct sight lines into neighbors' windows and livable outdoor spaces.
- Use landscaping to provide a buffer or screening between properties.

2. No building facade shall extend more than eighty (80) feet in length without variations in the wall surface through setbacks or changes in the wall plane. Variations at a minimum must be four (4) foot offset horizontally.

3. Two or more of the following design strategies shall be used to reduce the perceived height, bulk, and massing of the building:

- Variation in the vertical wall in locations in excess of item 2 above.
- Variation in parapet or roof by more than two (2) feet for every forty (40) feet.
- Variation of roof types, or alternating roofs and parapets.
- Variation of facade material, so that no material is more than 35% of the total facade area, including glazing.

"Architecture" as described in this portion of the guidelines and standards refers to all buildings that provide character, identity, and form to the built environment in the LAX Northside. The purpose of the architectural design guidelines and standards is to establish a framework for the design and articulation of buildings within the LAX Northside, defining the standards for how the built environment will look and the character it will capture. These guidelines and standards are intended to achieve compatibility with adjacent communities and uses while maintaining the flexibility needed to respond to individual identities and the latest best-practices in building design and sustainability.

06.4 Parking Structures

Parking structures shall comply with the following standards.

Standards

1. Parking structures shall be designed and sited to reduce visual impact from public view and neighboring residential development.

2. The following strategies shall be considered when siting and designing a parking structure:

- o Minimize visual and lighting impacts on neighboring properties.
- o Vehicle ramps within the interior of the structure to limit headlight exposure.
- o Utilize exterior screen systems for planting and vegetation, or additional architectural articulation to improve aesthetic quality.
- o Provide additional accent or facade articulation at vehicular and pedestrian entries to the garage.

3. All lighting within and on the roof of the parking structure shall be shielded so that the light sources are not visible from adjacent property or rights-of-way.

4. A ten-foot minimum landscape strip shall be provided at the base of the structure where pedestrian or vehicle access is not provided.

06.5 Pedestrian Amenities

Additional information specific to the character, quality and appropriateness of pedestrian amenities is illustrated at the end of this Section.

1. Pedestrian amenities shall be selected to complement the overall character of the development and adhere to the following objectives:

- o Furnishings shall be attractive, functional, durable and easy to maintain.
- o Amenities shall help promote safe, visually pleasing, and comfortable pedestrian environments.

2. Trash receptacles, benches, bollards, planters and bike racks shall be located in areas with high pedestrian activity such as pedestrian walkways, project entry plazas and building entrances, seating areas, and transit stops.

4. Vertical circulation elements (stairs and elevators) shall be designed as an integral part of the overall architecture of the building and shall complement its massing and form.

5. Minor surface detailing shall not be used as a substitute for distinctive building massing. Minor surface detailing includes score lines or changes in color, rather than a change or relief in the wall plane.

06.2 Facade Articulation and Materials

Architectural details should be used to enhance buildings by adding color, shadows, and interesting forms. They should not, however, be used as a substitute for genuine building massing. This is particularly important on frontages facing Westchester Parkway, La Tijera Boulevard, and internal to the project area where buildings are oriented toward parking and primary vehicular access areas within the retail and office environments.

1. Building massing shall be broken down into smaller units, with vertical and horizontal queues related to the pedestrian scale.

2. Mirror or reflective surfaces shall not be primary building material.

06.3 Roofs

Roof design and mechanical equipment screening are important design features. Integrating full roof forms and elements, green roofs, and the application of a painted finish can provide opportunities to improve the visual quality of roofs.

1. Roof parapets shall be articulated with details including, but not limited to precast treatments, continuous banding, or projecting cornices, lintels, caps, corner details, or variety in pitch (for example, articulated, sculptural roof lines/forms).

2. Roof parapets shall not appear "tacked on" and shall convey a sense of permanence. Where lower or vertical elements are proposed, parapets shall wrap to create the appearance of a complete structure.

3. All roof mounted mechanical equipment shall be screened to the height of the equipment. Line of sight screening is not acceptable.

- o Buildings with flat or low-pitched roofs shall incorporate parapets, pitched facades, or architectural elements designed to screen roof mounted mechanical equipment.

- o Screening shall be architecturally compatible in color, shape, size, and material with the primary building and shall be carefully integrated into the overall building design.

4. Roof access shall be provided from the interior of the building. Exterior roof access ladders are prohibited.

5. Roof surfaces shall be light in color or are encouraged to be constructed as a green roof.

06.6 Building Lighting

Building lighting shall be designed and placed to limit impacts on adjacent properties or disrupt the function of the airfield. The quality of light, level of light as measured in foot-candles, and the type of bulb or source shall be carefully addressed. Lighting levels shall not be so intense as to draw attention to the flow or glare of the project site. Lighting shall incorporate current energy-efficient fixtures and technology.

- 1. Lighting shall be designed to provide ambiance, safety, and security without unnecessary spillover or glare onto adjacent properties. Indirect wall lighting or "wall washing" and overhead down lighting may be used to help reduce light trespass into adjacent properties.
- 2. Spot lighting or glare from any site lighting shall be shielded from adjacent properties and directed at a specific object or target area.
- 3. Building light fixtures shall be designed or selected to be architecturally compatible with the main structure.
- 4. When security lighting is necessary, it shall be recessed, hooded, and located to illuminate only the intended area. Off-site glare and light trespass shall be prevented.
- 5. Exposed bulbs are prohibited.

06.7 Stormwater Management

All parcels shall integrate Low Impact Design (LID) best practices into future developments to promote and facilitate water conservation.

- 1. Site development shall comply with all applicable Regional Water Quality Control Board and County of Los Angeles regulations for water quality and quantity including preparation of a Standard Urban Stormwater Mitigation Plan (SUSMP) with Operation and Maintenance Guidelines.
- 2. Natural vegetation and native and/or drought tolerant plants shall be planted in parking lot islands and other landscaped areas where feasible.
- 3. Natural drainage systems shall be utilized to the maximum extent feasible.
- 4. Impervious area shall be minimized.
- 5. Non-structural BMP's shall be used unless they are infeasible in which case the infeasibility shall be documented and structural Best Management Practices are implemented.
- 6. Stormwater shall be pre-treated prior to infiltration or discharge from site.



Bike Racks



Bollards

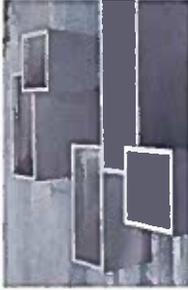


Trash Receptacles

Appropriate Pedestrian Amenities

LAX Northside Center and LAX Northside Campus Districts

The following images provide examples of sturdy, durable, and attractively designed pedestrian amenities. These images do not specify the specific items, but capture the spirit of the project in their articulation and represent input from community workshops.



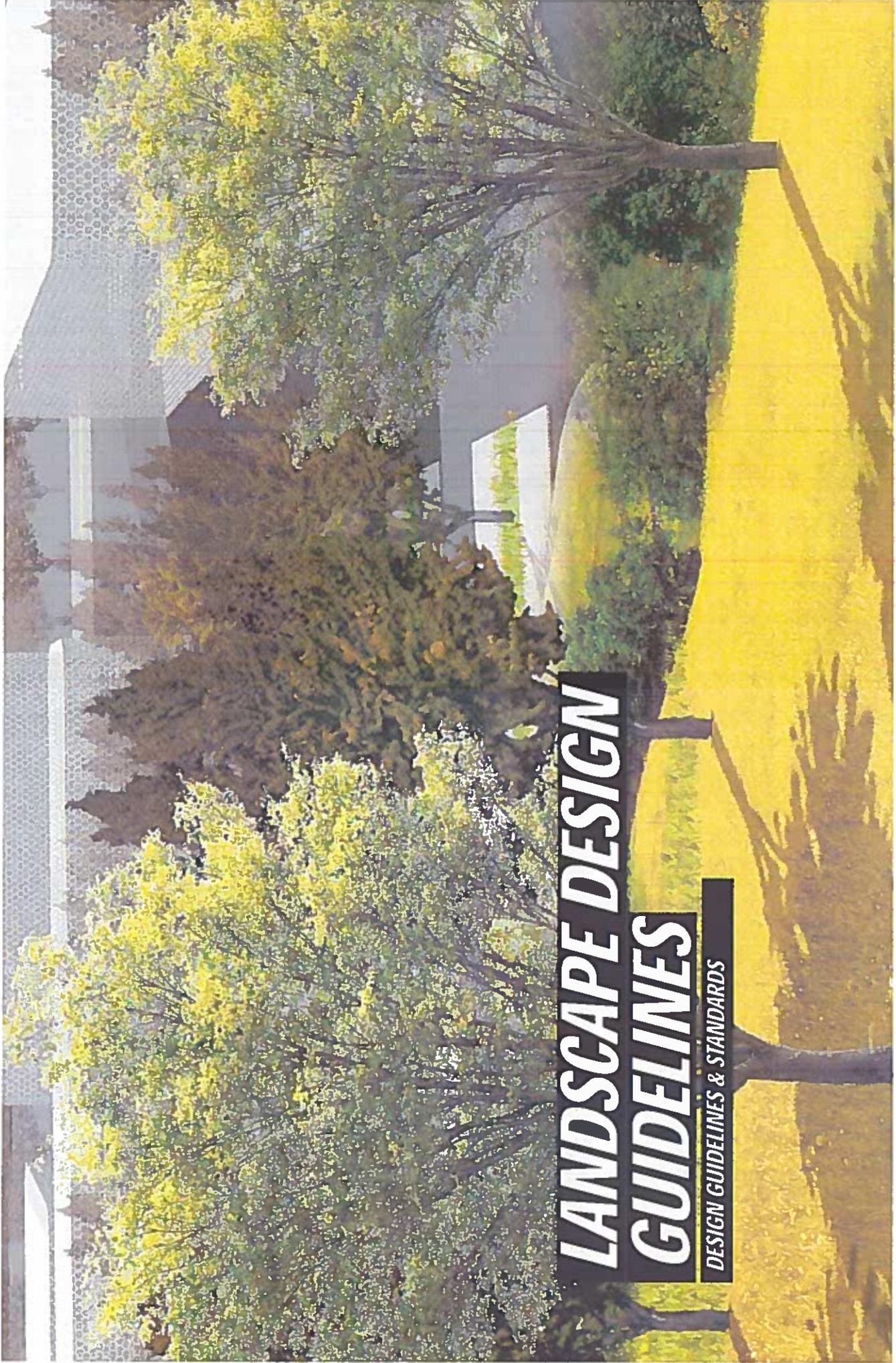
Planters



Benches



Benches



LANDSCAPE DESIGN GUIDELINES

DESIGN GUIDELINES & STANDARDS



07 LANDSCAPE DESIGN GUIDELINES

“Landscape” as described in this portion of the design guidelines and standards refers to all plant materials that provide character, identity and form to the natural environment in the LAX Northside. These items include streets and landscape setbacks, the variety and placement of selected plant materials, walkways and the paseo, signage, lighting, site furnishings, and arrangement of major functional elements including development entries, parking lots, buildings, service areas, and other locations throughout the LAX Northside. The objective of the Landscape Design guidelines and standards is to ensure that landscaping as a design element will help create, convey, and reinforce the overall character of the LAX Northside, even while project architecture and the design of building sites may vary in type, size, style and location.

The landscape design guidelines support the overall development concepts of the three Districts: the LAX Northside Center District, the LAX Northside Campus District, and the LAX Northside Airport Support District. For several operational reasons, it is critical to prevent future interactions between wildlife and the working airfield. An overall landscape concept has been established to achieve this goal. This concept presents a hybrid landscape that, much like a gradient, provides non-native planting strategies along Westchester Parkway, a mix of non-native and native plantings in the development zones and parking areas, and a full native planting palette for all areas that exist along the northern property lines, adjacent to the residential communities.

The landscape guidelines and standards have been organized around seven areas that exist within the three Districts of the LAX Northside. These areas have been selected to help focus specific plants from the overall planting palette into appropriate locations. The seven areas are 1) Landscape Setbacks, 2) Paseo and Streetscapes, 3) Airport Support, 4) Surface Parking, 5) Recreation, 6) Parking and Development, and 7) Urban Form Line. The following provide the conceptual direction for these areas:

Landscape Setback Zone

Landscape setbacks are used primarily to screen development from neighbors and differentiate boundaries along property lines. These areas, depending on their location within the LAX Northside, will consist of drought tolerant, low maintenance and foot traffic durable materials that provide options for trees, shrubs and groundcover. The palette will combine fifty (50) percent non-native and fifty (50) percent native plant materials.

Paseo and Streetscapes Zones

The palette will primarily be evergreen and non-native, allowing a consistent visual appeal year round, in addition to being drought tolerant and non-invasive. The palette will combine seventy (70) percent non-native and thirty (30) percent native plant materials.

Airport Support Zone

Plantings will be limited within the Airport Support zone due to its proximity to the adjacent airfield. Most plant material will be groundcover and shrubs, and limited trees. This zone will combine eighty (80) percent native and twenty (20) percent non-native plant materials.

LAX Northside Design Guidelines Checklist

07 Landscape Design Guidelines

All Projects shall comply with the following landscape standards.

07.1 Landscape Design

1. Landscaped areas shall be planted in accordance with the Landscape Zones established in this chapter.
2. Plant materials are restricted to those specified in the following plant palette and shall be located within the zones identified on the Landscape Zone map, Figure 7.2.
3. Landscapes are required to achieve the following percentage breakdown in their overall composition. These percentages are required on a project by project basis.

Planting Zone	Native (%)	Non-Native (%)
A. Paseo /Streetscapes	30	70
B. Landscape Buffers	100	
C. Landscape Seals	50	50
D. Parking and Developments	60	40
E. Recreation	80	20
F. Airport Support	80	20

4. Trees and large shrubs shall be planted at a spacing of two times the full growth radius. For example, a tree that grows to 30' canopy shall be planted 60' on center.
5. Casting of seeds for lawns, such as with hydro-seeding, is prohibited.

07.2 Site Maintenance

These maintenance guidelines shall apply to individual parcel developers and are applicable to all zones and districts within the LAX Northside.

1. Areas not used for structures, walkways, paved driveways, or storage areas shall maintain a well-kept landscaped condition and according to the maintenance specifications to be provided by the lessee.
2. All trees and large shrubs shall be regularly maintained in order to have a thin and open canopy.
3. All trees replaced within the medians and right-of-ways shall adhere to the species specified in this document and as stated below:
 - a. Any tree replaced in the median of Westchester Parkway shall be replaced with a Arborea 'Marina', also known as the Marina Strawberry Tree.
 - b. Any tree replaced along the right-of-way on Westchester Parkway or La Tijera Boulevard shall be replaced with a Metrobiota exoniata, also known as the New Zealand Christmas Tree.

Landscape Buffer Zone

The buffers will consist of one hundred (100) percent locally-native, drought tolerant plant materials intended to reduce undesirable impacts while requiring limited to no maintenance.

Recreation Zone

Primarily, all recreation areas will be designated in Area 1 and the western portion of Area 2. These areas are intended to be open space areas that require specific and particular groundcover for active playing fields, and intensive uses, such as dog parks and running paths. The planting palette for this area type is drought tolerant, non-invasive and will require frequent maintenance due to its use. The palette will favor locally native species and will combine eighty (80) percent native and twenty (20) percent non native plant materials.

Parking and Development Zone

One of the largest landscaped areas within the LAX Northside area will be the surface parking areas required for each development. The planting palette for these areas will consist of a hybrid mix of forty (40) percent non-native and sixty (60) percent native plants, and it is recommended that the trees, shrubs and groundcover options be compatible with storm water management systems, such as bioswales or permeable paving systems.

Urban Tree Line

One of the most distinguished design features presented for the LAX Northside is the introduction of a continuous line of trees running along Westchester Parkway. This line of trees will run the entire length of the LAX Northside and will provide an edge through which development frontages engage and interact. This row of trees is intended to be planted with a single tree species, the Aleppo Pine (*Pinus halepensis*) that is an evergreen species known for its low maintenance, capability for slender but tall growth in a conical form, and vibrant light green needles. This defining line will help create an identity for the LAX Northside, while buffering visual and audible impacts from future developments on adjacent neighbors.

Tables 07.2.1 through 07.2.7 detail the permitted plants for each planting zones, and estimated growth is provided for trees. All species allowed in the Landscape Buffer Zone are native, while the other zones allow a mix of native and non-native species.

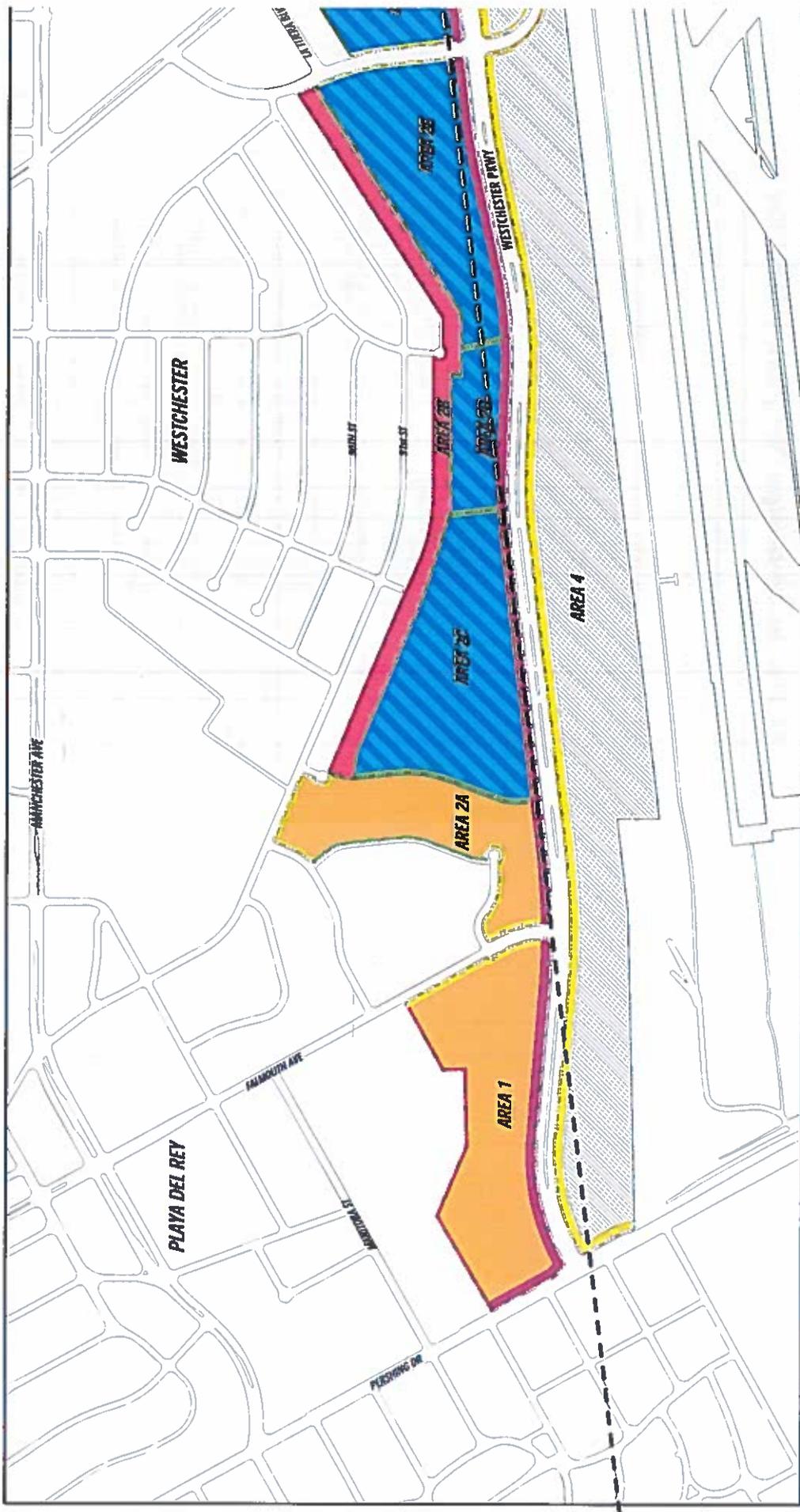


FIGURE 07.2
PLANTING ZONES

This figure provides information specific to the planting zones in the LAX Northside.



TABLE 07.2-1 // TREES

Estimated growth > 30' H

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES
1	Casuarina cunninghamiana	River She-Oak	70X30	X
2	Cupressus arizonica	Arizona Cypress	40X20	X
3	Cupressus guadalupensis	Guadalupe Cypress	50X15	X
4	Lyonothamnus floribundus	Catalina Ironwood	50X35	X
5	Melaleuca quinquevneria	Pink Tree	40X25	X
6	Pinus halpensis	Aleppo pine	60x40	X
7	Pinus pinea	Italian Stone Pine	75X50	X
8	Pinus torreyana	Torrey Pine	70X40	X

TABLE 07.2-2// SMALL TREES & LARGE SHRUBS

Estimated growth < 30' H

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES
1	Agonis flexuosa	Peppermint Tree	30x30	X
2	Arbutus Marina	Marina Strawberry Tree	25x40	X
3	Arbutus unedo	Strawberry Tree	30x30	X
4	Atriplex lentiformis lentiformis	Saltbush	15x10	X
5	Banksia ericifolia	Heath Banksia		X
6	Banksia integrifolia	Coast Banksia	30x30	X
7	Banksia praemorsa	Cut-leaf Banksia		X
8	Bulia Capitata	Pindo Palm or Jelly Palm	20X12	
11	Cassia leptophylla	Gold Medallion Tree	25X20	X
12	Ceanothus arboreus	Fetiteaf or Catalina Ceanothus	15-25x12	X
13	Cercis occidentalis	Western Redbud	15x10	X
14	x Chitalpa tashkentensis	Chitalpa	30x30	X
16	Corayline australis	Grass Palm	VAR.30	X
17	Dodonaea viscosa and cultivars	Hopseed Bush	18x12	X
18	Dracaena Draco	Dragon Tree	VAR.25	X
19	Eleagnus pungens	Silverberry		X
20	Garrya elliptica	Coast silk-tassel		X
21	Hakea suaveolens	Sweet Hakea		X
22	Heteromeles arbutifolia	Touyon	15x15	X

TABLE 07.2-2 // SMALL TREES & LARGE SHRUBS

Estimated growth < 30' H

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES						
23	Laurus nobilis	Sweet bay	30x20'	X	X	X	X	X	X	
24	Leptospermum laevigatum	Australian Tea Tree	25x25'	X	X	X	X	X	X	
25	Leucospermum cordifolium	Nodding Pincushion		X	X	X	X	X	X	
26	Lysiloma watsonii	Feather Bush	25x25'	X	X	X	X	X	X	
27	Maytenus phyllanthoides	Mangie Duice, Sweet Mangrove		X	X	X	X	X	X	
28	Metaleuca armillaris		15x30'	X						
29	Metaleuca elliptica	Granite Honey Myrtle	18x15'	X						
30	Metaleuca ericifolia	Swamp Paperbark	18x24'	X						
31	Metaleuca imatifolia	Flax Leaf Paperbark	30x25'	X	X	X	X	X	X	
32	Metaleuca hypericifolia	Hillock Bush		X	X	X	X	X	X	
33	Metaleuca nesophila	Pink Metaleuca	18x20'	X	X	X	X	X	X	
34	Metrosideros excelsa	New Zealand Christmas Tree	30x30'	X	X	X	X	X	X	
35	Metrosideros collina 'Springfire'	Lenua	18x18'	X	X	X	X	X	X	
36	Myrica californica	Pacific Wax Myrtle	15-30' H	X				X		X
37	Myrsine africana	African Boswood		X	X	X	X	X	X	
38	Parkinsonia x Desert Museum (Cercidium x)	Desert Museum Palo Verde	25x25'	X	X	X	X	X	X	
39	Protea spp	Protea		X	X	X	X	X	X	
46	Salix exigua	Narrow-leaf Willow								X
47	Salix lasiolepis	Arroyo Willow								X
48	Simmondsia chinensis	Jojoba		X	X	X	X	X	X	X

TABLE 07.2-3 // SHRUB & PERENNIALS

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES						
1	Acalypha californica	California copperleaf		X						
2	Arctostaphylos catalinae	Catalina Manzanita		X	X	X	X	X	X	
3	Arctostaphylos edmundsii	Little Sur Manzanita		X	X	X	X	X	X	
4	Arctostaphylos glandulosa	Eastwood Manzanita		X	X	X	X	X	X	
5	Arctostaphylos glauca 'Los Angeles'	Los Angeles Big Berry Manzanita		X	X	X	X	X	X	
6	Arctostaphylos hookeri	Hooker's Manzanita		X	X	X	X	X	X	
7	Arctostaphylos hybrids, cultivars	Manzanita		X	X	X	X	X	X	
8	Arctostaphylos insularis	Island Manzanita		X	X	X	X	X	X	

TABLE 07.2-3 // SHRUB & PERENNIALS

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH		ZONES								
			H	W	1	2	3	4	5	6	7		
9	Arctostaphylos microdensis	Park View Manzanita, Morro Manzanita	X	X	X	X	X	X	X	X	X	X	X
10	Arctostaphylos nummularia sensu lato	Glossleaf Manzanita	X	X	X	X	X	X	X	X	X	X	X
11	Arctostaphylos oregonensis (A. cruzensis)	SLO Valley Manzanita	X	X	X	X	X	X	X	X	X	X	X
12	Arctostaphylos pacifica s.	San Bruno Carpet	X	X	X	X	X	X	X	X	X	X	X
13	Arctostaphylos pajaroensis	Brother James Manzanita	X	X	X	X	X	X	X	X	X	X	X
14	Arctostaphylos pumila	Sandmat Manzanita	X	X	X	X	X	X	X	X	X	X	X
15	Arctostaphylos purissima	Ln Purissima Manzanita	X	X	X	X	X	X	X	X	X	X	X
16	Arctostaphylos ruelii	Sand Mesa Manzanita	X	X	X	X	X	X	X	X	X	X	X
17	Arctostaphylos uva-ursi	Bear Berry, Kinnikinnick	X	X	X	X	X	X	X	X	X	X	X
18	Agapanthus africanus	Lily Of The Nile	X	X	X	X	X	X	X	X	X	X	X
19	Artemisia californica	California Sagebrush	X	X	X	X	X	X	X	X	X	X	X
20	Artemisia oblongifolia	Mugwort	X	X	X	X	X	X	X	X	X	X	X
21	Asteriscus sericeus	Canary Island Daisy	X	X	X	X	X	X	X	X	X	X	X
22	Astragalus trichopodus var. lonchus	Santa Barbara Milk Veitch	X	X	X	X	X	X	X	X	X	X	X
23	Atriplex canescens	Four-wing Salt Bush	X	X	X	X	X	X	X	X	X	X	X
24	Atriplex nummularia	Saltbush or Sand Mat	X	X	X	X	X	X	X	X	X	X	X
25	Baccharis pilularis spp.	Coyote Bush	X	X	X	X	X	X	X	X	X	X	X
26	Calliandra californica	Red Baja Fairy Duster	X	X	X	X	X	X	X	X	X	X	X
27	Ceanothus foliosus	Waxy Leaf Mountain lilac	X	X	X	X	X	X	X	X	X	X	X
28	Ceanothus gloriosus	Mountain lilac	X	X	X	X	X	X	X	X	X	X	X
29	Ceanothus griseus	Carmel Ceanothus	X	X	X	X	X	X	X	X	X	X	X
30	Ceanothus heartiorum	San Simeon Ceanothus	X	X	X	X	X	X	X	X	X	X	X
31	Ceanothus maritimus	Bluff California lilac	X	X	X	X	X	X	X	X	X	X	X
32	Ceanothus thyrsifolius	Coastal Mountain lilac	X	X	X	X	X	X	X	X	X	X	X
33	Centaurea ciliarata	Velvet Centaurea, Dusty Miller	X	X	X	X	X	X	X	X	X	X	X
34	Cercocarpus betuloides	Mountain mahogany	X	X	X	X	X	X	X	X	X	X	X
35	Chamaelucium uncinatum	Wax Flower	X	X	X	X	X	X	X	X	X	X	X
36	Cistus spp.	Rock rose	X	X	X	X	X	X	X	X	X	X	X
37	Coreopsis gigantea	Giant coreopsis	X	X	X	X	X	X	X	X	X	X	X
38	Coreopsis maritima	Sea Dahlia	X	X	X	X	X	X	X	X	X	X	X
39	Coreopsis spp.	Australian Fuchsia	X	X	X	X	X	X	X	X	X	X	X
40	Dendromecon harfordii	Charmel Island Bush Poppy	X	X	X	X	X	X	X	X	X	X	X
41	Dendromecon rigida	Tree poppy, Bush Poppy	X	X	X	X	X	X	X	X	X	X	X
42	Dianella spp.	Dianella	X	X	X	X	X	X	X	X	X	X	X
43	Echium candicans	Primo of Madeira	X	X	X	X	X	X	X	X	X	X	X
44	Encelia californica	California Coast Sunflower	X	X	X	X	X	X	X	X	X	X	X
45	Encelia farinosa	Britchebush or Inchenso	X	X	X	X	X	X	X	X	X	X	X

TABLE 07.2-3 // SHRUB & PERENNIALS

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES								
				1	2	3	4	5	6	7		
46	Encelia ventorum	Baja Bush Sunflower		X	X	X	X	X	X	X	X	X
47	Euphorbia canum	California fuchsia		X	X	X	X	X	X	X	X	X
48	Eremophila hydropšana	Blue bells		X	X	X	X	X	X	X	X	X
49	Eromophila x	Emu Bush		X	X	X	X	X	X	X	X	X
50	Ericameria laurifolia	Turpentine Bush		X	X	X	X	X	X	X	X	X
51	Erigeron glaucus	Seaside daisy		X	X	X	X	X	X	X	X	X
52	Erigeron karwinskianus	Santa Barbara Daisy		X	X	X	X	X	X	X	X	X
53	Erigeron parvifolium	Coast Buckwheat		X	X	X	X	X	X	X	X	X
54	Eriophyllum nevini	Island snowflake, Camino Silverbacc		X	X	X	X	X	X	X	X	X
55	Erysimum insulare ssp. suffrutescens	Island Wallflower		X	X	X	X	X	X	X	X	X
56	Euphorbia rigida (E. biglandulosa)	Gopher Plant		X	X	X	X	X	X	X	X	X
57	Gaivazia speciosa	Island Snapdragon		X	X	X	X	X	X	X	X	X
58	Gaura lindheimeri	Gaura		X	X	X	X	X	X	X	X	X
59	Gaillardia	Blanket flower		X	X	X	X	X	X	X	X	X
60	Grevillea spp	Grevillea		X	X	X	X	X	X	X	X	X
61	Grindelia spp	Gum plant		X	X	X	X	X	X	X	X	X
62	Hamelia patens	Texas Firecracker		X	X	X	X	X	X	X	X	X
63	Harvardia pallens (Pithecolobium Paltens)	Haujillo		X	X	X	X	X	X	X	X	X
64	Hazardia cana	San Clemente Island Hazardia		X	X	X	X	X	X	X	X	X
65	Helichysum italicum	Licorice Plant		X	X	X	X	X	X	X	X	X
66	Heuchera sanguinea	Coralbelts		X	X	X	X	X	X	X	X	X
67	Iris douglasiana and PCH hybrids	Douglas Iris		X	X	X	X	X	X	X	X	X
68	Isooma menziesii var. menziesii	Coast Golden Bush		X	X	X	X	X	X	X	X	X
69	Isoomeris arborea	Blacklerpod		X	X	X	X	X	X	X	X	X
70	Ilex vomitoria 'Stokes'	Stokes Yaupon		X	X	X	X	X	X	X	X	X
71	Juniperus procumbens	Spreading Juniper		X	X	X	X	X	X	X	X	X
72	Juniperus rigida conifera	Shore Juniper		X	X	X	X	X	X	X	X	X
73	Justicia spicigera	Mexican Honeysuckle		X	X	X	X	X	X	X	X	X
74	Lavatera assurgensiflora	Island Tree Mallow		X	X	X	X	X	X	X	X	X
75	Lepectochia calycina	Pitcher sage		X	X	X	X	X	X	X	X	X
76	Lepetchinia fragrans	Fragrant Pitcher Sage		X	X	X	X	X	X	X	X	X
77	Lotus scoparius scoparius	Deerweed		X	X	X	X	X	X	X	X	X
78	Lupinus arboreus var. arboreus	Yellow Tree Lupino		X	X	X	X	X	X	X	X	X
79	Lupinus chamosis	Dune lupino		X	X	X	X	X	X	X	X	X
80	Lycium californicum	California Box Thorn		X	X	X	X	X	X	X	X	X
81	Mimulus aurantiacus	Slicky Monkeyflower		X	X	X	X	X	X	X	X	X
82	Mimulus clevelandii	Cleveland's Monkey Flower		X	X	X	X	X	X	X	X	X

TABLE 07.2-3// SHRUB & PERENNIALS

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES							
				1	2	3	4	5	6	7	
86	Oenothera elata hookerii	Hooker's Evening Primrose		X	X	X	X	X	X	X	X
87	Petalagonium tomentosum	Peppermint-scented Geranium		X	X	X	X	X	X	X	X
88	Penstemon spp	Penstemon		X	X	X	X	X	X	X	X
89	Phlomis fruticosa	Jerusalem Sage		X	X	X	X	X	X	X	X
90	Phormium tenax	New Zealand Flax		X	X	X	X	X	X	X	X
91	Polypodium californicum	California polypody fern		X	X	X	X	X	X	X	X
92	Rhaphirolepis spp	Hawthorn		X	X	X	X	X	X	X	X
93	Rhus integrifolia	Lemonade Berry		X	X	X	X	X	X	X	X
94	Rhus laurina	Laurel Sumac		X	X	X	X	X	X	X	X
95	Rosmarinus spp.	Rosemary		X	X	X	X	X	X	X	X
96	Russelia equisetiformis	Coral Fountain		X	X	X	X	X	X	X	X
97	Rusellia x	Coral Fountain cultivars		X	X	X	X	X	X	X	X
98	Salvia apiana	White Sage		X	X	X	X	X	X	X	X
99	Salvia brandegeei	Brandegee's Sage		X	X	X	X	X	X	X	X
100	Salvia clevelandii	Cleveland Sage		X	X	X	X	X	X	X	X
101	Salvia columbariae	Chia		X	X	X	X	X	X	X	X
102	Salvia leucantha	Mexican Bush Sage		X	X	X	X	X	X	X	X
103	Salvia leucophylla	Purple Sage		X	X	X	X	X	X	X	X
104	Salvia millifera	Black Sage		X	X	X	X	X	X	X	X
105	Salvia munzii	San Miguel Min or Munz's Sage		X	X	X	X	X	X	X	X
106	Salvia spathacea	Hummingbird Sage		X	X	X	X	X	X	X	X
107	Salvia spp	Sage, Non native spp		X	X	X	X	X	X	X	X
108	Senna oligophylla	Outback Cassia		X	X	X	X	X	X	X	X
109	Tecoma stans	Yellow bells		X	X	X	X	X	X	X	X
110	Tecoma hybrids/cultivars			X	X	X	X	X	X	X	X
112	Teucrium chamaedrys	Germander		X	X	X	X	X	X	X	X
114	Teucrium cosnii	Majorcan Germander		X	X	X	X	X	X	X	X
116	Teucrium fruticos	Bush Germander		X	X	X	X	X	X	X	X
118	Teucrium x lucidrys	Germander		X	X	X	X	X	X	X	X
118	Trichostema lanatum	Woolly Blue Curis or Romero		X	X	X	X	X	X	X	X
119	Westringia fruticosa	Coast Rosemary		X	X	X	X	X	X	X	X

TABLE 07.2-4 // GROUND COVER & VINES

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES									
				1	2	3	4	5	6	7			
1	<i>Abronia maritima</i>	Red sand verbena		X	X	X	X	X	X	X	X	X	X
2	<i>Abronia umbellata</i>	Beach Sand Verbena		X	X	X	X	X	X	X	X	X	X
3	<i>Abronia villosa</i>	Desert sand verbena		X	X	X	X	X	X	X	X	X	X
4	<i>Achillea millefolium</i>	Yarrow		X	X	X	X	X	X	X	X	X	X
5	<i>Ambrosia pumila</i>	San Diego Ambrosia											
6	<i>Antigonon leptopus</i>	Baja Red		X	X	X	X	X	X	X	X	X	X
7	<i>Armeria maritima</i>	Tinetti, Sea Pink		X	X	X	X	X	X	X	X	X	X
8	<i>Armeria douglasiana</i>	Mugwort		X	X	X	X	X	X	X	X	X	X
9	<i>Aster chilensis</i>	Coast Aster or California Aster		X	X	X	X	X	X	X	X	X	X
10	<i>Asteriscus maritimus</i>	Gold coin		X	X	X	X	X	X	X	X	X	X
11	<i>Asteriscus sericeus</i>	canary island daisy		X	X	X	X	X	X	X	X	X	X
12	<i>Atriplex barklayana</i>	Dwarf Saltbush											
13	<i>Atriplex leucophylla</i>	Beach Saltbush		X	X	X	X	X	X	X	X	X	X
14	<i>Bougainvillea</i>	Bougainvillea		X	X	X	X	X	X	X	X	X	X
15	<i>Calyptegia macrostegia</i>	Island Morning Glory		X	X	X	X	X	X	X	X	X	X
17	<i>Cissus</i> spp	Grape Ivy		X	X	X	X	X	X	X	X	X	X
18	<i>Distictis buccinatoria</i>	Blood red trumpet vine		X	X	X	X	X	X	X	X	X	X
19	<i>Eriophyllum confertiflorum</i>	Golden Yarrow		X	X	X	X	X	X	X	X	X	X
20	<i>Fragaria chiloensis</i>	Beach Strawberry		X	X	X	X	X	X	X	X	X	X
21	<i>Gazania rigens</i>	Trailing Gazania		X	X	X	X	X	X	X	X	X	X
22	<i>Geranium incanum</i>	Carpet Geranium		X	X	X	X	X	X	X	X	X	X
23	<i>Hardenbergia complanata</i>	Lilac Vine, Native wisteria		X	X	X	X	X	X	X	X	X	X
24	<i>Hardenbergia violacea</i>	Purple Vine Lilac		X	X	X	X	X	X	X	X	X	X
25	<i>Hibbertia scandens</i>	Guinea Gold Vine		X	X	X	X	X	X	X	X	X	X
26	<i>Hyloteleus undulatus</i>	Playa, Dragonfruit		X	X	X	X	X	X	X	X	X	X
27	<i>Kniphofia uvularia</i>	Hok Poken		X	X	X	X	X	X	X	X	X	X
28	<i>Lantana montevidensis</i>	Purple Trailing Lantana		X	X	X	X	X	X	X	X	X	X
29	<i>Lantana</i> spp.	Lantana		X	X	X	X	X	X	X	X	X	X
30	<i>Lavandula</i> spp	Lavender		X	X	X	X	X	X	X	X	X	X
31	<i>Lesingia filaginifolia</i>	California aster		X	X	X	X	X	X	X	X	X	X
32	<i>Lepidactylon californicum</i>	Prickly Philox		X	X	X	X	X	X	X	X	X	X

TABLE 07.2-4 // GROUNDCOVER & VINES

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES
33	<i>Limonium californicum</i>	Sea Lavender, Marsh Rosemary		X X X X X
34	<i>Lobelia laniflora</i>	Loose Flowered Lobelia		X X X X X
35	<i>Lonicera japonica</i>	Japanese Honeysuckle		X X X X X
36	<i>Lolium heermannii</i> var. <i>heermannii</i>	Heermann's Bird's Foot Trefail		X X X X X
37	<i>Monardaella linoides</i>	Willow Mint		X X X X X
38	<i>Muhlenbeckia complexa</i>	Wire Vine, Matress Vine		X X X X X
39	<i>Oenothera caespitosa</i>	White Evening Primrose		X X X X X
40	<i>Osteospermum</i> spp.	Trailing african daisy		X X X X X
41	<i>Parthenocissus quinquefolia</i>	Virginia Creeper		X X X X X
42	<i>Plectostachys serpyllifolia</i>	Dwarf Plectostachys, S. African Whit		X X X X X
43	<i>Rosa multiflora</i>	Baja wild rose		X X X X X
44	<i>Santolina chamaecyparissus</i>	Lavender, Cotton		X X X X X
45	<i>Sidalcea malviflora</i>	Crackerbloom		X X X X X
46	<i>Senecio flaccidus</i> var. <i>douglasii</i>	Butterwood, Bush groundsel		X X X X X
47	<i>Sphagneticola trilobata</i>	Yellow Dot		X X X X X
48	<i>Tanacetum camphoratum</i>	Camphor Dunn tansy		X X X X X
49	<i>Thymus</i> spp.	Thyma		X X X X X
50	<i>Thunbergia alata</i>	Black-eyed Susan		X X X X X
51	<i>Thunbergia grisea</i>	Orange Clock Vine		X X X X X
52	<i>Zephyranthes</i> s.	Rain Lily		X X X X X

TABLE 07.2-5 // HERBACIOUS & BULB-LIKE

QTY	SCIENTIFIC NAME	COMMON NAME	ESTIMATED GROWTH H x W	ZONES
1	<i>Allium haematochiton</i>	Red-Stunned Onion		X X X X X
2	<i>Asclepias speciosa</i>	Butterfly weed		X X X X X
3	<i>Calochortus catalinae</i>	Catalina mariposa lily		X X X X X
4	<i>Camissonia cheiranthifolia</i>	Beach evening primrose		X X X X X
5	<i>Drimiopsis maculata</i>	African Hosta		X X X X X
6	<i>Eschscholzia caespitosa</i>	Footfall Poppy		X X X X X
7	<i>Eschscholzia californica</i> var. <i>maritima</i>	California Poppy		X X X X X
10	<i>Ratibida columifera</i>	Mexican Hat		X X X X X

Landscape Materials & Their Locations

LAX Northside Center and LAX Northside Campus Districts

These images show examples of recommended plantings.



Urban Tree Line - Aleppo Pine



Westchester Parkway Median - Marina Strawberry Tree



Westchester Right-of-Way - New Zealand Christmas Tree



Parking Lot Bioswales - Jaumea



Parking Lot Bioswales - Arroyo Willow



Parking Lot Bioswales - Western Redbud



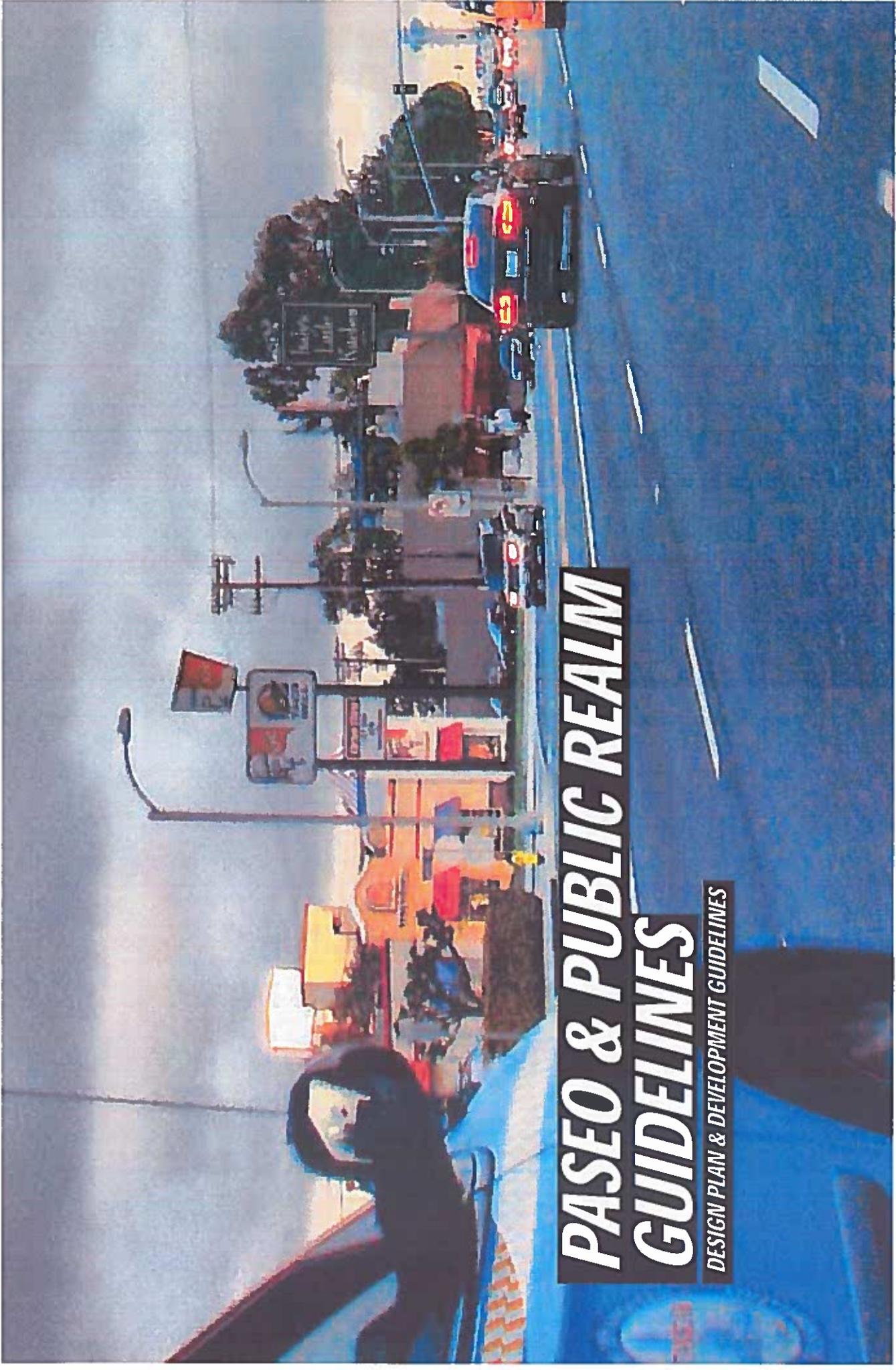
Buffer & Open Space - Coast Buckwheat



Buffer & Open Space - California Poppy



Buffer & Open Space - Beach Evening Primrose



PASEO & PUBLIC REALM GUIDELINES

DESIGN PLAN & DEVELOPMENT GUIDELINES



08 PASEO & PUBLIC REALM GUIDELINES

"Public realm" as described in this portion of the guidelines and standards refers to the extended right-of-way introduced in the LAX Northside that will accommodate the introduction of a continuous paseo experience connecting Sepulveda Boulevard with Pershing Drive along Westchester Parkway. The paseo will provide character, identity and form to the built environment in the LAX Northside. The purpose of the public realm and paseo guidelines and standards is to establish a framework for the design and articulation of active and passive recreation within the LAX Northside, defining the guidelines and standards for how this pedestrian infrastructure will look and the character it will capture. These guidelines and standards include restrictions intended to achieve compatibility with adjacent communities and uses, such as the airport, while maintaining the flexibility needed to respond to individual identities and the latest best-practices in recreation design and sustainability.

LAX Northside Design Guidelines Checklist

08 Public Realm and the Paseo

The public realm and paseo shall conform to the following standards.

08.1 Path Dimensions and Locations

Paving shall consist of stabilized decomposed granite in the following depths and locations:

- | | |
|--------------------------|---|
| <input type="checkbox"/> | 1. A minimum of twelve (12) feet between the existing sidewalk, within the 50-foot building setback located in Area 11 along the Runway Protection Zone (RPZ) boundary. |
| <input type="checkbox"/> | 2. A minimum of twelve (12) feet between the existing sidewalk and the 18-foot building setback located in Area 12A East along Westchester Parkway. |
| <input type="checkbox"/> | 3. A minimum of twelve (12) feet between the existing sidewalk and the 15-foot building setback located in Area 12A West along Westchester Parkway. |
| <input type="checkbox"/> | 4. A minimum of twelve (12) feet between the existing sidewalk and the 38-foot building setback located along Westchester Parkway in Areas 1, 2 and 3. |
| <input type="checkbox"/> | 5. A minimum of twelve (12) feet between the existing sidewalk and the 38-foot building setback located along Westchester Parkway and Pershing Drive in Area 1. |

08.2 Streetcapes

The public realm streetscape includes only the pedestrian accessible paseo and does not apply to existing right-of-way along Westchester Parkway.

Where sidewalks are being introduced, in particular Area 11 they shall be ten (10) feet wide and shall be designed to the standards set forth by the City of Los Angeles.

- | | |
|--------------------------|--|
| <input type="checkbox"/> | 1. All tree wells shall have root barriers to prevent material deterioration of the sidewalks and recreation paths. |
| <input type="checkbox"/> | 2. All soil in tree wells shall be finished with a minimum of two (2) inches of decomposed granite that is not stabilized. |

08.3 Street Furnishings

Street furniture elements include bench seating, bollards, planters, trash receptacles, and bike and newspaper racks located in the public right-of-way at locations such as bus shelters, street intersections, transit stations, and public plazas where high numbers of pedestrians commonly congregate or where entrances are provided to developments.

General objectives are as follows: 1) To provide street furniture and amenities that are functional, durable, and easy to maintain; 2) to provide street furniture which provides access and ease of use for handicapped persons; and, 3) to provide amenities to help promote safe, visually pleasing, and comfortable pedestrian environments.

08.3A Bench Seating

1. Benches shall be located along walkways, with a maximum distance of one thousand (1,000) feet between each seating area. In addition, various configurations and seat types shall be located in appropriate quantities to respond to user needs at transit stations, retail environments, bus shelters, street intersections, and public plazas.

2. Benches shall be durable and sturdy, with attractive design.

08.3B Bike Racks

1. Bike racks shall be located along walkways, near building entrances, intersections, transit stations, bus shelters, and any other pedestrian gathering areas. Spacing shall be at a maximum distance of one thousand (1,000) feet and in clusters of three (3).

2. Bike racks shall be durable and sturdy, with attractive design.

08.3C Lighted Bollards (excluding safety bollards)

1. Lighted bollards shall be located at street intersections where they will be used to define the boundary between pedestrian and vehicular zones. Lighted bollards may also be used to delineate pedestrian walkways.

2. Lighted bollard spacing shall be at a minimum distance of twenty (20) feet along both sides of the paseo.

08.3D Planters

1. Planters shall be used in conjunction with other street furniture, such as benches, bollards, or trash receptacles. Planters shall be located in areas where pedestrians gather.

2. Planters shall not exceed 36 inches in height.

3. Unless maintained on a regular schedule, all planters shall be irrigated.

4. Planters shall be planted with materials selected from the list of acceptable plants specified for the Paseo and Streetscape Zones located in sections 07.3 and 07.4 of this document.

08.3E Trash Receptacles

1. Trash receptacles shall be located along pedestrian walkways, near parcel entry plazas, seating areas, transit stops, public plazas, and other pedestrian gathering areas.

08.3E Trash Receptacles, continued

2. Trash receptacle spacing shall not exceed a distance of one thousand (1000) feet and shall be placed adjacent to benches and planters.

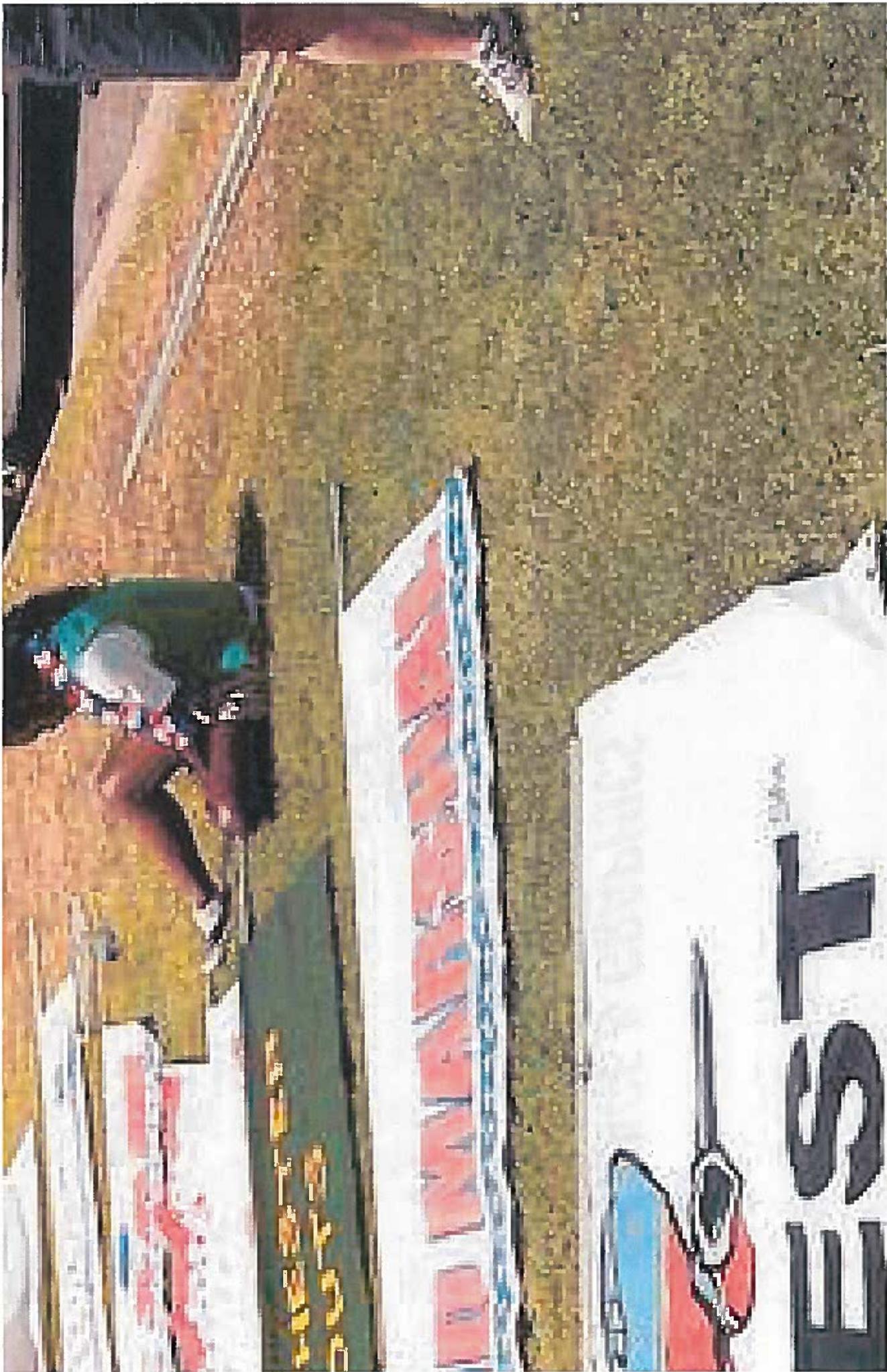
3. All trash receptacles shall be covered.



SIGNAGE & GRAPHICS GUIDELINES

DESIGN GUIDELINES & STANDARDS





09

SIGNAGE & GRAPHICS GUIDELINES

"Signage" as described in this portion of the design guidelines and standards refers to all graphic information that conveys location or direction and provides character, identity and form to the built environment in the LAX Northside. The purpose of the signage and graphics design guidelines and standards is to establish a framework for the design and character of signage for tenants and the public realm of the LAX Northside. This means defining the guidelines and standards for size, location, and material for signage. This Chapter includes the guidelines and standards to achieve compatibility with adjacent uses, while maintaining the flexibility to respond to individual identities.

LAX Northside Design Guidelines Checklist

09 Signage and Graphics Design Guidelines

09 Development Guidelines

Signage and graphics shall comply with the following standards.

09 Standards

1. Address signs shall be required for all tenants.

09.1 Tenant Identification Sign

"Identification Signs," as defined by these guidelines, refers to, and shall be limited to, tenant's trade name only. Tenant's customary signature or logo, hallmark, insignia, or other trade identification may be permitted as a form of identification signage.

- | | |
|--------------------------|---|
| <input type="checkbox"/> | 1. A maximum of two (2) illuminated identification signs on two separate elevations of the building are allowable. |
| <input type="checkbox"/> | 2. Signage shall be located on building frontages and primary entry facades. |
| <input type="checkbox"/> | 3. Illumination brightness of signs shall be restricted to no greater than two foot candles above ambient lighting, measured at the property line of the nearest residentially zoned property. |
| <input type="checkbox"/> | 4. Surface brightness of all translucent materials shall be consistent in all components of the sign. |
| <input type="checkbox"/> | 5. All conduits, lamps or transformers specific to the operation of the sign shall be entirely concealed within the sign. |
| <input type="checkbox"/> | 6. Tenant signage may not project above the building, as in the manner of common billboards. However, tenants are allowed to use the flat roof surfaces for signage and advertisements as long as the signage and/or advertisement is not visible to adjacent residential properties. |
| <input type="checkbox"/> | 7. An exposed light source (neon, incandescent) is prohibited. |
| <input type="checkbox"/> | 8. Signs employing animated components, moving/flashing or blinking lights, exposed raceways, exposed ballast boxes or transformers, unedged or unencapsled plastic letters or letters with no returns and exposed fastenings, luminous-vacuum formed type plastic letters, and sandblasted wood type construction shall be prohibited from use in the LAX Northside. |
| <input type="checkbox"/> | 9. Visible sign manufacturer's names, stickers, stamps or decals are prohibited on any sign or graphic. |
| <input type="checkbox"/> | 10. Simulated materials (i.e., wood grained plastic laminate, etc.) are prohibited as a primary sign surface. |

09.1 Tenant Identification Sign, Continued

- | | |
|--------------------------|---|
| <input type="checkbox"/> | 11. Each Identification Sign shall not exceed four (4) feet six (6) inches in height in the LAX Northside Center and three (3) feet in height in the LAX Northside Campus Districts. |
| <input type="checkbox"/> | 12. Signs shall not overlap or directly impact the clarity of architectural features, such as mullions or window breaks. |
| <input type="checkbox"/> | 13. Signs shall not overlap or directly impact the clarity of architectural features, such as mullions or window breaks. |
| <input type="checkbox"/> | 14. A maximum of two tenant identification signs shall be placed on a building, a maximum of one per façade can be illuminated. If a retail building is free standing, an additional identification monument sign is permitted. |
| <input type="checkbox"/> | 15. One monument sign is permitted at the site entrance of a recreational use and shall not exceed four (4) feet six (6) inches in height, measured from the finish grade. |

09.2 Temporary Signage

Temporary Signage, as defined in these guidelines refers to all forms of signage that are temporary in their use, such as construction related signage (walls/barricades, entries, etc.), lease signs, flags, banners and pennants.

- | | |
|--------------------------|--|
| <input type="checkbox"/> | 1. Lease signs attached to buildings shall be limited to the ground floor. |
| <input type="checkbox"/> | 2. A maximum of three flag poles for advertisement flags will be allowed for each building, and flag poles should be no taller than 30 feet. |
| <input type="checkbox"/> | 3. Flags shall be no larger than 6' x 9'. |
| <input type="checkbox"/> | 4. Banners and pennants shall only be permitted in interior court areas and not visible from public right-of-ways. |
| <input type="checkbox"/> | 5. Special occasion banners or pennants shall be temporarily erected. |
| <input type="checkbox"/> | 6. Temporary signs shall be submitted to the reviewing authority for approval prior to their installation. |
| <input type="checkbox"/> | 7. No temporary sign shall be visible for more than a consecutive 30-day term. |

09.3 Maintenance

These maintenance guidelines shall apply to individual parcel developers and are applicable to all Districts of the LAX Northside.

- | | |
|--------------------------|--|
| <input type="checkbox"/> | 1. Every sign shall be maintained in a clean safe and good working condition, including the replacement of defective parts, defaced or broken faces, lighting and other acts required for maintenance of the sign. |
| <input type="checkbox"/> | 2. Sign display surfaces shall be kept neatly painted or finished at all times. |
| <input type="checkbox"/> | 3. The base of any sign erected on the ground shall be kept clear of weeds, rubbish or other combustible material at all times. |
| <input type="checkbox"/> | 4. All signage shall be removed, or the face of said signs shall be removed and replaced with blank panels painted to match adjacent background colors within ninety (90) days of a close of business. |



RIOS CLEMENTI HALE STUDIOS

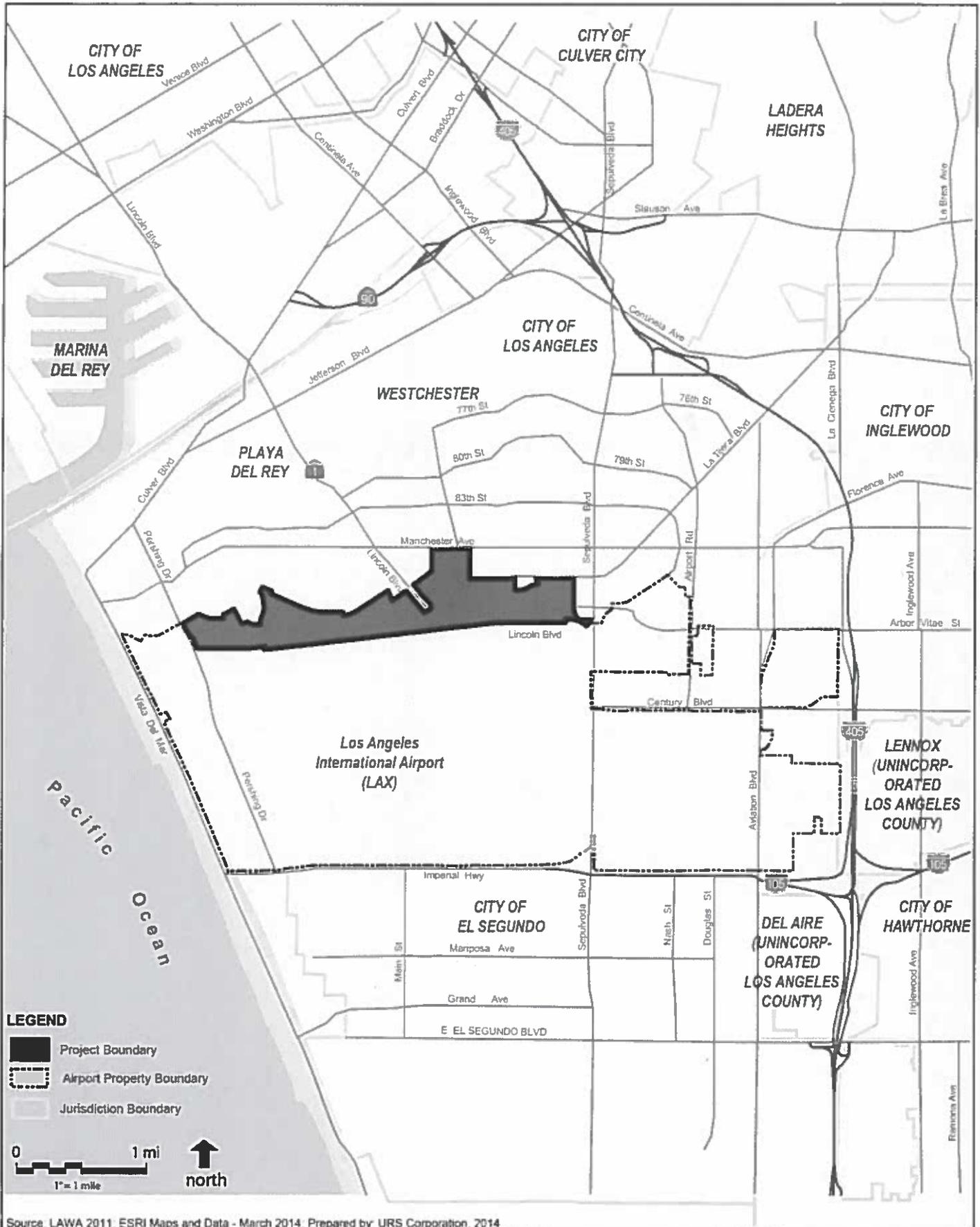
MAY 2011

Attachment D:

Northside Sub-area Map

Attachment E:

Vicinity Map

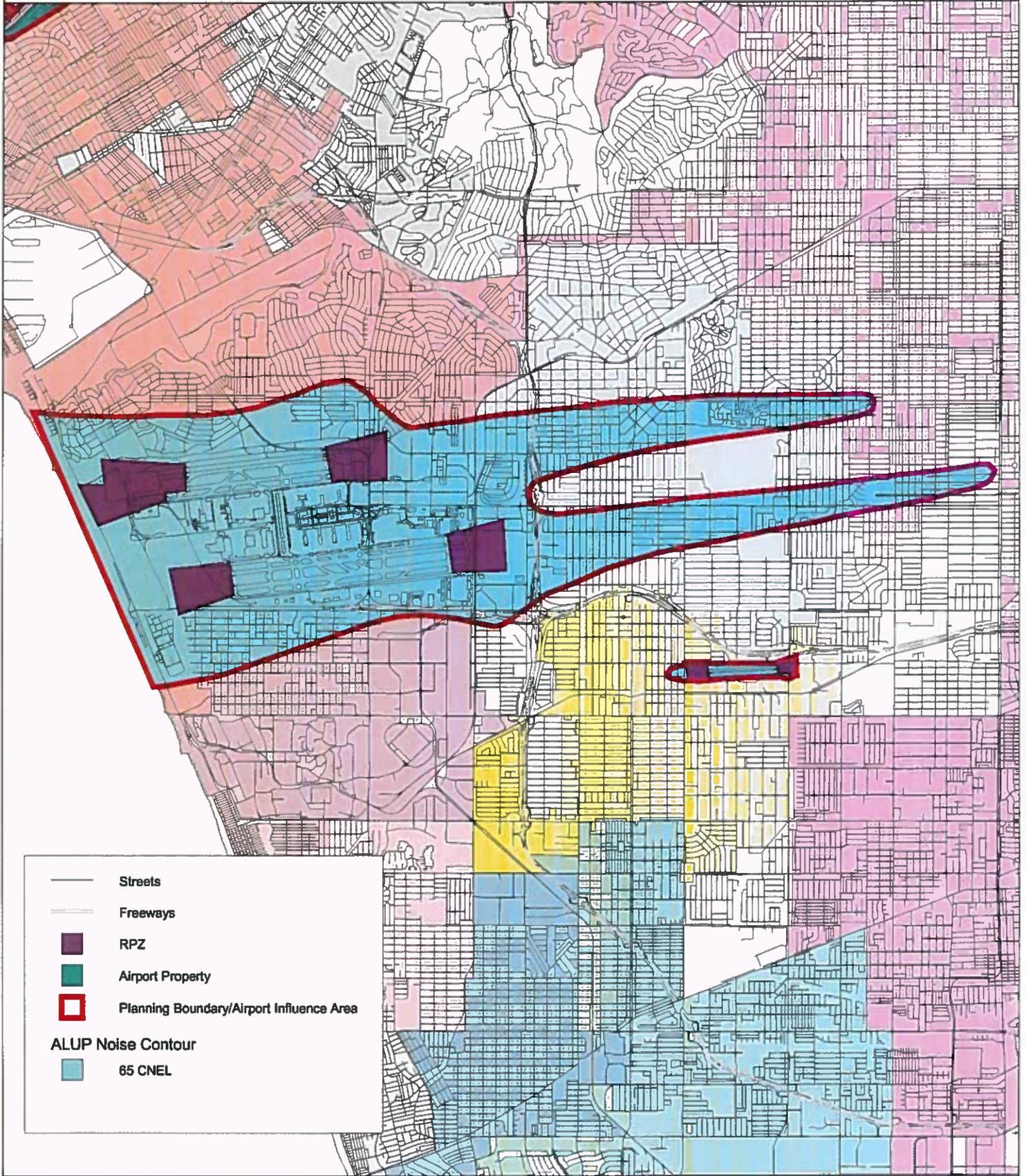


Source: LAWA 2011, ESRI Maps and Data - March 2014. Prepared by URS Corporation, 2014

Attachment F:

Airport Influence Area

LOS ANGELES INTERNATIONAL AIRPORT



-  Streets
-  Freeways
-  RPZ
-  Airport Property
-  Planning Boundary/Airport Influence Area

ALUP Noise Contour

-  65 CNEL



LOS ANGELES COUNTY
AIRPORT LAND USE COMMISSION
320 W. Temple Street
Los Angeles, CA 90012
(213) 974-6425

AIRPORT INFLUENCE AREA



5/13/03

Attachment G:

Land Use

Table 2-2

LAX Northside Plan Update Land Use Categories

Land Use Category	Permitted Uses
Office, Research and Development	Office (including Airport-related administrative offices), research and development, media, technology, higher educational, and parking (above and below ground), but excluding biological and/or hazardous materials research, development, or testing
Mixed Use- Commercial	Retail, commercial, office (including Airport-related administrative offices), restaurants, services, hotel, transit station, medical, and parking (above and below ground), except big box retail stores over 100,000 square feet and auto-dealerships
Community and Civic	Community-serving uses, nonprofits, civic uses, cultural uses, Airport-related community-serving uses, and parking (above and below ground)
Open Space and Recreation	Active and passive recreation, including but not limited to golf course, play fields, soccer fields, baseball and softball fields, dog parks; buffer areas; below-grade storm water treatment facilities; and parking (above and below ground)
Airport Support	Maintenance and repair shops, indoor storage and warehouses, administrative offices, radars, surveillance facilities, utilities, construction lay down, airport recycling yards, and parking (above and below ground), excluding aircraft engine testing

Note:

The list of permitted uses contained in this table is not exhaustive. Please see the Draft LAX Northside Specific Plan Amendment for a complete list of uses permitted by right and with restrictions.

Source: LAWA, 2014.

Attachment H:

LAX Specific Plan text amendments

2023-2024

2023-2024

Year	Month	Day	Event	Location	Time
2023	Jan	1	New Year's Day	Home	12:00
2023	Jan	15	Winter Solstice	Home	18:00
2023	Feb	1	Valentine's Day	Home	12:00
2023	Feb	15	Winter Carnival	City Park	10:00
2023	Mar	1	St. Patrick's Day	Home	12:00
2023	Mar	15	Spring Equinox	Home	12:00
2023	Apr	1	Easter	Home	12:00
2023	Apr	15	Spring Festival	City Park	10:00
2023	May	1	May Day	Home	12:00
2023	May	15	Summer Solstice	Home	18:00
2023	Jun	1	Father's Day	Home	12:00
2023	Jun	15	Summer Carnival	City Park	10:00
2023	Jul	1	Independence Day	Home	12:00
2023	Jul	15	Summer Festival	City Park	10:00
2023	Aug	1	August 1st	Home	12:00
2023	Aug	15	Summer Festival	City Park	10:00
2023	Sep	1	September 1st	Home	12:00
2023	Sep	15	Autumn Equinox	Home	12:00
2023	Oct	1	October 1st	Home	12:00
2023	Oct	15	Halloween	Home	18:00
2023	Nov	1	November 1st	Home	12:00
2023	Nov	15	Thanksgiving	Home	18:00
2023	Dec	1	December 1st	Home	12:00
2023	Dec	15	Winter Solstice	Home	18:00
2023	Dec	25	Christmas	Home	18:00
2024	Jan	1	New Year's Day	Home	12:00
2024	Jan	15	Winter Solstice	Home	18:00
2024	Feb	1	Valentine's Day	Home	12:00
2024	Feb	15	Winter Carnival	City Park	10:00
2024	Mar	1	St. Patrick's Day	Home	12:00
2024	Mar	15	Spring Equinox	Home	12:00
2024	Apr	1	Easter	Home	12:00
2024	Apr	15	Spring Festival	City Park	10:00
2024	May	1	May Day	Home	12:00
2024	May	15	Summer Solstice	Home	18:00
2024	Jun	1	Father's Day	Home	12:00
2024	Jun	15	Summer Carnival	City Park	10:00
2024	Jul	1	Independence Day	Home	12:00
2024	Jul	15	Summer Festival	City Park	10:00
2024	Aug	1	August 1st	Home	12:00
2024	Aug	15	Summer Festival	City Park	10:00
2024	Sep	1	September 1st	Home	12:00
2024	Sep	15	Autumn Equinox	Home	12:00
2024	Oct	1	October 1st	Home	12:00
2024	Oct	15	Halloween	Home	18:00
2024	Nov	1	November 1st	Home	12:00
2024	Nov	15	Thanksgiving	Home	18:00
2024	Dec	1	December 1st	Home	12:00
2024	Dec	15	Winter Solstice	Home	18:00
2024	Dec	25	Christmas	Home	18:00

LOS ANGELES INTERNATIONAL AIRPORT (LAX)

Specific Plan
~~Specific Plan~~

Ordinance No. ~~176,345-~~
~~176,345~~
Effective January 20, 2005

~~As amended by Ordinance No. 179,148 and _____ January 20,~~
~~2005~~

~~As amended by Ordinance Nos. 179,148 and _____~~

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Section 1.	Establishment of the LAX Specific Plan
Section 2.	Purposes
Section 3.	Relationship to the Los Angeles Municipal Code and Other Ordinances
Section 4.	Application of Specific Plan to Development in Specific Plan Area
Section 5.	Definitions
Section 6.	Safety of Airport Operations
Section 7.	LAX Plan Compliance Review
Section 8.	Land Use
Section 9.	Airport Airside Sub-Area
Section 10.	Airport Landside Sub-Area
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Section 12.	Transportation Regulations
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Section 14.	Sign Regulations
Section 15.	Severability
Section 16.	Certification and Signature Page

~~Appendix A. Requirements for Development within the LAX Northside Sub-Area~~

~~Appendix A. Requirements for Development within the LAX Northside Sub-Area~~

Map 1	Specific Plan Boundary Map
Map 2	Specific Plan Sub-Areas Map
Map 3	Parcels within the LAX Northside Sub-Area

Map 4 LAX Northside Sub-Area Land Use Map
Map 5 LAX Northside District Map

SUBMITTAL DRAFT

LAX Northside Plan Specific Plan Amendment

FebruaryAugustAUGUSTJULY 29, -2014

Ordinance No. _____
~~ORDINANCE NO. 182542~~

An ordinance establishing a Specific Plan, known as the LAX Specific Plan, within the LAX Plan area.

**THE PEOPLE OF THE CITY OF LOS ANGELES
DO ORDAIN AS FOLLOWS:**

Section 1. -ESTABLISHMENT OF THE LAX SPECIFIC PLAN.- The City Council hereby establishes the LAX Specific Plan for the area bounded generally by 88th Street, Manchester Avenue, and 91st Street on the north, Imperial Highway on the south, Pershing Drive on the west, and La Cienega Boulevard on the east, as shown on Map 1.

Sec. Section 2. PURPOSES.

This Specific Plan is intended to:

- 1.- Recognize the uniqueness of LAX as a regional economic engine, an international gateway to the Pacific Rim, and an important public amenity;
2. Provide regulatory controls and incentives for the systematic and incremental execution of the LAX Plan, an element of the General Plan, to provide for public needs, convenience and general welfare as the development of the airport necessitates;
3. Ensure the orderly development of infrastructure consistent with the intensity and design of the LAX Plan by establishing general procedures -for development within the Specific Plan Area;
- 4.- Promote the development of a regional airport system in Southern California through an improved regional ground access system;
5. -Provide the appropriate zoning regulations for the development of the LAX Plan in conformance with the goals and objectives of other local and regional plans and policies;
6. -Ensure the Los Angeles World Airport's ability to operate LAX safely and efficiently throughout implementation of the LAX Plan;
7. -Recognize the important relationship between LAX and its neighbors and avoid development impacts to the extent practical and feasible;
8. -Protect airport-related and community businesses by providing regulatory controls and incentives consistent with these goals; and

9. Ensure on-going participation in improvements to LAX by appropriate stakeholders- business, labor, community, airline industry trade groups, ~~and~~ government- through consultation with stakeholders.

Sec. Section 3. RELATIONSHIP TO THE LOS ANGELES MUNICIPAL CODE AND OTHER ORDINANCES.

A. The regulations of this Specific Plan are in addition to those set forth in the planning and zoning provisions of the Los Angeles Municipal Code (LAMC), Chapter 1 as amended, and any other relevant ordinances, and do not convey any rights not otherwise granted under the provisions and procedures contained in the LAMC or other ordinances, except as specifically provided in this ordinance.

B. -Wherever this Specific Plan contains provisions that establish regulations (including, but not limited to, standards such as densities, uses, parking, signage, open space and landscape requirements), which are different from, more restrictive or more permissive than would be allowed pursuant to the provisions contained in the LAMC or any other relevant ordinances, this Specific Plan shall prevail and supersede the applicable provisions of the LAMC and those relevant ordinances.

C. This Specific Plan shall supersede any and all "T" Tentative and "Q" Qualified classifications applicable to property within the Specific Plan Area prior to the adoption date of this Specific Plan, including those contained in Ordinance No. 159,526. ~~The substance of the conditions in Ordinance No. 159,526 is incorporated in this ordinance as Appendix A and relate to property generally known as LAX Northside.~~

D. Site Plan Review and "Major" Development Projects Ordinances. Notwithstanding LAMC Sections 16.05 and 12.24 U 14, Site Plan Review and "Major" Development Projects approvals shall not ~~be required~~apply within this Specific Plan Area.

E. Specific Plan Procedures. Notwithstanding LAMC Sections 11.5.7 B, 11.5.7 C, 11.5.7 D, 11.5.7 E, 11.5.7 H, and 11.5.7 J, Project Permit Compliance approvals, including modifications, adjustments and interpretations, shall not ~~be required~~apply within ~~the Airport Airside and Airport Landside Sub Areas~~ of this Specific Plan Area.

F. Mini-Shopping Centers and Commercial Corner Development Ordinance. Notwithstanding LAMC Sections 12.22 A 23 and 12.24 W 27, Mini-Shopping Center and Commercial Corner Development approvals shall not ~~be required~~apply within this Specific Plan Area.

G. Landscape Ordinance. The provisions of LAMC Sections 12.21 A 6, 12.40, 12.41, 12.42, and 12.43 shall not apply within the Specific Plan Area.

H. Hotels When Located Within 500 Feet of an A or R Zone. Notwithstanding LAMC Section 12.24 W 24, approvals for hotels located within 500 Feet of an A or R Zone shall not be required within this Specific Plan Area.

I. Parking. Where this Specific Plan contains language or standards that require more parking or permit less parking than LAMC Section 12.21 A 4, this Specific Plan shall supersede the LAMC. In addition, the requirements of Los Angeles Ordinance No. 182,386 shall not be required within this Specific Plan Area.

J. Telecommunications. Notwithstanding LAMC Section 12.24 W 49, approvals for Telecommunications Facilities shall not be required within this Specific Plan Area.

~~**J. Telecommunications.** Notwithstanding LAMC Section 12.24 W 49, approvals for Telecommunications Facilities shall not be required within this Specific Plan Area.~~

K.- Kennels When Located Within 500 Feet of a Residential Zone.- Notwithstanding LAMC Section 12.24 W 25, approvals for kennels or facilities for the boarding of dogs when located within 500 feet of any residential zone shall not be required within this Specific Plan Area.

L. Citywide Design Guidelines. Any ordinance amending LAMC Section 11.5.4 to require projects to comply with the Citywide Design Guidelines shall not apply within this Specific Plan Area.

~~See Section 4.~~ 4. APPLICATION OF SPECIFIC PLAN TO DEVELOPMENT -IN SPECIFIC PLAN AREA.

A. Except for the provisions of ~~Section~~**Sections 7 and 11**, the regulations of this Specific Plan apply to all development located in the Specific Plan Area which requires a grading, building, mechanical, electrical, plumbing and/or sign permit. ~~The provisions of Section~~**Sections 7 and 11** apply only to Projects as defined in this Specific Plan.

B. No grading permit, building permit, or use of land permit shall be issued, and no construction shall occur, for any development within the Specific Plan Area unless the Executive Director has approved the proposed development.

~~See Section 5.~~ DEFINITIONS. Whenever the following terms are used in this Specific Plan, with the first letter or letters capitalized, they shall be construed as defined in this section. Words and phrases not defined in this section shall be construed as defined in LAMC Section 12.03.

Administrative Clearance. A ministerial process to determine compliance of a Project with all applicable provisions of this Specific Plan and the Design Guidelines and Guidelines and Standards for the LAX Northside Sub-Area, as issued by the Executive Director pursuant to Section 11.

Air Cargo Facility. Buildings, including any associated equipment and vehicles, used for the storage and transport of cargo to and from commercial aircraft.

Airport. An area of land that is used or intended to be used for the landing and taking off of

aircraft, and includes its buildings, facilities and ancillary uses, if any.

Airport Airside Sub-Area. –That area as shown on Map 2.

Airport ~~landside~~ Landside Sub-Area. –That area as shown on Map 2.

Applicant. Any entity, LAWA or any person, as defined in LAMC Section 11.01, submitting a request for LAX Plan Compliance Review or Administrative Clearance.

Automated People Mover System (APM). A rail or fixed guideway based transportation system or systems that moves passengers to and from the Central Terminal Area to the landside access facilities (CONRAC AND ITF) and other mass transportation facilities in an above-grade configuration, and to and from the Central Terminal Area to the Tom Bradley International Terminal and the Midfield Satellite Concourse in a below-grade configuration.

BOAC. The Board of Airport Commissioners of Los Angeles World Airports.

Central Terminal Area (CTA). The primary passenger check-in and processing center and the transition to and from landside facilities (ITF and CONRAC).

CEQA. California Environmental Quality Act.

City Engineer.– The City Engineer of the Department of Public Works, Bureau of Engineering, or his or her designee.

Commercial Passenger Vehicle Staging and Holding Area. An area for taxis, rental car company, hotel/motel, door-to-door, and scheduled buses and vans, chartered buses and vans, etc. to wait before picking up passengers.

Consolidated Rental Car Facility (CONRAC). A consolidated facility to accommodate rental car operators at LAX. This facility may include a customer service facility, ready/return garage, rental car storage, and maintenance support.

Executive Director. The Executive Director of Los Angeles World Airports, or his or her designee.

Federal Aviation Administration (FAA). A federal agency charged with regulating air commerce to promote its safety and development, encouraging and developing civil aviation, air traffic control and air navigation, and promoting the development of a national system of airports.

Fixed-Base Operators (FBO). A person, firm or corporation authorized by issuance of a commercial operator's permit to provide aeronautical activities, services or products at the airport for compensation or hire.

Fuel Farm. –Facility used for the storage of aircraft jet fuel prior to distribution to the aircraft via hydrant system or fueling trucks.

Intermodal Transportation Facility-Facility (ITF). A facility providing remote passenger pick up and drop off areas, public parking, and connections to public transit and other commercial vehicles (i.e. door-to-door shuttles and scheduled buses).

LADOT. The City of Los Angeles Department of Transportation.

LADOT General Manager. The General Manager of the City of Los Angeles Department of Transportation, or his or her designee.

LAMC. Los Angeles Municipal Code.

LAWA. Los Angeles World Airports.

LAX. Los Angeles International Airport.

LAX Master Plan Stakeholder Liaison. A person appointed by BOAC to assist the board and other decision-makers in communicating with the stakeholders regarding LAX Master Plan decisions.

LAX Northside. The development project that is to be built out within the LAX Northside Sub-Area, previously the subject of Ordinance No. 159,526 and a Final Environmental Impact Report.

LAX Northside Sub-Area. That area as shown on Map 2-4.

LAX Plan. The area plan adopted by City Council as the land use element of the City's General Plan for the airport and LAX Northside.

LAX Plan Compliance Review. -A determination issued pursuant to Section 7 of this Specific Plan. Approvals pursuant to Section 7 of this ordinance are not required for a Project within the Northside Sub-Area of this Specific Plan.

Midfield Satellite Concourse. A component of the terminal facilities located west of the existing Central Terminal Areas accessed via the Automated People Mover System. This component includes uses such as passenger waiting areas, aircraft loading and unloading, retail, and airline operations areas.

Mitigation Monitoring and Reporting Program (MMRP). The program adopted by BOAC and City Council for applying, monitoring and reporting with respect to the LAX Plan's master plan commitments and mitigation measures as future site specific improvements and uses contemplated by the LAX Plan are developed. The Board of Airport Commissioners may modify the program in accordance with CEQA.

~~**Midfield Satellite Concourse.** A component of the terminal facilities located west of the existing Central Terminal Area accessed via the Automated People Mover System. This component includes uses such as passenger waiting areas, aircraft loading and unloading, retail, and airline operations areas.~~

~~Mitigation Monitoring and Reporting Program (MMRP). The program adopted by BOAC and City Council for applying, monitoring and reporting with respect to the LAX Plan's master plan commitments and mitigation measures as future site specific improvements and uses contemplated by the LAX Plan are developed. The Board of Airport Commissioners may modify the program in accordance with CEQA.~~

Navigational Aid (Nav Aid). Any facility used by an aircraft or its pilot for guiding or controlling flight in the air or the landing or takeoff of an aircraft.

Project. The construction, erection, addition to, or structural alteration of any building or structure, or use of building or land, or change of use of building or land located in whole or in part within the Airport Airside or Airport Landside Sub Areas of the Specific Plan Area.

A Project shall not include the following:

1. Interior or exterior remodeling of a building;
2. The change of use of a building or land, or the relocation of existing uses, if the change of use or relocation: (a) changes the footprint of a building or structure by 10% or less in square feet; or (b) increases the usable floor area by 10% or less;
3. Activity requiring building permits for the alteration of structures solely relating to mechanical, electrical, or plumbing work internal to the structure, fascia, or any combination of these;
4. Signs;
5. Fences/walls, retaining walls, or support structures;
6. The placement of Navigational Aids and any other equipment mandated by the FAA, TSA, or any other governmental agency;
7. Improvements mandated by the FAA, TSA, or any other governmental agency;
8. Airline operation activities;
9. Non-Master Plan Project infrastructure upgrades, airport maintenance activities, and repair of existing facilities approved by BOAC;
10. Temporary uses, for a period not to exceed 120 days;
11. Emergency uses and/or activities in response to safety and security issues (i.e., activities required by heightened security levels or unsafe operating conditions);
12. Special events, as authorized by the Executive Director or BOAC;
13. Notwithstanding LAMC Section 12.23 A 4, the rehabilitation or reconstruction of a conforming or nonconforming building or structure that was damaged or destroyed by fire, flood, wind, earthquake, or other natural or man-made disaster;

14. Any construction required in order to comply with an order issued by the U.S. Department of Transportation, the FAA or the Department of Building and Safety to repair or replace an unsafe or substandard condition;

15. ~~Construction, the value of which is within the jurisdiction of the Executive Director to approve and does not need to be approved by BOAC under Los Angeles Administrative Code Section 10.1.1 and BOAC Resolution No. 19593 or any subsequent amendment; or~~

16. Security improvements to existing facilities.

Run-up Enclosure. Specialty facility used to test aircraft engines and disperse sound to reduce noise impacts on surrounding areas.

Runway. A defined rectangular area on the airport used to prepare for the takeoff or landing of aircraft along its length.

Specific Plan Area. That area within the heavy dashed lines on Map 1 in this Specific Plan.

Taxiway. A specially designated and prepared surface on an airport, including a taxilane, for aircraft to taxi to and from Runways, hangars, and aircraft parking positions.

Trip. A vehicle trip will be administratively defined by agreement between the General Manager of LADOT and LAWA to include the entrance or exit of a vehicle from airport or airport-related property.

~~Sec. Section~~ 6. -SAFETY OF AIRPORT OPERATIONS. Notwithstanding any other provision of this Specific Plan, no use, development or activity within the Specific Plan Area may compromise the safety of airport flight operations in any way. ~~Final authority for determining whether airport flight operation safety is compromised rests solely with the U.-S. Department of Transportation and the FAA.~~

~~Sec. Section~~ 7. LAX PLAN COMPLIANCE REVIEW.

A. General. The provisions of Subsections B, C, D, E and H of LAMC Section 11.5.7 do not apply to any Projects proposed for construction within the Airport Airside and Airport Landside Sub Areas of the Specific Plan Area.

B. Prohibition. ~~No~~ Except for the LAX Northside Sub-Area, no grading permit, building permit, or use of land permit shall be issued, and no construction shall occur, on any Project within the Airport Airside and Airport Landside Sub-Areas unless the City Council grants an LAX Plan Compliance approval pursuant to the procedures ~~set forth in this section.~~

C. Recommendation by Executive Director. ~~The Executive Director shall have the authority to recommend approval, approval with conditions, modification or denial of a request for an LAX Plan Compliance determination. This recommendation shall be made to BOAC and the City Council pursuant to the procedures set forth in this section after consideration of the traffic generation report and aviation activity analysis required in Subsection G 1 below and following consultation with the LAX Master Plan Stakeholder Liaison.~~

D. Findings. The Executive Director shall recommend to BOAC that the proposed Project be granted an LAX Plan Compliance approval upon written findings that the Project satisfies each of the following requirements:

1. **LAX Plan Consistency.** That the Project complies with the LAX Plan, any design guidelines required by the LAX Plan, and all applicable provisions of this Specific Plan; and

2. **Environmental Compliance.** That the Project has been adequately analyzed in compliance with CEQA, and the applicable master plan commitments and mitigation measures ~~contained in the MMRP (as may be modified by BOAC in accordance with CEQA) or identified in any subsequent environmental review have been incorporated into the Project to the extent feasible.~~

E. Rights Granted Under LAX Plan Compliance. ~~The issuance of an LAX Plan Compliance approval indicates compliance with the LAX Plan and this Specific Plan, but does not in any way indicate compliance with other applicable provisions of LAMC Chapter I (Planning and Zoning Code), nor with Chapter IX (Building Code).~~

F. Procedures.

1. The Executive Director shall review a Project for LAX Plan Compliance based upon the following information:

(a) A written description of the Project including location, size, proposed use, and any other pertinent information;

(b) A completed initial environmental study, including a traffic study, where appropriate, or other analysis;

(c) The most recent annual traffic generation report as required in Subsection G 1 below; and

(d) The most recent annual aviation activity analysis as required in Subsection G 1 below.

2. Executive Director's Review.

(a) Upon receipt of a request for review, the Executive Director shall transmit a copy of the written description of the Project and appropriate documents to the LADOT General Manager, the City Engineer, the Councilmember of the district in which the Specific Plan Area is located and the LAX Master Plan Stakeholder Liaison, and post notice of the application on the LAWA website. -The LADOT General Manager and the City Engineer shall submit any written comments concerning parking, driveways, access, circulation, and infrastructure improvements to the Executive Director within 15 working days from the date the documents were received, unless the LADOT General Manager and the Executive Director agree more time is necessary. -The Executive Director shall ensure that LAWA communicates with the LAX Master Plan Stakeholder Liaison. -The Executive Director shall consider the comments and concerns of the stakeholders as early in the process as reasonable.

(b) The Executive Director shall determine whether the Project complies with the LAX Plan and all applicable provisions of this Specific Plan.

(c) The Executive Director shall determine whether the environmental clearance for the Project complies with CEQA.

(d) If the Executive Director determines that the Project is consistent with the LAX Plan, all applicable provisions of this Specific Plan and with the requirements of CEQA, the Executive Director shall prepare a written report and transmit this report to BOAC for its action on the LAX Plan Compliance request. This written report shall include findings to support the Executive Director's recommendation, the applicable master plan commitments and mitigation measures, the applicable mitigation measures identified in any subsequent environmental review, the applicable traffic improvements and right-of-way dedications, and any conditions of approval that shall be imposed on the Project. -As a part of this written report, the Executive Director shall summarize the traffic generation report and aviation activity analysis required in Subsection G 1 below, and the results of the consultation with the LAX Master Plan Stakeholder Liaison. -The Executive Director shall also attach the reports submitted by the LADOT General Manager and the City Engineer.

(e) If the Executive Director determines that the Project is not consistent with the LAX Plan and all applicable provisions of this Specific Plan, the Executive Director may direct staff to reconsider the Project, analyze or redesign the Project, or recommend that **BOAC applicant** seek an amendment to the LAX Plan and/or an amendment or exception to the Specific Plan pursuant to LAMC Sections 11.5.6 and/or 11.5.7 F and G, as appropriate.

~~the LAX Plan and/or an amendment or exception to the Specific Plan pursuant to LAMC Section 11.5.6 and/or 11.5.7 F and G as appropriate.~~

3. Notice Requirements for BOAC Hearing. After receipt of the Executive Director's report and recommendation, BOAC shall set the matter for hearing. Notice of the time, place, and purpose of the hearing shall be given in the following manner:

(a) By posting the BOAC meeting agenda in the LAWA offices, on the LAWA website, and on the City of Los Angeles website at least 72 hours prior to the meeting.

(b) For the CTA, ITF, CONRAC, Midfield Satellite Concourse, and APM (except that portion that connects the CTA with Tom Bradley International Terminal and the Midfield Satellite Concourse) Projects, notice shall also be given by sending written notice by First Class Mail at least 15 days prior to the meeting date to: the Department of City Planning; the Department of Building and Safety; the Councilmember(s) of the district in which the Specific Plan Area is located; LADOT, the Bureau of Engineering; the LAX Master Plan Stakeholder Liaison; the Department of Neighborhood Empowerment; the West Los Angeles Area Planning Commission; the president(s) of local neighborhood councils; the Airlines for America trade association, Airlines Airport Affairs Committee, and any other airline industry trade groups identified by the LAX Master Plan Stakeholder Liaison as stakeholders; the LAX Coastal Chamber of Commerce; and local homeowners groups and interested parties who have filed a written request with LAWA.

4. Recommendation by BOAC. BOAC shall recommend to City Council that it approve, approve with conditions, modify or deny a request for LAX Plan Compliance. BOAC shall make the same findings required to be made by the Executive Director, supported by facts in the record. BOAC shall recommend that all appropriate master plan commitments and mitigation measures, the applicable mitigation measures identified in any subsequent environmental review, and any other requirements are included as a condition of the approval.

5. City Council Determination. City Council shall approve, approve with conditions, modify or deny a request for LAX Plan Compliance. The City Council shall make the same findings required to be made by the Executive Director, supported by facts in the record. The City Council shall ensure that all appropriate master plan commitments and mitigation measures, the applicable mitigation measures identified in any subsequent environmental review, and any other requirements are included as a condition of the approval.

~~Except for the Projects referred to as the Intermodal Transportation Facility, Automated People Mover System, and North Runway configuration, listed in Subsection H1 below, if the City Council does not take final action on the application for an LAX Plan Compliance determination within 45 days of receipt of the recommendation from BOAC, the recommendation from BOAC shall become the final decision on the application. Within this 30-day period, any action of the City Council to modify BOAC's recommendation or deny an LAX Plan Compliance determination required ten affirmative votes.~~

~~Except for the Projects referred to as the Intermodal Transportation Facility, Automated People Mover System, and North Runway configuration listed in Subsection H 1 below, if the City Council~~

~~does not take final action on the application for an LAX Plan Compliance determination within 45 days of receipt from the recommendation from BOAC, the recommendation from BOAC shall become the final decision on the application. Within this 30 day period, any action of the City Council to modify BOAC's recommendation or deny an LAX Plan Compliance determination requires ten affirmative votes.~~

G. Monitoring and Reporting.

1. LAWA shall prepare and submit annual reports to BOAC, the Department of City Planning, LADOT and City Council for the following:

(a) **Traffic Generation Report.** A traffic report, based on the information required by Section 12 C 1, that identifies the current number of Trips being generated by LAX (inclusive of all three Sub-Areas), the number of Trips anticipated to be generated at the completion of any Master Plan Project(s) in development at the time of the report, the Trips proposed to be generated following the full implementation of the Master Plan as informed by current and Project-based Trip counts, and the number of Trips anticipated to be generated by ~~on-going~~ Master Plan construction activities.

(b) **Aviation Activity Analysis.** An aviation activity analysis that identifies the current number of passengers, volume of air cargo and aircraft operations served at LAX, and the volume of aviation activity anticipated to be served by on-going Master Plan construction activities. In order to monitor that regional aviation system improvements are taking place in a timely manner, LAWA will also compile aviation activity statistics for other airports in the Los Angeles region for monitoring and reporting purposes. Passengers, volume of air cargo and aircraft operations activity at all airports with scheduled passenger or cargo activity in Los Angeles, Orange, Riverside, San Bernardino and Ventura counties will be compiled in coordination with the Southern California Association of Governments (SCAG). The analysis shall also include the proportion of aviation activity served at each airport in the region.

(c) **Mitigation Monitoring and Reporting Program.** A status report on compliance with Master Plan commitments and mitigation measures contained in the Mitigation Monitoring and Reporting Program.

2. The first of each annual report required in Subdivision 1 shall be submitted one year following the City Council's approval of the LAX Plan and MMRP, and the last report shall be submitted at the end of the year following completion of any pending Master Plan Project.

~~3. As part of the Amendment Study requirement in Subsection H below, LAWA shall prepare and submit to BOAC and the City Council a study analyzing the potential security benefits of those projects yet to be initiated in the Master Plan.~~

H. Additional Study Requirements.

1. **Specific Plan Amendment Study.** LAWA shall initiate a Specific Plan Amendment Study with corresponding environmental analysis in compliance with

CEQA, in the following ~~two~~three circumstances:

(a) If the annual traffic generation report required in Subsection G 1 above, and/or the annual traffic generation report considered together with any Project-specific traffic study, shows that any Master Plan Projects will be generating net new airport peak hour Trips in excess of 8,236 (unless the total Trips for that year are related to construction or phasing impacts).

(b) If the annual aviation activity analysis required in Subsection G 1 above forecasts that the annual passengers for that year are anticipated to exceed 78.9 million.

2. ~~2-~~ LAX Domestic Passenger and Airline Markey Survey/Study. LAWA shall ~~initiate~~initiate an LAX ~~Domestic~~Domestic Passenger Survey/Study and corresponding Airline Survey/Study, if the annual aviation activity analysis required in Section G 1 above forecasts that the annual passengers for that year are anticipated to exceed 75 million.

(a) **LAX Domestic Passenger Survey and Study.** LAWA shall conduct a survey and study of LAX domestic passengers (those passengers not flying internationally or connecting to international flights) designed to identify, at a minimum, (i) those LAX domestic passengers with origination or destination locations closer to other commercial airports in the region; (ii) why those domestic passengers chose to fly ~~ou~~of, or into, LAX rather than another commercial airport closer to their location of origin or destination; and (iii) what actions, consistent with federal, state and local laws, LAWA could take to encourage those domestic passengers to use an airport closer to their location of origin or destination for domestic flights.

(b) **Airline Survey and Study.** Upon completion o the LAX Domestic Passenger Survey and Study described in 2(a) above, LAWA shall conduct a survey and study of airlines then serving the Southern California commercial air travel market designed to identify what action(s), consistent with federal, state and local laws, LAWA could take to encourage those airlines to provide increased domestic service at other airports in the region, particularly those owned or operated by LAWA.

I. LAX Master Plan Stakeholder Liaison. The LAX Master Plan Stakeholder Liaison shall communicate with the stakeholders on Master Plan issues. The LAX Master Plan Stakeholder Liaison shall ensure that notice is provided to the stakeholders at the earliest reasonable time of initiation of Projects and any Specific Plan Amendment Study.

See Section 8. LAND USE.

A. Designation of Sub-Areas. The Specific Plan is divided into three Sub-Areas, as

shown on Map 2. The Sub-Areas are designated as: Airport Airside, Airport Landside, and LAX Northside.

B. Unified Development. For purposes of applying building ordinances and regulations, as well as the regulations of this Specific Plan, to the Airport Airside and Airport Landside Sub-Areas, these areas may be treated as a unified development (or unified developments).

1. A unified development shall mean an area or development that:

(a) has a combination of functional linkages, such as pedestrian or vehicular connections; and

(b) is composed of two or more contiguous parcels or lots of record, except where divided by a public street (or streets).

2. Before applying building ordinances and regulations to any area or development within the Airport Airside and Airport Landside Sub-Areas, the Department of Building and Safety shall determine whether or not the area or development is a unified development, or part of a unified development, as defined in this subsection. The following provisions may be applied to any area or development determined to be a unified development:

(a) **Location of Parking Area.** Notwithstanding LAMC Section 12.21 A 4(g), parking to serve the area or development may be located at any location within the Airport Airside and Airport Landside Sub-areas. Further, the provisions of LAMC Section 12.26 E 5 shall not apply;

(b) **Floor Area Ratio (FAR).** The total buildable area of all parcels may be used to determine FAR;

(c) **Building Site.** Construction of a building and/or structure may be across common interior property lines;

(d) **Driveways, Entry/Exiting, and Drainage.** Driveway access, building entry and exiting, and drainage may be across common interior property lines;

(e) **Fire Resistance and Opening Protection of Exterior Walls.** The requirements for fire resistance and opening protection of exterior walls are not applicable to the common interior property lines. The property line abutting public roadways or adjacent to properties outside of the boundaries of the unified development is not a common interior property line;

(f) **Construction and Maintenance.** The construction and maintenance of all buildings, structures, and site improvements within each unified development shall be in accordance with all building ordinances and regulations that would otherwise be applicable if the buildings, structures, and improvements were located on or within a single lot.

C. Yard and Setback Regulations. Notwithstanding the provisions set forth in the LAMC, no front, side, or rear yards or building setbacks are required in the Airport Airside or Airport

Landside Sub-Areas. –Landscape buffers shall be consistent with any street frontage and landscape guidelines as may be required by the LAX Plan and adopted by BOAC. –These landscape buffers shall not be measured from lot lines, but rather from perimeter roadways - or as specified otherwise in any adopted street frontage and landscape guidelines.

Development in the LAX Northside Sub-Area shall provide setbacks and landscape buffers as specified in Section 11 and the referenced LAX Northside Design Guidelines and Guidelines and Standards, as may be amended by BOAC from time to time.

~~Development in the LAX Northside Sub Area shall provide setbacks and landscape buffers as specified in Appendix A.~~

Sec. Section 9. AIRPORT AIRSIDE SUB-AREA.

A. Purpose.– To allow for the safe and efficient operation of airport airfield activities. Aircraft are permitted to operate under power in this Sub-Area, except as prohibited in Subsection D below.

B. Permitted Uses. –The following uses shall be permitted in the Airport Airside Sub–Area, also designated as the LAX-A Zone, within the Specific Plan Area, subject to approval by the Executive Director:

1.- All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14, including, but not limited to:

- (a) Airline clubs, retail uses, and restaurants;
- (b) Aviation school;
- (c) Establishments for the sale and service of alcoholic beverages for on-site and off-site consumption shall be allowed as permitted by and pursuant to any application procedures set forth in the LAMC;
- (d) Incidental retail uses- permanent or temporary retail uses, which may include kiosks and carts; and
- (e) Surface and structured parking lots (including those at-grade, above–grade, and subterranean).

2. All of the uses permitted in the M2 Zone, as specified in LAMC Section 12.19, including, but not limited to:

- (a) –Aircraft under power;
- (b) –Airline catering and food preparation;
- (c)– Airline maintenance and support, including but not limited to storage, aircraft engine or airframe repair and testing, and aircraft maintenance shops;_
- (d) Air Cargo Facilities;
- (e)– Commercial Passenger Vehicle Staging and Holding Area;
- (f) –Fixed-Base –Operators;
- (g)– Helicopter operations, including but not limited to helicopter landings and take-offs, helipads, heliports, and helistops;_
- (h) Navigational Aids and any other equipment mandated by the FAA, TSA, or any other governmental agency;
- (i) Run-up Enclosures;
- (j) Runways, Taxiways, aircraft parking aprons, and service roads; and

(k) Passenger handling facilities, including but not limited to baggage handling and processing, passenger holdrooms, boarding gates, ticketing, and passenger -check-in functions.

3. In addition, the following uses shall be permitted:

- (a) Aggregate/asphalt grinding and recycling facility;
- (b) Airport police fire arms training facility;
- (c) Aircraft rescue, firefighting and training facilities;
- (d) Automated People Mover System, its stations and related facilities;
- (e) Compressed Natural Gas/~~Liquified~~Liquefied Natural Gas (“CNG/LNG”) stations, central utility plant, and other fueling and energy sources;
- (f) Fuel Farm;
- (g) Hazardous waste storage;
- (h) Hydrogen cell;
- (i) Oil drilling for remediation purposes only;
- (j) Security-related equipment and facilities;
- (k) Surface water runoff treatment plant;
- (l) Uses customarily incident to any of the above uses, and accessory buildings or uses;
- (m) Uses and operations determined to be of a similar nature, or deemed necessary for the safe and efficient operation of the airport, by the Executive Director; and
- (n) All uses requiring a Conditional Use Permit pursuant to LAMC Section 12.24.

C. Prohibited Uses.

The following uses shall be prohibited within the LAX-A Zone:

Any building containing dwelling units.

D. Imperial Terminal Area. The Imperial Terminal Area is the approximately 42.5-acre area north of Imperial Highway between Main Street and California Street, as shown on Map 2. In this area, aircraft maneuvering may be conducted by tug and tow procedures. The use regulations, for both permitted and prohibited uses, specified in this section for the LAX-A Zone shall apply, except, the following uses shall be prohibited:

- 1. Aircraft under power; and
- 2. Helicopter operations, including but not limited to helicopter landings and take-offs, helipads, heliports, and helistops.

See Section 10. -AIRPORT LANDSIDE- SUB-AREA.

A. Purpose. To allow for the safe and efficient operation of airport facilities, the primary function of which is to provide access to the airport and process passengers. Aircraft are not permitted to operate under power in this Sub-Area.

B. Permitted Uses. -The following uses shall be permitted in the Airport Landside Sub-

Area, also designated as the LAX-L Zone, within the Specific Plan Area, subject to approval by the Executive Director:

1. All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14, including, but not limited to:

- (a) Airline clubs, retail uses, and restaurants;
- (b) Aviation school;
- (c) Establishments for the sale and service of alcoholic beverages for on-site and off-site consumption shall be allowed as permitted by and pursuant to any applicable procedures set forth in the LAMC;
- (d) Rental car operations, including but not limited to vehicle maintenance and car washing;
- (e) Incidental retail uses- permanent or temporary retail uses, which may include kiosks and carts; and
- (f) Surface and structured parking lots (including those at-grade, above-grade, and subterranean).

2. All of the uses permitted in the M2 Zone, as specified in LAMC Section 12.19, including, but not limited to:

- (a) Airline catering and food preparation;
- (b) Airline maintenance and support, including but not limited to storage, aircraft engine or airframe repair and testing, and aircraft maintenance shops;
- (c) Air Cargo Facilities;
- (d) Commercial Passenger Vehicle Staging and Holding Area;
- (e) Helicopter operations, including but not limited to helicopter landings and take-offs, helipads, heliports, and helistops;
- (f) Navigational Aids and any other equipment mandated by the FAA, TSA, or any other governmental agency;
- (g) Passenger handling facilities, including but not limited to baggage handling and processing, passenger holdrooms, boarding gates, ticketing, and

3. In addition, the following uses shall be permitted:

~~passenger check-in functions; and~~

~~(h) Service roads.~~

(a) Passenger pick up and drop off areas;

(b) Automated People Mover System, its stations and ~~to stations and~~ related facilities;

(c) CNG/LNG stations, central utility plant, and other fueling and energy sources;

(d) Security-related equipment and facilities;

(e) Uses customarily incident to any of the above uses, and accessory buildings ~~or~~ and uses;

(f) Uses and operations determined to be of a similar nature, or deemed necessary for the safe and efficient operation of the airport, by the Executive Director; and

(g) All uses requiring a Conditional Use Permit pursuant to LAMC Section 12.24. Notwithstanding the foregoing, as specified above, all establishments

for the sale and service of alcoholic beverages shall be allowed as permitted by and pursuant to any applicable procedures set forth in the LAMC.

C. Prohibited Uses.

The following uses shall be prohibited within the LAX-L Zone:

- (a) Aircraft under power; and
- (b) Any building containing dwelling units.

See Section 11. LAX NORTHSIDE SUB-AREA.

A. Purpose. To provide for the redevelopment of land previously used for residential purposes with uses that are consistent with airport needs and neighborhood conditions. This Sub-Area serves as an airport buffer zone for the Westchester community.

B. General. The LAX Northside Sub-Area has fifteen areas - Areas 1, 2, 3, 4A, 4B, 5 through 11, 12A, 12B and 13 - as shown on Map 3. The provisions of Section 7 do not apply to a Project proposed for construction within the LAX Northside Sub-Area of the Specific Plan Area.

C. Relationship to Ordinance No. 159,526. The LAX Northside Sub-Area, also designated as the LAX-N Zone, is comprised of property previously entitled under Ordinance No. 159,526. The limitations, including permitted uses, requirements and conditions of development in Ordinance No. 159,526 are incorporated into the Specific Plan as Appendix A. The requirements in Appendix A shall apply to all development in this Sub-Area, and is incorporated, as appropriate, in this ordinance in Section 11, and in the referenced LAX Northside Plan Design Guidelines and Standards, as may be amended by BOAC from time to time.

D. Project Permit Compliance Review. The Project Permit Compliance Review Administrative Clearance. The procedures set forth in LAMC Section 11.5.7 C this Subsection D shall apply to all projects in the LAX Northside Sub-Area.

1. Except as provided herein, no grading permit, foundation permit, building permit, use of land permit, or permit for a change of use shall be issued for any Project on any lot located in whole or in part within the LAX Northside Sub-Area unless an Administrative Clearance has been issued pursuant to the procedures set forth in this Section. The provisions of this Section shall replace the Project Permit Compliance provisions in LAMC Section 11.5.7. The provisions of Subsections B, C, D, E and H of LAMC Section 11.5.7 do not apply to any Projects proposed for construction within the Specific Plan Area.

a. No Administrative Clearance shall be approved for a Project that would cause overall development in the Specific Plan Area to exceed the Site Development Standards in Subsection H below.

b. No Administrative Clearance shall be issued unless the Project complies with all applicable provisions of this Specific Plan, including compliance with all applicable Project Design Features and Mitigation Measures as set forth in the

referenced LAX Northside Design Guidelines and Standards, as may be amended by BOAC from time to time.

2. Executive Director's Authority. The Executive Director shall have the authority to review each Project for compliance with all applicable provisions of this Specific Plan and if in compliance, to grant an Administrative Clearance.

a. Procedures. The Executive Director shall establish the appropriate procedures, forms and fees required for the Administrative Clearance application. The Los Angeles World Airports (LAWA) shall notify the Department of City Planning, the Council Office and Neighborhood Council representing the proposed Project.

b. Ministerial Review. The Administrative Clearance shall be a ministerial review of the applicable provisions of this Specific Plan and determination of whether a Project is in substantial conformance with those regulations, guidelines and mitigation measures as set forth in this Specific Plan, as applicable, and including the LAX Northside Plan Design Guidelines and Design Guidelines and Standards, as may be amended by BOAC from time to time.

3. Modification of Administrative Clearance. Once an Administrative Clearance has been issued, any subsequent proposed material modification to the Project shall require a review by the Executive Director, who shall grant approval of the modification if he or she finds the modification complies with this Specific Plan and the LAX Northside Plan Design Guidelines and Guidelines and Standards, as may be amended by BOAC from time to time.

a. Procedures. To modify an approved Project, LAWA shall file an application pursuant to the application procedures set forth in Subsection D.2 above. The application shall include a description of the proposed modification.

b. Exceptions. Any demolition or construction for which a permit is required to comply with an order issued by the Department of Building and Safety to repair or replace or demolish an unsafe or substandard condition.

4. Application Materials. To apply for an Administrative Clearance, an applicant shall file an application with the Executive Director, on a form provided by LAWA, and include a complete Project Description, site plan, elevations, and landscape plan, that conforms with the LAX Northside Plan Design Guidelines and Guidelines and Standards, updated trip rate information (if applicable) or other updated information necessary to comply with the LAX Northside Plan Design Guidelines and Guidelines and Standards.

5. Administration. LAWA shall maintain a record of the Trip allocations made pursuant to this Section 11 of the LAX Specific Plan, Trips or Transferred Trips utilized for

Projects subsequent to the effective date of the Specific Plan, Trips arising from demolition of any building or portion thereof, Trips arising from change of use of a building or portion thereof (changing the Trips generated by such building), any transfers of Trips between parcels within the LAX Northside Sub Area, any allocation of Trips to specific lots resulting from a subdivision, and such other records as may be necessary or desirable to provide an accurate and up-to-date account of the Trips and Transferred Trips available for use on any lot within the LAX Northside Sub Area of the LAX Specific Plan. Such records shall be available for public inspection. Any change in the number of Trips or Transferred Trips available to any lot or lots shall be evidenced in recorded document in a form designed to run with the land and signed by LAWA or any subsequent owner(s) of the lot or lots involved.

E. Design Guidelines and Standards. Projects within the LAX Northside Sub-Area shall comply with the LAX Northside Plan Design Guidelines and Guidelines and Standards. The LAX Northside Design Guidelines and Standards, as adopted by BOAC, are incorporated herein by reference. Said Design Guidelines and Guidelines Standards, excluding Site Development Standards, may be revised/amended from time to time, as necessary, subject to the approval of BOAC. The Los Angeles World Airports (LAWA) shall notify the Department of City Planning, the Council Office and local Neighborhood Council(s) representing the proposed Project at least 45 days prior to the proposed BOAC Board Meeting for which changes to the Design Guidelines and Standards is ajenized.

(notification or in consult with Planning?)

F. Permitted Uses. The following uses shall be permitted in the LAX Northside Sub-Area, also designated as the LAX-N Zone, within the Specific Plan Area:

1. Buffer Sub-Zone Uses (B)

- a. Underground infrastructure and utilities, with limited related surface structures
- b. Ancillary uses and structures related to the primary permitted uses

2. Community & Civic Sub-Zone Uses (CC)

- a. Non-Profit business or institutions that serve the local community
- b. Outside recreation related to Non-Profit institutions such as swimming pools and athletic courts / fields
- c. City, County or State government buildings including senior center, fire stations, and libraries
- d. Cultural institutions such as performing arts or museums
- e. Other community-serving uses
- f. Non-Profit Club or lodge
- g. Underground infrastructure and utilities, with limited related surface structures
- h. Ancillary uses and structures related to the primary permitted uses

3. Office & Research Sub-Zone Uses (RD)

- a. Community & Civic Sub-Zone uses, except no conditional approvals are required
 - b. Office, general business or professional
 - c. Medical or Dental Office, including Surgery Center, Outpatient Services, Primary Care Clinic, and Pharmacy
 - d. Research and Development including office, engineering, showroom, laboratory. Limited test and assembly of not-for-sale prototypes is permitted
 - e. Business College, professional or scientific school or college, not including trade schools involving shop work, or the repair or maintenance of machinery or equipment
 - f. Media, Post-Production, or Broadcast studio, without transmission towers
 - g. Animal care or boarding facility, with conditions
 - h. Underground infrastructure and utilities, with limited related surface structures
 - i. All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14
 - j. Ancillary uses and structures related to the primary permitted uses
4. Recreation & Open Space Sub-Zone Uses (OS)
- a. Golf Course
 - b. Athletic fields, outdoor athletic courts
 - c. Public shade structures, picnic areas, and restrooms
 - d. Dog Park, with conditions
 - e. Below grade storm water treatment facilities, with conditions
 - f. Underground infrastructure and utilities, with limited related surface structures
 - g. Ancillary uses and structures related to the primary permitted uses
5. Mixed Use Sub-Zone Uses (MU)
- a. Banks or financial institutions
 - b. Retail, including merchandise sales and local services such as Bakery, Barber Shop, Beauty Shop, Book Store, Stationary Store, Software or Computer Store, Toy Store, Clothes Cleaners, Tailors, Florist or Gift Shop, Real Estate, Hardware or Appliance Store, or Jewelry Store.
 - c. Professional Office
 - d. Restaurants, including sit down, quick serve, and drive through
 - e. Hotel, including related restaurants, services, and parking
 - f. Transit station
 - g. Medical or Dental Office, including Surgery Center, Outpatient Services, Primary Care Clinic, and Pharmacy
 - h. Animal medical clinic
 - i. Parking
 - j. Wholesale stores, with conditions
 - k. Auto repair or service, with conditions

- l. Auto fueling stations, with conditions
- m. Underground infrastructure and utilities, with limited related surface structures
- n. All of the uses permitted in the C2 Zone, as specified in LAMC Section 12.14
- o. Ancillary uses and structures related to the primary permitted uses

6. Airport Support Sub-Zone Uses (AS)

- a. Maintenance and repair shops
- b. Indoor storage and warehouses
- c. Exterior storage
- d. Administrative offices
- e. Radars & surveillance facilities
- f. Utilities and Utility related structures
- g. Construction material temporary storage
- h. Recycling sorting and storage
- i. Parking
- j. Underground infrastructure and utilities, with limited related surface structures
- k. All of the uses permitted in the M2 Zone, as specified in LAMC Section 12.19
- l. Ancillary uses and structures related to the primary permitted uses

7. Additional Permitted Uses. In addition to those uses permitted by Subsections F.1 – F.6 of Section 11, uses similar or incident to those hereby permitted, all uses requiring a Conditional Use Permit pursuant to LAMC Section 12.24, and uses and operations deemed necessary for the safe and efficient operation of the airport shall be allowable as determined by the Executive Director.

G. Prohibited Uses. The following uses shall be prohibited in the LAX Northside Sub-Area, also designated as the LAX-N Zone, within the Specific Plan Area:

- 1. Residential, or dwelling units of any kind
- 2. K-12 education
- 3. Retail stores over 100,000 gross square feet
- 4. Auto dealerships
- 5. Adult business as defined in LAMC 12.70
- 6. Massage parlors as defined in LAMC 12.70
- 7. Parking as a primary use, except in Airport Support and Mixed Use Sub-Zones
- 8. Hazardous materials testing

H. Site Development Standards

- 1. Total Floor Area Permitted
 - a. The total floor area of all development within the LAX Northside Sub-Area shall not exceed 2,320,000 square feet.
 - b. No Administrative Clearance shall be approved for a Project within

the LAX Northside Sub-Area that exceeds the total allowable square footage (see Table 1) within the LAX Northside Sub-Area subject to transfers and equivalencies as permitted pursuant to Subsection H.3 below.

2. Density Allocation

a. The total floor area within the LAX Northside Sub-Area shall not exceed the following:

1. North of Westchester Parkway, West of Lincoln Boulevard: 645,000 sf (Campus District: Areas 1, 2 and 3)
2. North of Westchester Parkway, East of Lincoln Boulevard: 1,075,000 sf (Center District: Areas 11, 12 and 13)
3. South of Westchester Parkway: 600,000 sf (Airport Support District: Areas 4 - 10)

**Table 1
LAX Northside Sub-Area Development Standards
Permitted Land Uses, Heights, and Square Footages**

Area	Permitted Land Use Category^a	Maximum Height From Grade	Net New Square Footage
LAX Northside Campus District			
Area 1	Open Space and Recreation		
	Office, Research and Development	45	10,000^b
Areas 2 & 3	Office, Research and Development		
	Community and Civic	Area 2: 45	
	Open Space and Recreation	Area 3: 60	1,065,000
LAX Northside Center District			
Areas 11, 12A East	Mixed Commercial Use		
	Community and Civic	60	470,000
Area 12A West	Community and Civic	30	130,000
Area 12B	Open Space and Recreation	N/A^b	N/A^b
Area 13	Community and Civic	45	45,000
Airport Support District			
Areas 4-10	Airport Support	30	600,000
Total			2,320,000

Footnotes: (a) For a more detailed list of permitted uses, see Subsection F. (b) Intended for recreational support structures including snack shops, toilets, office space, equipment storage, and maintenance storage. No change proposed to Westchester Golf Course.

3. Transfer and Equivalency Program

a. Purpose

The Transfer and Equivalency Program, including the Land Use Equivalency Matrix, is established to provide development flexibility by permitting transfers and exchanges of development rights and land uses between Areas within the LAX Northside Center, LAX Northside Campus, and LAX Northside Airport Support Districts over the life of the Specific Plan, while maintaining the intent and regulatory requirements of the Specific Plan. The Transfer and Equivalency Program allows for Floor Area reallocations between land uses and Areas within Districts, utilizing conversion factors that are based upon environmental impact and vehicle trip equivalencies.

b. Authority

1. The Executive Director shall have the authority to grant a Transfer of Floor Area or Equivalency Exchange to an applicant.
2. In granting a Transfer of Floor Area or Equivalency Exchange, the Executive Director or the City Council on appeal, shall make the following findings:
 - i. That the Transfer of Floor Area or Equivalency Exchange is consistent with the objectives of the Specific Plan, in conformance with the LAX Plan, and in compliance with the LAX Northside Plan Update Design Guidelines and Plan and Development Guidelines Standards;
 - ii. That the modification in floor area and/or use is appropriate with respect to its location, the transportation network and other public improvements, and will not result in impacts greater than those specified in the Environmental Impact Report for the LAX Northside Plan Update; and
 - iii. That the modification in floor area and/or use is compatible with existing and/or proposed surrounding development.

c. Limitations

1. Transfers of floor area or land uses are allowed within the LAX Northside Campus District (between Areas 1-3), within the LAX Northside Center District (between Areas 11-13), and within the Airport Support District (between Areas 4-10). Transfers between LAX Northside Districts are prohibited.
2. The land use designated on all Areas within a designated LAX Northside District may be exchanged for another land use, so long as the new use is otherwise permitted by this Specific Plan and the Floor Area of the new use is in conformance with the Land Use Equivalency Matrix. In no event shall the maximum permitted Floor Area exceed 2,320,000 square feet.
3. In no event shall the maximum number of trips generated by the

LAX Northside exceed 23,635 total daily vehicle trips.

4. Floor Area and associated trips which have been transferred, but not utilized, may be transferred to any other Area within the same District.

d. Procedure

1. The owner of a lot or lots seeking a Transfer or Equivalency Exchange shall file an application for Administrative Clearance approval with the Executive Director. The application shall be accompanied by a proposed Transfer or Equivalency Exchange Plan.
2. LAWA shall establish and maintain a record of all Transfers of Floor Area and Equivalency Exchanges. The records shall be transmitted annually to the Board of Airport Commissioners for its review and shall be available for public inspection.
3. Any Transfer of Floor Area or Equivalency Exchange approved pursuant to this Section shall be evidenced, prior to the issuance of a building permit, by a Covenant approved by the Executive Director and executed and recorded by the transferor and transferee, the obligations and benefits of which run with the land and are binding on subsequent owners or assignees. The Covenant shall specify the total floor area being transferred and the remaining floor area, if any, that may be transferred between Areas.

4. Accounting

a. Floor Area Calculations: Every application for a Transfer or Equivalency Exchange shall include a table that identifies at least the following:

1. Proposed project floor area.
2. New total of developed floor area within the LAX Northside Sub-Area.
3. Total allowed floor area within the LAX Northside Sub-Area, including any density reallocations previously approved.
4. New total developed area for all areas within the LAX Northside Sub-Area.
5. Total allowed floor area within the LAX Northside Sub-Area boundary.

b. Trip Generation Calculations: Every application for a Transfer or Equivalency Exchange shall include a table that identifies at least the following:

1. Proposed project trip generation.
2. New total trip generation within the LAX Northside Sub-Area.
3. New total trip generation for all areas within the LAX Northside Sub-Area boundary.

Table 2

LAX Northside Land Use Equivalency Matrix

From this land use ↓	To this land use →	Open Space (Dog Park) (fields)			Community Civic (ksf)			Office (ksf)			R&D (ksf)			LAX Facilities (employees)			Retail (ksf)		
		Daily	AM	PM	Daily	AM	PM	Daily	AM	PM	Daily	AM	PM	Daily	AM	PM	Daily	AM	PM
Open Space (Dog Park)		1.00	1.00	1.00	3.28	0.91	15.01	7.80	1.04	16.59	10.32	1.43	23.23	35.67	10.94	36.91	2.37	2.00	7.92
Community/Civic		0.30	1.10	0.07	1.00	1.00	1.00	2.38	1.15	1.11	3.14	1.57	1.55	10.87	12.02	2.46	0.72	2.20	0.53
Office		0.13	0.96	0.06	0.42	0.87	0.90	1.00	1.00	1.00	1.32	1.37	1.40	4.57	10.49	2.23	0.30	1.92	0.48
R&D		0.10	0.70	0.04	0.32	0.64	0.65	0.76	0.73	0.71	1.00	1.00	1.00	3.46	7.65	1.59	0.23	1.40	0.34
LAX Facilities		0.03	0.09	0.03	0.09	0.08	0.41	0.22	0.10	0.45	0.29	0.13	0.63	1.00	1.00	1.00	0.07	0.18	0.21
Retail		0.42	0.50	0.13	1.38	0.45	1.90	3.29	0.52	2.10	4.35	0.71	2.93	15.03	5.47	4.66	1.00	1.00	1.00

I. Building Development Limitations

A. Building height shall be measured from mean sea level (MSL)

B. Building height limitations are as follows:

1. Area 1 and 2	45 ft
2. Area 3	60 ft
3. Area 4 through 10	30 ft
4. Area 12B	30 ft
5. Area 12A West	20 ft
6. Area 11 & 12A East	60 ft
7. Area 13	45 ft

C. Setbacks are measured from the LAX Northside Sub-Area Area boundaries or Right of Way within the LAX Northside Sub-Area boundaries, regardless of internal parcels or lot lines. Setbacks apply to Buildings, Ancillary Structures, and Parking. Only landscaping is allowed within setback areas.

D. Setback limitations are as follows:

1. Areas 1, 2, and 3 at Westchester Parkway	38 ft
2. Area 1 west boundary	38 ft
3. Area 1 north boundary	80 ft
4. Area 1 east boundary at Falmouth Ave	30 ft
5. Area 2 west boundary at Falmouth Ave	30 ft
6. Area 2 between OS and RD Sub-Zones	20 ft
7. Area 2 between RD/CC and B Sub-Zones	20 ft
8. Area 2 east boundary at Loyola Ave	15 ft
9. Area 2 north boundary	15 ft
10. Area 3 west boundary at Loyola Ave	15 ft
11. Area 3 east and north boundaries	20 ft
12. Area 13 west boundary at Lincoln Blvd	15 ft
13. Area 13 north and east boundaries	20 ft
14. Area 12A West north and west boundaries	20 ft
15. Area 12A West south and east boundaries	15 ft
16. Area 12A East west at Emerson Ave, south, east boundaries	15 ft

17.	Area 12A east, west	20 ft
18.	Area 12A East north at 88th Ave	30 ft
19.	Area 11 east, west	15 ft
20.	Area 11 north	30 ft
21.	Area 11 south	50 ft
22.	Area 4 west	50 ft
23.	Area 4 southwest	20 ft
24.	Areas 4 thru 6 north	50 ft
25.	Areas 5 thru 10 east and west boundaries	15 ft
26.	Areas 7 thru 10 north boundaries	15 ft
27.	Areas 4 thru 10 south boundaries at airfield	0 ft

E. Building Orientation

1. Sixty-five percent (65%) of the proposed project ground floor building square footage shall be located within 250 feet of the Westchester Parkway property line.
2. Auxiliary buildings shall not be located along Westchester Parkway, Sepulveda Westway, La Tijera Boulevard, Loyola Boulevard, Falmouth Avenue, or Pershing Drive.

J. Adjustments and Exceptions Any project that does not comply with the standards and limitations within this Specific Plan and the LAX Northside Plan Design Guidelines may request relief from the Department of City Planning pursuant to Municipal Code Section 11.5.7.E and 11.5.7.F. In addition to the notice provisions set forth in those sections, the Department of City Planning shall provide written notice of any adjustment or exception application and any adjustment or exception determination to the cities of El Segundo, Inglewood, Culver City and the County of Los Angeles.

~~E. Design Plan and Guidelines.~~ In addition to Appendix A, projects shall comply with the "Design Plan and Guidelines for LAX Northside" by Albert C. Martin and Associates, dated April 20, 1989, and any amendment adopted by BOAC after that date

~~F. Area 13.~~ Notwithstanding the provisions of Subsections C and E above, the requirements in Appendix A and the "Design Plan and Guidelines for LAX Northside" by Albert C. Martin and Associates, dated April 20, 1989, shall not apply to Area 13 of the LAX Northside Sub-Area. Area 13 shall be used for recreational facilities and other public benefit type uses, including child care, children's play area, picnic amenities, athletic fields, parks, libraries, and similar uses.

Sec.12. Section 12. TRANSPORTATION REGULATIONS.

A. Right-of-Way Dedications.

- 1.- Right-of-way dedications, or any similar grant of rights to use land for public street

purposes, shall be made for any public streets to the satisfaction of the LADOT General Manager and the City Engineer. All dedications shall be in conformance with City of Los Angeles standard street dimensions, unless deemed unwarranted by the LADOT General Manager and the City Engineer based on any environmental review and/or traffic analyses. The LADOT General Manager and the City Engineer may also allow variations from dedications to standard street dimensions if the variation is necessary to assure proper integration of a dedication into existing on-site conditions. The following public roadways within the Specific Plan Area are designated as major or secondary highways:

- (a) 96th Street
- (b) 111th Street
- (c) Airport Boulevard
- (d) Aviation Boulevard
- (e) Bellanca Avenue
- (f) Century Boulevard
- (g) Fallmouth Avenue
- (h) Imperial Highway
- (i) La Cienega Boulevard
- (j) La Tijera Boulevard
- (k) Lincoln Boulevard
- (l) Pershing Drive
- (m) Sepulveda Boulevard
- (n) Westchester Parkway/Arbor Vitae Street

2. The use of airspace above a public street shall be permitted so long as the surface elements of that street may be maintained for street purposes.

3. Any roadway within the Airport Airside and Airport Landside Sub-Areas, which is not already designated as a public street, may be designated as an internal airport roadway. The LADOT General Manager's authority in the design of these roadways shall be advisory only, except that the intersection of these roadways with public streets shall be to the satisfaction of the LADOT General Manager and the City Engineer. Maintenance of internal airport roadways shall be the responsibility of LAWA.

B. Required Traffic Improvements - Airport Airside and Airport Landside Sub-Areas Only.

A transportation improvements phasing plan shall be prepared by LAWA and approved by the LADOT General Manager that is in conformance with the LAX Plan and the certified Master Plan Final Environmental Impact Statement/Environmental Impact Report, and includes all on-site and off-site improvements as required by the LADOT General Manager. Revisions may be made to the transportation improvements phasing plan in conformance with any subsequent environmental review, or where appropriate, as determined by the LADOT General Manager.

Transportation improvements shall be constructed or suitably guaranteed to the satisfaction of the LADOT General Manager and the City Engineer in connection with individual Projects or development sites as specified in the transportation improvements phasing plan. Prior to the issuance of any final certificate of occupancy in the final phase of the transportation improvements phasing plan, all required improvements in the entire phasing plan shall be funded, completed, or resolved to the satisfaction of the LADOT General Manager. If a proposed traffic

mitigation measure does not receive the required approval, a substitute mitigation measure may be provided subject to approval by the LADOT General Manager, or other governing agency with jurisdiction over the mitigation location, upon demonstration that the substitute measure is equivalent or superior to the original measure in mitigating the Project's significant traffic impact.

C. Project Trip Generation.

1. In an effort to monitor traffic impacts and traffic mitigation measures, LADOT and LAWA shall jointly conduct traffic counts or otherwise determine the traffic impacts of Projects within the Master Plan. The conclusions of these counts and other determinations shall be incorporated into a traffic generation report, which shall be approved by the LADOT General Manager and annually submitted to BOAC, City Council, and the Department of City Planning.

The Master Plan FEIS/EIR forecasts the net new Trips at full build out of the Master Plan, after implementation of mitigation measures, to be no more than 8,236 trips at airport peak hour. If the annual traffic generation report described above, and/or the annual traffic generation report considered together with any Project-specific traffic study, shows that development of the Master Plan is likely to increase the Trips beyond 8,236, LAWA shall complete the Specific Plan Amendment Study required in Section 7 H of this Specific Plan.

No Specific Plan Amendment Study shall be required if the annual traffic generation report, and/or the annual traffic generation report considered together with any Project-specific traffic study, determines that the net new Trips are anticipated to exceed 8,236 in the airport peak hour, but this increase in Trips will only be temporary until the Project(s) and associated mitigation measures are complete and/or if this increase in Trips is consistent with the number of Trips anticipated to occur during the peak year of traffic impacts as analyzed in the

Master Plan FEIS/EIR. In this case, the traffic generation report shall evaluate the effectiveness of future Projects and mitigation measures in ultimately reducing the number of net new Trips to 8,236 in the airport peak hour at build-out of the Master Plan. Any LAX Plan Compliance approval for a Project shall include any conditions necessary to ensure the ultimate, reduction. If Trip reduction program measures are recommended, LAWA shall include in future annual reports an analysis of the on-going effectiveness of those measures and, if the Trip reductions are not effectuated, additional measures may be implemented and/or a Specific Plan Amendment Study may be triggered.

2. LAX Northside Sub-Area. As part of the annual traffic generation report, the number of Trips generated by each project shall be documented so that the total number of Trips generated by on-going development is monitored and reviewed by the LADOT General Manager and the Executive Director for consistency with the maximum allowable number of a.m. and p.m. peak Trips. LADOT and LAWA shall agree on procedures for this documentation. These counts shall be taken at the expense of the Applicant.

A reduction in the total number of Trips permitted to be generated by development in the LAX Northside Sub-Area from that which was approved for this area under

previous entitlements, is required as a result of the LAX Plan and shall be imposed by way of this Specific Plan.

All projects within the LAX Northside Sub-Area together shall not generate more than 3,922,009 project-related Trips in the a.m. peak hour (part of the total 6,496 net new a.m. peak hour Trips for the LAX Master Plan) and 4,422,543 project-related Trips in the p.m. peak hour (part of the total 6,914 net new p.m. peak hour Trips for the LAX Master Plan). The number of Trips generated by a project shall be based on the trip generation rates used in Ordinance No. 168,999 (Coastal Transportation Corridor Specific Plan Part (CTCSP)) and/or determined appropriate by the LADOT General Manager and on square footages of the proposed project. In conjunction with each application for Project Permit Compliance Review for a project within the LAX Northside Sub-Area, the Applicant shall estimate the number of Trips generated by each project and submit the estimate to the LADOT General Manager for review and approval. This subsection shall not apply to development within Area 13 as shown on Map 3.

D. -Automated People Mover System. The APM(e) shall be optimally designed and constructed to minimize disruption and vehicle delay on the public roadway and transit system, and shall be elevated above street level wherever possible to minimize at-grade crossing points with public roadways. LAWA shall consult with the LADOT General Manager and the City Engineer early in the design process of the APM.

Any rules and regulations of the Public Utilities Commission of the State of California governing the APM, including but not limited to its design, operation, and maintenance,

shall supersede any other provision of this Specific Plan and any building or zoning ordinances to the contrary.

Sec. Section 13. PARKING REGULATIONS.

A. Requirements- Airport Airside and Airport Landside Sub-Areas Only.

1. It is anticipated that approximately 32,155 off-street parking spaces may be provided at build out of the LAX Master Plan, however, notwithstanding LAMC Section 12.21 A 4, no more than 35,712 off-street parking spaces shall be provided at build-out of the LAX Master Plan for passengers, visitors, and airport and airline employees. –Parking shall be comprised of short-term, long-term, and employee parking spaces.
2. –Notwithstanding- LAMC Section 12.21 A 4 (g), parking may be located at any location within the Airport Airside and Airport Landside Sub-Areas.
3. Subsections 1 and 2 above shall not apply to off-street parking for visitors and employees of cargo-only facilities and commercial/industrial buildings located within the area bounded by Century Boulevard, Aviation Boulevard, Imperial Highway and Sepulveda Boulevard, and the area west of Sepulveda Boulevard and north of Imperial Highway. Off-street parking for these cargo-only facilities and commercial/industrial buildings shall be provided in compliance with the provisions of LAMC Section 12.21.A.4(c). The Executive Director may also permit two or more of these uses to share off-street parking spaces, if the Executive Director determines that a lower total number of parking spaces than would otherwise be required will provide adequate parking for these uses.

B. –Requirements - LAX Northside Sub-Area Only. All projects within the LAX Northside Sub-Area shall provide off-street parking ~~as specified in Appendix per LAMC 12.24.A.4. and the "Design Plan and Guidelines for LAX Northside" by Albert C. Martin and Associates, dated April 20, 1989, and any amendment adopted by BOAC after that date~~

Sec. Section 14. SIGN REGULATIONS.

A. General Requirements. The Department of Building and Safety shall issue sign permits for any signs otherwise requiring a permit pursuant to Article 6, Chapter IX of the LAMC that are regulated by this Specific Plan. – All signs and sign support structures that are erected and maintained on property owned or controlled, in whole or in part, by LAWA shall be reviewed by the Department of Building and Safety pursuant to LAMC ~~LAMC~~ Sections 14.4.1, 14.4.2, 14.4.3, 14.4.4 A –14.4.4 I, 14.4.11 ~~C - C~~ 14.4.11. E, 14.4.12 F,

14.4.15 ~~–~~ 14.4.16A, 14.4.16 C, 14.4.16 E, 14.4.18 H, 14.4.181, 14.4.19, 91.6201.2.1, ~~–~~

91.6201.3, ~~–~~ 91.6202, ~~2,~~ 91.6204, 91.6205 ~~–~~ 91.6205.18 ~~–~~ 91.6207 ~~–~~ 91.6209 ~~–~~ 91.6210, ~~–~~

91.6211 ~~–~~ 91.6212, 91.6213, ~~a,~~ and 91.6216. The Executive Director shall review signs for conformance with all other sign provisions of Article 4.4, Chapter 1 and Article 1, ~~–~~

Division 62 of Chapter IX of the LAMC and provide a written approval to the Department ~~o-~~

of Building and Safety prior to issuance of any sign permit. Types of signs identified in this Specific Plan are defined in LAMC Section 14.4.2. Where materials for signs are not specified in the LAMC, materials shall be approved by the Departments of Building and Safety and Fire. Except for Subsections D and E, the provisions of this Section shall apply only to signs within the Airport Airside and Airport Landside Sub-Areas.

B. Area, location, Spacing and Height and Review Procedure. Signs shall not be subject to the LAX Plan Compliance Review procedure, but shall be reviewed and approved by the Executive Director prior to issuance of any permit for, or installation of, a sign or sign support structure.

Prior to approving any sign, the Executive Director shall determine if the proposed sign is in conformance with the sign guidelines adopted by BOAC. If the adopted guidelines address area, spacing, location, and height of signs, the Executive Director may only approve a sign if it is consistent with those guidelines. If the guidelines do not address area, spacing, location, and height of signs, then the Executive Director may only approve signs that are consistent with the appropriate area, spacing, location, and height regulations in Article 4.4, Chapter I of the LAMC.

Prior to approving any sign, the Executive Director, after consultation with LADOT, shall also determine that the sign is not a hazard to traffic nor will it result in unsafe freeway exposure.

C. Off-Site, Supergraphic, and Mural Signs. Alteration, redesign or replacement of existing off-site signs, or erection, construction or installation of new off-site signs, supergraphic signs, and mural signs shall be permitted pursuant to the establishment of a sign district as set forth in LAMC Section 13.11.

D. Requirements - LAX Northside Sub-Area Only. Signs within the LAX Northside Sub-Area shall be in compliance with the requirements set forth in ~~Appendix A~~ Section 11 and the "LAX Design Plan and Development Guidelines and Standards for the LAX Northside" by ~~Albert C. Martin and Associates, dated April 20, 1989~~ Sub-Area, and any amendment adopted by BOAC after that date.

E. State and Federally Required Signage. Any signs or sign restrictions mandated by the FAA, TSA, or any other State or Federal regulatory agency shall supersede any other provision of this Specific Plan to the contrary.

See Section 15. SEVERABILITY. If any provision of this Specific Plan or its application to any person or circumstance is held to be unconstitutional or otherwise invalid by any court of competent jurisdiction, the invalidity shall not affect other Specific Plan provisions, clauses or applications, which can be implemented without the invalid provision, clause or application, and to this end the provisions and clauses of this Specific Plan are declared to be severable.

See Section 16. The City Clerk shall certify to the passage of this ordinance and have it published in accordance with Council policy, either in a daily newspaper circulated in the City of Los Angeles

or by posting for ten days in three public places in the City of Los Angeles: -one copy on the bulletin board located in the Main Street lobby to the City Hall; one copy on the bulletin board located at the ground level at the Los Angeles Street entrance to the Los Angeles Police Department; and one copy on the bulletin board located at the Temple Street entrance to the Los Angeles County Hall of Records.

I hereby certify that the foregoing was introduced at the meeting of the Council of the City of Los Angeles, of _____, and was passed at its meeting of

MAX 2 1 2013

~~JUNE LAGMAY, City Clerk~~

Approved _____

Mayer

~~Approved as to Form and Legality~~

~~CARMEN A. TRUTANICH, City Attorney~~

By _____

~~[LAWA REDLINE DRAFT – APRIL 18]~~JULY/AUGUST~~JULY 29-1-24, 2014]~~

~~Pursuant to Charter Section 550, I approve this ordinance on behalf of the City Planning Commission and recommend it be adopted, _____.~~

~~File No(s) CF No. 13-0285; CPC File No. 2012-3357 GPA SP~~

DRAFT

DECLARATION OF POSTING ORDINANCE

I, ~~MARIA VIZCARRA~~, state as follows: I am, and was at all times hereinafter mentioned, a resident of the State of California, over the age of eighteen years, and a Deputy City Clerk of the City of Los Angeles, California.

~~Ordinance No. 182542 - Establishing a Specific Plan, known as the LAX Specific Plan within the LAX Plan area~~ a copy of which is hereto attached, was finally adopted by the Los Angeles City Council on ~~May 21, 2013~~, and under the direction of said City Council and the City Clerk, pursuant to Section 251 of the Charter of the City of Los Angeles and Ordinance No. 172050, on ~~May 24, 2013~~ I posted a true copy of said ordinance at each of the three public places located in the City of Los Angeles, California, as follows: 1) one copy on the bulletin board located at the Main Street entrance to the Los Angeles City Hall; 2) one copy on the bulletin board located at the Main Street entrance to the Los Angeles City Hall East; 3) one copy on the bulletin board located at the Temple Street entrance to the Los Angeles County Hall of Records.

Copies of said ordinance were posted conspicuously beginning on ~~May 24, 2013~~ and will be continuously posted for ten or more days.

I declare under penalty of perjury that the foregoing is true and correct.

Signed this ~~24th~~ day of ~~May, 2013~~ at Los Angeles, California.

Maria Vizcarra, Deputy City Clerk

Ordinance Effective Date: ~~July 3, 2013~~
Rev. (2/2106)

Council File No. 13-0285

APPENDIX A

Requirements for Development within the LAX Northside Sub-Area

1. The subject property shall be used for those principal and accessory uses in development areas as indicated on Map 3, as follows:
 - a. Area East of Lincoln Boulevard
 - ~~Area 8~~ Commercial uses, including office and restaurant uses; and automobile service station.
 - ~~Area 9~~ Commercial uses, including office and restaurant uses; automobile service station in the western portion only; public automobile parking in the eastern portion only; and an airport view site.
 - ~~Area 10~~ Public automobile parking.
 - ~~Area 11~~ Commercial uses, including hotel, office, restaurant, service and retail uses and a movie theater complex.
 - ~~Area 12A~~ Commercial uses, including offices, hotel, restaurant, service and retail uses.
 - ~~Area 12B~~ A commercial golf course, including golf driving tees and ranges and similar commercial golf uses.
 - b. Area Between Lincoln Boulevard and Falmouth Avenue (Extended)
 - ~~Area 2~~ Offices, business park and research and development center.
 - ~~Area 3~~ Commercial uses, including hotel, offices, restaurant, service and retail uses.
 - ~~Area 4B~~ Light industrial uses, airline and airport support services, including flight kitchens, caterers, ticket reservation centers, custom house brokers, warehouse, airport maintenance and ground services (excluding engine shops), freight forwarders, package delivery services, security services, import/export services and other similar accessory or support services; business park and research and development center; and offices within 400 feet west of the prolongation of Loyola Boulevard on the south side of Westchester Parkway.
 - ~~Areas 5, 6 and 7~~ Offices, business park and research and development center; airport view site; airline and airport support and accessory uses.
 - c. Area West of Falmouth Avenue (Extended)
 - ~~Area 1~~ Offices, business park and research and development center.
 - ~~Area 4A~~ Light industrial uses, airline and airport support services, including flight kitchens, caterers, ticket reservation centers, custom house brokers, warehouse, airport maintenance and ground services (excluding engine shops), freight forwarders, package delivery services, security services, import/export services and other similar accessory or support services; business park and research and development center; and offices within 400 feet east of Pershing Drive on the south side of Westchester Parkway.

2. ~~No aircraft or engine run-ups shall be permitted within the project boundaries.~~
3. ~~The height of structures shall be controlled as follows:~~
 - a. ~~Area 2~~ No structure located within 100 feet of the north airport property line between Loyola Boulevard and Hastings Avenue shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot. All accessory facilities on the roofs of such buildings, such as air conditioning units and other equipment, shall not be visible from nearby residential properties to the north and are exempted from the height restriction.
 - b. ~~Areas 1 and 4A~~ No structures located within 200 feet of the north airport property line between Falmouth Avenue and Pershing Drive shall exceed three stories (including parking levels) or 45 feet in height measured from the finished grade of the lot. No structure located within Areas 1 or 4A south thereof, shall exceed four stories (including parking levels) or 55 feet in height measured from the finished grade of the lot. All accessory facilities on the roofs of buildings within Area 1, such as air conditioning units and other equipment, shall not be visible from nearby residential properties to the north and are exempted from the height restriction.
4. ~~Structures shall observe minimum setbacks, as follows:~~
 - a. ~~Area 2~~ All structures shall observe a minimum setback of 50 feet from the north airport property line between Loyola Boulevard and Hastings Avenue. Open parking areas are permitted within the required building setback area, except as may be modified by required landscape buffer setbacks.
 - b. ~~Area 1~~ All structures shall observe a minimum setback of 100 feet from the north airport property line between Falmouth Avenue and Pershing Drive. Open parking areas are permitted within the required building setback area, but not within the required landscape buffer setback.
5. ~~Landscaped buffer setbacks shall be required as follows:~~
 - a. ~~Areas 11 and 12A~~ A 30-foot wide landscaped buffer setback along 88th Street between Sepulveda Westway and Liberator Avenue shall be required and contain no buildings or structures, except for walls or fences and include trees, 15-gallon and 10-foot tall at the time of planting, planted at a maximum of 30 feet apart, of a type similar to those contained in Section 7 of the Planning Department's Technical Report on "Shrubs and Trees for Landscaping and Screening". Further, a 15-foot wide landscaped buffer setback along Liberator Avenue and 88th Place

~~(adjacent to the Emerson Manor School site) shall be required and include a dense screen of closely planted evergreen trees.~~

~~b. Area 2 A 15-foot wide landscaped buffer setback along Cum Laude Avenue and 92nd Street (adjacent to the St. Bernard High School site) shall be required and include a dense screen of closely planted evergreen trees.~~

~~c. Area 1 A 50-foot wide landscaped buffer setback along the north airport property line between Falmouth Avenue and Pershing Drive shall be required and include low, minimum-view obstructing shrubs and ground cover (no trees) which are compatible, where appropriate, with adjacent off-airport landscaping.~~

~~6. Prior to the issuance of building permits, an on-site roadway/highway construction phasing plan and individual parcel access plan shall be submitted to the Department of City Planning for approval and for coordination and review with LADOT and the Bureau of Engineering.~~

~~7. Prior to the recordation of a final tract map covering all or any portion of the subject property and prior to the approval of any variance granting use of any portion of the property before recordation of a final tract map, the Board of Airport Commissioners shall cause to be prepared and shall approve a project design plan, together with developmental guidelines for the entire project area. The primary purposes of the design plan and guidelines shall be to provide future developers with a unifying theme and design handbook and to provide the Department of Airports with a basis for reviewing and coordinating project development plans. The project design plan shall establish a unifying architectural theme, while the elements of the developmental guidelines shall include consideration of: Building design, height, bulk, locations and interrelationships; building materials, colors and textures; public and private area landscaping; public and private signage and graphics; airport view sites; street furniture; exterior lighting; internal pedestrian and vehicular circulation; parking area design; on and off airport interfaces; and other design considerations as may be appropriate. The project design plan and developmental guidelines shall be governed by the conditions of approval attached to this change of zone, wherever relevant.~~

~~8. The project design plan and developmental guidelines shall treat Area 11 as a single comprehensive planned unit, providing a compatible interface with the existing and potential future uses on the privately owned property adjoining the east side of Sepulveda Westway between La Tijera Boulevard and Will Rogers Street. Convenient pedestrian access to Area 11 shall be provided from Sepulveda Westway and the Sepulveda Westway frontage shall be attractively landscaped. The design of Area 11 shall provide, to the extent practicable and~~

~~feasible, for visual continuity and accessibility between Area 11 and the properties on the east side of Sepulveda Westway.~~

- ~~9. All open areas not used for buildings, driveways, parking areas, recreational facilities or walks shall be attractively landscaped in accordance with a landscape development plan prepared by a licensed landscape architect or licensed architect. Approved copies of such plans shall be submitted to the Department of Building and Safety before issuance of a building permit. All landscaped areas shall be equipped with automatic sprinklers and shall be maintained in a first class condition at all times. All types of plants selected and required watering systems for such landscaping shall, to the extent possible, conserve water and shall be consistent with any water conservation ordinance enacted by the City.~~
- ~~10. All open parking areas located within Area 1 shall devote at least four percent of the parking area to landscaping, with at least three-fourths of the four percent devoted to interior landscaping distributed throughout the parking area.~~
- ~~11. A 6 foot high chain link fence shall be provided along the airport property line in the following locations: Along Liberator Avenue and 88th Place (adjacent to the Emerson Manor School site in Area 12A); along Cum Laude Avenue and 92nd Street (adjacent to the St. Bernard Senior High School site in Area 2); and between Falmouth Avenue and Pershing Drive (along the north boundary of Area 4).~~
- ~~12. A solid 8 foot high masonry block wall shall be constructed along the north airport property line between Loyola Boulevard and Hastings Avenue (along the north boundary of Area 2) and trees and tall shrubs shall be planted immediately south of the wall on airport property to provide additional visual screening above the wall. The wall shall be installed and measured in height from the finished grade of the alley between Loyola Boulevard and Rayford Drive and from the finished grade of the adjoining residential lots between Rayford Drive and Hastings Avenue.~~
- ~~13. All development on the property shall provide off street parking on the following basis: One space for each guest room; one space for each 300 square feet of floor area for office use and for business parks and research and development centers; one space for each 250 square feet of floor area for retail and service commercial uses; 16 spaces for each 1,000 square feet of floor area for restaurants; and not less than three stalls for each four employees on the main shift for light industrial uses. Floor area shall mean the area enclosed within the walls of buildings, exclusive of floor area devoted to off street parking or accessory areas, as defined under Section 12.21-A, 4 of the Municipal Code. The Director of Planning may modify these parking standards where appropriate to permit reduce parking in mixed use or joint use facilities.~~

- ~~14. The hours of operation for trash pickup or freight deliveries or pickup, within 300 feet of the north boundary line of the project, shall be limited to between 7 a.m. and 9 p.m.~~
- ~~15. Adequate protection against exterior noise shall be included in the design and construction of hotels and motels. Adequate protection shall mean a noise reduction (exterior to interior) sufficient to insure that the interior community noise equivalent level (CNEL) in all habitable rooms does not exceed 45dB during aircraft operations.~~
- ~~16. All central air heating and/or air conditioning units shall be installed with an air filtration system (either charcoal or electronic) to improve the air quality effects on the project occupants. This requirement shall not preclude the installation of operable windows for passive or natural heating or cooling opportunities.~~
- ~~17. The use of any outdoor public address or paging system shall be permitted only south of the Westchester Parkway and shall utilize: (1) a low-pressure speaker system with each speaker having an audible range limited to a 400 square-foot area and placed a maximum of 40 feet apart or (2) a sound system designed by a qualified sound engineer so as to reduce the impulse noise level to inaudibility beyond the premises of the establishment.~~
- ~~18. In Areas 1, 2, 4A, 4B, 5, 6, and 7, all buildings and enclosing walls or fences shall be constructed, the machinery and equipment shall be so installed and maintained and the activities shall be so conducted that all noise, vibration, dust, odor and other objectionable factors shall be confined or reduced to the extent that there will be no reduction in the use of property by persons residing adjacent to the subject property. Whenever there is any difficulty in determining the application of these provisions to any specific case, the Department of Building and Safety shall make such determination.~~
- ~~19. All Building Identification Signs shall be designed by the architect of the building or facility. These Building Identification Signs shall be wall signs designed for placement on the face of the proposed building and not projecting above the roof or parapet wall. Except that free-standing Identification or Project Directory Signs not exceeding 50 sq. ft. in area on each face and not exceeding 8ft. in height may be permitted subject to individual review and approval by the Department of Airports.~~
- ~~20. All lighting shall be directed onto the site and no flood lighting shall be located as to be seen directly by the adjacent residential areas. This condition shall not preclude the installation of low level security lighting.~~
- ~~21. All buildings within 100 feet of the north airport property line between Loyola Boulevard and Hastings Avenue shall be designed so that all second story or higher windows that overlook the adjacent single family residential homes shall~~

not be transparent below an eye level of 6 feet on any floor, but shall be translucent; or the building shall be designed in such a way that windows will not overlook the single-family residential properties to the north.

- ~~22. Vehicular driveway ingress and egress from the subject property shall be prohibited from Cum Laude Avenue and from 92nd Street (Area 2); from Lincoln Boulevard between Sepulveda Boulevard and the airport property line north of the Westchester Parkway (Areas 6, 7, 8, 9, 10 and 12A); and from 88th Street, Liberator Avenue, 88th Place and Emerson Avenue (Area 12A).~~
- ~~23. In Areas 1, 2, 5, 6, and 7, all activities shall be conducted within an enclosed building. Within Areas 4A and 4B, all outdoor activities shall be located on the southerly side of the buildings and screened from public streets.~~
- ~~24. All Utilities within the project shall be installed underground.~~
- ~~25. All development within the project shall contain trash compactors to reduce the volume of solid waste to be removed from the site.~~
- ~~26. LAWA and individual developers shall develop and implement a transportation system and parking management plan which shall include the following as a minimum:
 - ~~a. Individual developers shall encourage public transit use by offering subsidized transit passes to employees.~~
 - ~~b. The Department of Airports shall evaluate the potential for subscription bus service for large concentrations of employees.~~
 - ~~c. Each developer employing 500 or more permanent employees shall provide company-owned vans and/or offer subsidies to individual vanpool operators and encourage the formation of vanpools.~~
 - ~~d. The Department of Airports shall make carpooling information available to building occupants and encourage formation of carpools and vanpools and shall establish a carpool matching service.~~
 - ~~e. Individual developers shall develop and implement programs to offer preferential parking to carpooling and vanpooling building occupants.~~
 - ~~f. The Department of Airports shall make transit schedules, maps and other transit information available to building employees and users.~~
 - ~~g. To the extent feasible, individual developers shall encourage flexible working hours.~~~~

- ~~h. The Department of Airports and each developer employing 500 or more permanent employees shall appoint a rideshare coordinator to administer the functions required by this condition.~~
- ~~27. Prior to major grading of the project site, a rodent control effort shall be undertaken to reduce the existing rodent population within the project area.~~
- ~~28. Developers of individual projects shall take the following steps relative to energy conservation:~~
 - ~~a. Consult with the Los Angeles Department of Water and Power and Southern California Gas Company to determine feasible energy conservation features which could be incorporated into the design of structures, beyond those required by State and City laws.~~
 - ~~b. Utilize, where cost effective, alternative sources of energy such as solar water and pool heating.~~
 - ~~c. Utilize the Department of Water and Power's energy pre-audits on all new industrial developments.~~
- ~~29. Individual developers shall consult with the Los Angeles Police Department on the provision of security measures for developments, which measures shall include the following at the minimum:~~
 - ~~a. For each developer responsible for 200,000 or more square feet of total floor area (Los Angeles Municipal Code 12.21.1), security guards shall patrol all levels of buildings and access to and from buildings shall be monitored by a security guard on duty during nighttime and weekends.~~
 - ~~b. Subterranean parking levels shall be lighted 24 hours a day.~~
 - ~~c. All parking levels, corridors and arcades shall be lighted during the nighttime and weekends.~~
- ~~30. A qualified archaeologist shall be available, as needed, during site grading and subsurface operations, with the authority to assure reasonable measures to insure protection and recovery of significant archaeological resources.~~
- ~~31. All conditions imposed by this action, with the exception of Condition No. 9 relating to a project design plan and development guidelines, may be fulfilled incrementally as individual phases of the subject property are developed.~~